



Debbie Middleton
Middlesbrough Borough Council
PO BOX 504
Civic Centre
Middlesbrough
TS1 9FY

23rd December 2024

BY EMAIL TO: debbie.middleton@middlesbrough.gov.uk craig.cowley@middlesbrough.gov.uk

Ref: PROJ-2371 Linthorpe Road Reinstatement Works / City Region Sustainable Transport Settlements (CRSTS1) PRGM-1389 / CF25-2024

Dear Debbie,

PART 1 - OFFER LETTER – PROJ-2371 - Linthorpe Road Reinstatement Works ('the Project')

Tees Valley Combined Authority ('TVCA') is pleased to confirm approval of the funding application in relation to the Project.

This Offer Letter, and the documents attached to it, constitute the terms and conditions of the Grant ('the Agreement'). The Agreement is therefore made up of the following Parts, which are to be read as a single document:-

- a) **Part 1 – this Offer Letter** – which you are required to sign to confirm your acceptance of the Grant on the terms and conditions set out in this Agreement.
- b) **Part 2 – the Funding Particulars** – which set out the amount of the Grant you will receive, the specific key terms which apply to the provision of the Grant, and any amendments to TVCA's Standard Terms and Conditions of Funding.
- c) **Part 3 – the Standard Funding Terms** – which apply to the provision of the Grant (subject to any amendments set out in the Funding Particulars); and
- d) **Part 4 – the Business Case** - which sets out the purposes for which the Grant was sought.

The words and expressions used in this Offer Letter have the meaning set out in the Standard Funding Terms at Part 3. The expressions 'You' and 'Yours' means the 'Grant Recipient' or 'the Grant Recipient's'.

1. Use of the Grant

The Grant is made strictly for the Agreed Purposes as set out in the Funding Particulars.

2. Next Steps

You are required to sign and date two (2) copies of this Offer Letter and to return both copies to TVCA at the following address within fourteen (14) days of receipt:-

Sally Cowley, Appraisal Manager, Tees Valley Combined Authority, Teesside Airport Business Suite, Teesside International Airport, Darlington DL2 1NJ.

TVCA shall sign both copies on receipt and return one fully signed copy to you for your records.

Please note that TVCA will be unable to release any payment to You until TVCA receives the signed Offer Letter from You in hard copy. TVCA reserves the right to rescind the offer of the Grant if TVCA does not receive a signed Offer Letter within fourteen (14) days of the date of this Offer Letter.

3. Effect of Acceptance of the Grant

In signing this Offer Letter, You confirm that:-

- a) You accept the Grant offered on the terms of the Agreement;
- b) You are duly authorised to accept the Grant by the Grant Recipient;
- c) You will comply in full with all obligations set out in the Agreement; and
- d) You understand that TVCA may withhold, or require repayment of, the Grant if You fail to comply in full with Your obligations as set out in the Agreement

IN WITNESS whereof the duly authorised representatives of the Parties have signed this Agreement as a Deed on day of 2025.

Sealed as a deed by affixing the common seal
of
**TEES VALLEY COMBINED
AUTHORITY** by

.....
Seal

.....
Authorised Signatory

Sealed as a deed by affixing the common seal
of **Middlesbrough Borough Council**

.....
Seal

.....
Authorised Signatory

PART 2 – FUNDING PARTICULARS

Project	PROJ-2371 Linthorpe Road Reinstatement Works
Grant Recipient	Middlesbrough Council
Grant Recipient's Address for Notice	PO BOX 504 Civic Centre, Middlesbrough TS1 9FY

Project Start Date	1 st July 2024																																
Additional Preconditions	Not Applicable																																
Agreed Purposes – Assets to be Funded	The Agreed Purposes is to remove the cycle lane infrastructure and reinstate the previous highway layout on Linthorpe Road (B1272) between Borough Road and Ayresome Street, including the reinstatement of 47 Pay & Display parking spaces.																																
Approved Use of Asset	The Approved Use of the Asset is to reinstate the previous highway layout on Linthorpe Road (B1272) between Borough Road and Ayresome Street including the reinstatement of 47 Pay & Display parking spaces. Such Pay & Display parking spaces shall remain the property of (and in the sole control of) the Grant Recipient.																																
Approved Use Period of Asset	Reflecting the appraisal period of 20 years, the highway layout where practical and feasible should remain as per proposed layout.																																
Key Milestones	<p>The Key Milestones are as follows:</p> <table border="1"> <thead> <tr> <th>Description</th> <th>Forecast Achievement Date</th> </tr> </thead> <tbody> <tr> <td>Civils Design Complete</td> <td>31st October 2024</td> </tr> <tr> <td>Traffic Signals Design Complete</td> <td>28th February 2025</td> </tr> <tr> <td>Approval to proceed with Permanent Traffic Regulation Orders (TRO's) by MBC Executive Committee</td> <td>5th February 2025</td> </tr> <tr> <td>Traffic Regulation Order Advertisement</td> <td>17th February 2025 – 31st March 2025</td> </tr> <tr> <td>Construction Contract Awarded</td> <td>31st May 2025</td> </tr> <tr> <td>Work Starts on Site</td> <td>31st May 2025</td> </tr> <tr> <td>Work Completes on Site</td> <td>31st December 2025</td> </tr> <tr> <td>Site Fully Open</td> <td>31st December 2025</td> </tr> <tr> <td>Financial Completion</td> <td>31st March 2026</td> </tr> <tr> <td>Year 1 Monitoring Report (defined below)</td> <td>31st March 2027</td> </tr> <tr> <td>Year 2 Monitoring Report (defined below)</td> <td>31st March 2028</td> </tr> <tr> <td>Year 3 Monitoring Report (defined below)</td> <td>31st March 2029</td> </tr> <tr> <td>Year 4 Monitoring Report (defined below)</td> <td>31st March 2030</td> </tr> <tr> <td>Year 5 Monitoring Report (defined below)</td> <td>31st March 2031</td> </tr> <tr> <td>Project Completion</td> <td>31st March 2031</td> </tr> </tbody> </table> <p>“Monitoring Report” is as defined and set out within Section 5.8 of the Business Case at Part 4 of this Agreement.</p>	Description	Forecast Achievement Date	Civils Design Complete	31 st October 2024	Traffic Signals Design Complete	28 th February 2025	Approval to proceed with Permanent Traffic Regulation Orders (TRO's) by MBC Executive Committee	5 th February 2025	Traffic Regulation Order Advertisement	17 th February 2025 – 31 st March 2025	Construction Contract Awarded	31 st May 2025	Work Starts on Site	31 st May 2025	Work Completes on Site	31 st December 2025	Site Fully Open	31 st December 2025	Financial Completion	31 st March 2026	Year 1 Monitoring Report (defined below)	31 st March 2027	Year 2 Monitoring Report (defined below)	31 st March 2028	Year 3 Monitoring Report (defined below)	31 st March 2029	Year 4 Monitoring Report (defined below)	31 st March 2030	Year 5 Monitoring Report (defined below)	31 st March 2031	Project Completion	31 st March 2031
Description	Forecast Achievement Date																																
Civils Design Complete	31 st October 2024																																
Traffic Signals Design Complete	28 th February 2025																																
Approval to proceed with Permanent Traffic Regulation Orders (TRO's) by MBC Executive Committee	5 th February 2025																																
Traffic Regulation Order Advertisement	17 th February 2025 – 31 st March 2025																																
Construction Contract Awarded	31 st May 2025																																
Work Starts on Site	31 st May 2025																																
Work Completes on Site	31 st December 2025																																
Site Fully Open	31 st December 2025																																
Financial Completion	31 st March 2026																																
Year 1 Monitoring Report (defined below)	31 st March 2027																																
Year 2 Monitoring Report (defined below)	31 st March 2028																																
Year 3 Monitoring Report (defined below)	31 st March 2029																																
Year 4 Monitoring Report (defined below)	31 st March 2030																																
Year 5 Monitoring Report (defined below)	31 st March 2031																																
Project Completion	31 st March 2031																																

Agreed Outputs	<p>The Agreed Outputs are as follows:</p> <table border="1" data-bbox="568 353 1385 548"> <thead> <tr> <th data-bbox="568 353 978 394">Description</th> <th data-bbox="978 353 1385 394">To be achieved</th> </tr> </thead> <tbody> <tr> <td data-bbox="568 394 978 465">Removal of existing cycle lanes on Linthorpe Road (km)</td> <td data-bbox="978 394 1385 465">1</td> </tr> <tr> <td data-bbox="568 465 978 548">Installation of Pay & Display parking spaces</td> <td data-bbox="978 465 1385 548">47</td> </tr> </tbody> </table>	Description	To be achieved	Removal of existing cycle lanes on Linthorpe Road (km)	1	Installation of Pay & Display parking spaces	47
Description	To be achieved						
Removal of existing cycle lanes on Linthorpe Road (km)	1						
Installation of Pay & Display parking spaces	47						
Amount of Grant	<p>A maximum sum of £2,169,699 (two million, one hundred and sixty-nine thousand, six hundred and ninety-nine pounds) subject to the terms and conditions of this Agreement.</p> <p>This amount includes a Quantitative Risk Assessment (QRA) value of £285,518 (two hundred and eighty-five thousand, five hundred and eighteen pounds) which can be drawn down by the Grant Recipient in line with the process as set out within the Business Case at Part 4 of this Agreement.</p> <p>As set out within the Business Case at Part 4 of this Agreement, approval of the draw down of the £285,518 (two hundred and eighty-five thousand, five hundred and eighteen pounds) QRA shall be subject to the approval of TVCA, at its absolute discretion.</p>						
Match Funding Required	<p>None</p>						
Funding Profile	<p>Unless otherwise agreed in writing and subject to the terms of this Agreement, payments will be made by TVCA to the Grant Recipient quarterly in arrears and be based upon actual expenditure incurred</p>						
Eligible Expenditure	<p>Eligible Expenditure incurred in respect of the Agreed Purposes, as per the following eligible expenditure headings:</p> <p>Construction costs £1,809,026 (One million, eight hundred and nine thousand and twenty-six pounds)</p> <p>Project Management Costs £75,155 (seventy-five thousand, one hundred and fifty-five pounds)</p> <p>Quantitative Risk Assessment (QRA) value £285,518 (two hundred and eighty-five thousand, five hundred and eighteen pounds) (Subject to the process set out within the Business Case at Part 4 of this Agreement).</p>						
VAT Position	<p>TVCA considers that all payments of Grant are outside of the scope of Value Added Tax, but where this is or becomes chargeable, all payments of Grant shall be deemed to be inclusive of Value Added Tax.</p>						
Agreed Completion Date	<p>31st March 2031</p>						
Claim Requirements	<p>As set out in clause 8 (Payment of Grant)</p>						

Acknowledgment or Publicity Requirements	See 'Additions to the Standard Funding Terms (if any)' below.
Commercially Sensitive Information	None
Monitoring Period	From the Project Start Date to 3 months after the Agreed Completion Date
Frequency and Format of Reports	As set out in Clause 12 (Monitoring & Reporting)
Amendments to the Standard Funding Terms (if any)	None
Additions to the Standard Funding Terms (if any)	<ol style="list-style-type: none"> 1. In addition to the public relations and marketing requirements set out in the Agreement, the Grant Recipient shall ensure that all site hoardings, billboards, websites and communications it releases relating to the Project in whatever format will be in full compliance with the Government Communication Service guidelines at Branding - GCS (as updated from time to time) (Guidelines). Any such logo and/or statements required by the Guidelines will be given equal prominence to the Grant Recipient's own branding and that of TVCA. 2. The Grant Recipient shall use all reasonable endeavours to assist TVCA with any reasonable request TVCA makes to the Grant Recipient to meet its obligations to the Department for Transport in respect of the Project. 3. The Grant Recipient shall provide a quarterly cash-flow showing eligible expenditure to TVCA prior to submission of the first quarterly claim.

PART 3 – TVCA STANDARD TERMS AND CONDITIONS OF FUNDING

1. DEFINITIONS AND INTERPRETATION

In this Agreement, unless the context otherwise requires, expressions shall have the following meanings set out in the Appendix to this Part 3 (TVCA Standard Terms and Conditions of Funding).

2. COMMENCEMENT AND TERM

This Agreement shall commence on the Project Start Date and shall continue for the Term, unless terminated in accordance with Clause 16 (Events of Default).

3. CONDITIONS PRECEDENT

3.1 TVCA will not make the first payment of Grant and/or any subsequent payment of Grant unless all of the Preconditions have been complied with, in addition to any Preconditions listed in the Contract Particulars:-

3.1.1 the Grant Recipient has satisfied TVCA that the Grant Recipient has sufficient Match Funding committed to achieve Completion of the Project by the Agreed Completion Date.

3.1.2 the Grant Recipient has satisfied TVCA that the Grant Recipient has all funding needed to pay for expenditure in relation to the Project which is Not Eligible Expenditure.

3.1.3 any Preliminary Expenditure, which is Eligible Expenditure, has been used in accordance with any conditions relating to it.

3.1.4 a Certificate of Title in respect of the ownership of the Site and the Grant Recipient's estate or interest therein (being either the freehold estate or a Long Lease) has been issued to TVCA by a firm of solicitors acceptable to TVCA; and

3.1.5 the Grant Recipient shall have supplied to TVCA and TVCA shall have approved the Project Specifications.

3.2 Notwithstanding clause 3.1, TVCA may waive in whole or in part any Precondition without prejudicing TVCA's right to require subsequent fulfilment of such Precondition provided always that the payment of a Grant Claim shall not operate as a waiver of any of the obligations in this clause 3 or exclude the right for TVCA to exercise any of its rights under this Agreement.

4. OBLIGATIONS OF TVCA

4.1 Subject to clause 4.2, TVCA shall make the Grant to the Grant Recipient in accordance with the Funding Profile.

4.2 TVCA's obligation to make the Grant is conditional on the Grant Recipient:-

4.2.1 having all necessary Consents to proceed with the Project.

4.2.2 subject to clause 3.2, having met all Preconditions in full.

- 4.2.3 satisfying TVCA that the Grant Recipient is meeting, and can continue to meet, its obligations as set out in clause 5 (Obligations of the Grant Recipient); and
- 4.2.4 satisfying TVCA that none of the circumstances described in clause 16 (Events of Default) have occurred.

4.3 For the avoidance of doubt, TVCA shall have no obligation to provide any further funding in relation to the Project, or any future project, other than as provided for specifically in this Agreement.

5. **OBLIGATIONS OF THE GRANT RECIPIENT**

5.1 The Grant Recipient agrees with TVCA that it will:-

- 5.1.1 progress the Project in an efficient and business-like manner.
- 5.1.2 not make any material alteration to the Project unless TVCA agrees otherwise in writing.
- 5.1.3 apply the Grant exclusively for the Agreed Purpose and in relation to Eligible Expenditure only.
- 5.1.4 deliver the Agreed Outputs.
- 5.1.5 meet the Key Milestones.
- 5.1.6 comply with the following at all times throughout the Project:-
 - a) the terms of any Consents.
 - b) all applicable Law including all requirements pursuant to the Public Contracts Regulations 2015 and Subsidy Control Law; and
 - c) the terms and conditions of this Agreement, including but not limited to:-
 - i. the monitoring and reporting requirements set out in clause 12 (Monitoring and Reporting);
 - ii. the intellectual property requirements set out in clause 13 (Intellectual Property); and
 - iii. the public relations and marketing requirements set out in clause 14 (Public Relations and Marketing);
- 5.1.7 not charge, assign, transfer or otherwise dispose of the benefit of this Agreement;
- 5.1.8 provide TVCA with such information as TVCA may reasonably require in connection with the Project;
- 5.1.9 submit Claim Forms with full supporting evidence in accordance with the Funding Profile (pursuant to clause 8 (Payment of Grant));

- 5.1.10 secure all other sources of funding required to deliver the Project as identified in the Application Form including any Match Funding; and
 - 5.1.11 meet all cost overrun(s) associated with the Project.
- 5.2 The Grant Recipient further agrees with TVCA that it shall as soon as reasonably practicable inform TVCA in writing of:-
 - 5.2.1 any Change; and/or
 - 5.2.2 any event which might adversely affect the carrying out and completion of the Project or any part of it.
- 5.3 The Grant Recipient may, with the consent of TVCA, task a Delivery Partner to carry out certain of the Project Activities provided that such shall not in any way reduce any of the obligations or liability of the Grant Recipient under this Funding Agreement. If Consent is given, the Delivery Partner shall be named in the Funding Particulars.
- 5.4 For the avoidance of doubt, the Grant Recipient shall be liable for the acts of its Delivery Partner including any Corrections resulting therefrom.
- 5.5 The Grant Recipient shall not use the Grant to:-
 - 5.5.1 make any payment to members of its Governing Body; or
 - 5.5.2 pay for any expenditure commitments of the Grant Recipient entered into before the Project Start Date,unless this has been approved in writing by TVCA.
- 5.6 The Grant Recipient shall not spend any part of the Grant on the delivery of the Project outside of the Approved Use Period.
- 5.7 Should any part of the Grant remain unspent after the expiration of the Approved Use Period, the Grant Recipient shall ensure that any unspent monies are returned to TVCA unless otherwise agreed in writing by TVCA.
- 5.8 Any liabilities arising after the Agreed Completion Date must be managed and paid for by the Grant Recipient using the Grant or other resources of the Grant Recipient. There will be no additional funding available from TVCA for this or any other purpose.
- 6. **PROJECT WORKS**
- 6.1 The Grant Recipient agrees with TVCA that:-
 - 6.1.1 it shall not carry out any Project Activities upon the Site without having obtained all necessary Consents for such activities, including traffic regulation orders in line with the Traffic Regulation Act 1984. All works to be delivered as permitted development consistent with the Town and Country Planning Act 1990;

- 6.1.2 it shall permit TVCA and its authorised representatives to attend any Project meetings including any Site meetings with the Grant Recipient's building contractor;
- 6.1.3 it shall permit TVCA and its authorised representatives to visit the Site upon reasonable notice to view the Works throughout the lifetime of the Project; and
- 6.1.4 it shall use all reasonable endeavours to ensure that the construction and operation of the Project reflect the overall principles of Sustainable Development.

7. WARRANTIES

7.1 The Grant Recipient represents and warrants to TVCA that:-

- 7.1.1 the execution of the Offer Letter has been validly authorised and the obligations assumed by the Grant Recipient constitute valid, legal, binding and enforceable obligations;
- 7.1.2 the Grant Recipient is not at the date of this Agreement aware of any:-
 - a) actual or potential litigation or administrative or arbitration proceeding which might have a material adverse effect on its business, assets or operations or might adversely affect its ability to perform its obligations under this Agreement; or
 - b) circumstances which may result in the bankruptcy or insolvency of the Grant Recipient, or the Grant Recipient making any composition with its creditors;
- 7.1.3 the information provided in the Application Form is true, complete and accurate;
- 7.1.4 it has disclosed to TVCA all information that might reasonably be thought to have influenced TVCA in awarding the Grant to the Grant Recipient; and
- 7.1.5 it is not aware of anything which may materially threaten the success of the Project.

8. PAYMENT OF GRANT

- 8.1 Subject to the Grant Recipient complying with the Claim Requirements, TVCA shall pay the Grant to the Grant Recipient on the Payment Date(s) and in the amounts set out in the Funding Profile.
- 8.2 The Grant Recipient shall make all Claims no less than fourteen (14) days in advance of each Payment Date by submitting the Claim Form to TVCA.
- 8.3 The Grant Recipient must notify TVCA promptly if at any time it becomes aware that it is unable to make a Claim in accordance with the Funding Profile.
- 8.4 Each Claim shall include accounting documents capable of being independently verified in such format and detail as may be acceptable to TVCA.

- 8.5 TVCA will aim to defray the relevant Grant within twenty (20) Working Days of receipt of a Claim, subject to the Grant Recipient satisfactorily meeting any request for further information or evidence. The time for payment of the Claim shall not be of the essence. TVCA shall have no liability to the Grant Recipient for any Losses caused by a delay in the payment of a Claim howsoever arising.
- 8.6 A progress report in respect of the Project must be submitted with each Claim in accordance with clause 12 (Monitoring and Reporting).
- 8.7 By submitting a Claim, the Grant Recipient warrants that:-
- 8.7.1 there is no Event of Default or material breach subsisting by reference to the facts and circumstances existing on each such date; and
 - 8.7.2 if it is not aware of anything which materially threatens the success or successful completion of the intention or purpose of this Funding Agreement.
- 8.8 TVCA shall apply the approach to Value Added Tax in relation to the Grant as set out in the Contract Particulars.
- 8.9 If in any Financial Year (the relevant year) there is a shortfall in the amount of Eligible Expenditure by reference to the amount planned in the Funding Profile, TVCA will be under no obligation to pay Grant for any additional Eligible Expenditure in the following year or any later Financial Year.
- 8.10 If in any Financial Year (the relevant year) there is an overspend in the amount of Eligible Expenditure by reference to the amount planned in the Funding Profile, TVCA will be under no obligation to pay Grant in the following year or any later Financial Year which was overspent in any relevant year.
- 8.11 The Grant shall be paid into a separate bank account in the name of the Grant Recipient which must be an ordinary business bank account. All cheques from the bank account must be signed by at least two individual representatives of the Grant Recipient.
- 8.12 The Grant Recipient shall not transfer any part of the Grant to bank accounts which are not ordinary business accounts within the clearing bank system, without the prior written consent of TVCA.
- 8.13 The Grant shall be shown in the Grant Recipient's accounts as a restricted fund and shall not be included under general funds.
- 8.14 The Grant Recipient shall keep separate, accurate and up to date accounts and records of the receipt and expenditure of the Grant monies received by it.
- 8.15 The Grant Recipient shall keep all invoices, receipts, and accounts and any other relevant documents relating to the expenditure of the Grant for a period of at least six (6) years following receipt of any Grant monies to which they relate. TVCA shall have the right to review, at the TVCA's reasonable request, the Grant Recipient's accounts and records that relate to the expenditure of the Grant and shall have the right to take copies of such accounts and records.

8.16 The Grant Recipient shall provide TVCA with a copy of its annual accounts within six (6) months (or such lesser period as TVCA may reasonably require) of the end of the relevant Financial Year in respect of each year in which the Grant is paid.

8.17 The Grant Recipient shall comply and facilitate TVCA's compliance with all statutory requirements as regards accounts, audit or examination of accounts, annual reports and annual returns applicable to itself and TVCA.

9. **RETENTION OF GRANT**

9.1 Without prejudice to any other provision of this Funding Agreement TVCA will retain 5% of the Grant which shall not be released unless and until the following events have occurred:-

9.1.1 financial completion of the Project;

9.1.2 TVCA has received, and is satisfied with, the final monitoring report following the final monitoring visit;

9.1.3 all audit issues outstanding are resolved; and

9.1.4 provided always that the release of the retention shall not operate as an acknowledgement or waiver and shall not preclude TVCA from exercising any of its rights under this Funding Agreement.

10. **CORRECTIONS**

10.1 Notwithstanding any other provision in this Agreement, TVCA may impose a Correction. If a Correction is imposed, a notice will be sent to the Grant Recipient setting out the Irregularity that TVCA considers has occurred together with the level of Correction imposed having regard to the Irregularity and/or the value of the Grant Claim to the extent that the Irregularity applies to it.

10.2 If a Correction is imposed, the Grant Recipient shall either pay the amount or agree to the Correction being offset from a future Grant Claim as the case may be. TVCA shall be at liberty to offset an amount of Grant in anticipation of a Correction pending the final outcome of any discussions or representations made by TVCA and/or the Grant Recipient in respect of the Correction.

10.3 The Grant Recipient shall be at liberty to make representations in writing to TVCA setting out the reasons it considers that the Correction should be adjusted together with evidence in sufficient detail to enable TVCA to reconsider the requirement for the Correction provided always that TVCA's decision shall be final and binding.

11. **PROVISIONS RELATING TO ASSETS**

Inventory of the Assets

11.1 Using the Asset Register Template, the Grant Recipient must establish and maintain an inventory of all of the Assets.

11.2 The Grant Recipient covenants with TVCA that it will not use any Asset for any purpose other than the Approved Use throughout the Approved Use Period of the Asset.

Change of use of any Asset

- 11.3 During the Approved Use Period of each Asset, the Grant Recipient shall not cease to use the Fixed Asset or the Major Asset, or any part of the Asset for the Approved Use without the prior written consent of TVCA. If consent is given, it may be subject to any conditions which TVCA may wish to impose. Such conditions may include repayment by the Grant Recipient to TVCA of all or part of the Grant paid to the Grant Recipient under this Funding Agreement.

Disposal of any Asset

- 11.4 The conditions of this clause shall apply throughout the Approved Use Period of any Asset:-

11.4.1 in respect of any Planned Disposal, the Grant Recipient need not seek the prior written consent of TVCA to such Planned Disposal, provided the Grant Recipient can demonstrate that the disposal meets the definition of a Planned Disposal as agreed in accordance with this Funding Agreement;

11.4.2 further to clause 11.4.1 above, where the Grant Recipient intends to make a Planned Disposal the Grant Recipient shall notify TVCA in good time prior to such Planned Disposal, supplying evidence that such disposal meets the definition of a Planned Disposal as set out herein, and TVCA shall promptly supply to the Grant Recipient a letter addressed to the relevant Land Registry consenting to the disposal;

11.4.3 the Grant Recipient must not Dispose of any interest in any Asset without the prior written consent of TVCA. If TVCA grants consent to a Disposal, such consent may be subject to satisfaction of certain conditions, to be determined by TVCA;

11.4.4 where the Grant Recipient Disposes of any interest in any Asset without the prior written consent of TVCA, then the proceeds of the Disposal (limited to the total amount paid by TVCA to the Grant Recipient under this Funding Agreement) shall be held on trust by the Grant Recipient for the benefit of TVCA;

11.4.5 the liability under clause 11.4.4 is separate from the liability to comply with any decision of TVCA under clause to require repayment of the whole or any part of the amount paid of the Grant, to the Grant Recipient; and

11.4.6 the Grant Recipient shall provide to TVCA as part of the progress report referred to in clause 12.1 information in relation to any Disposals which it either intends to make or has made with the prior written consent of TVCA at the date of such progress reports.

12. MONITORING AND REPORTING

- 12.1 As part of each Claim, the Grant Recipient shall provide a report including the following information:-

12.1.1 a financial summary which identifies:-

- a) the amount of the Grant spent since the last Payment Date (if any);

- b) how the Grant identified at (a) above was spent;
 - c) confirmation that each item of expenditure identified above at (b) above is Eligible Expenditure; and
 - d) any remaining balance of the Grant held by the Grant Recipient;
- 12.1.2 full evidence of the expenditure itemised at 12.1.1a) above
 - 12.1.3 an update of progress against Key Milestones and confirmation of any Key Milestones which have not been met; and
 - 12.1.4 an update of progress against delivering Agreed Outputs and confirmation of any Agreed Outputs which have not been delivered.
- 12.2 In addition to the requirements set out in clause 12.1 above, the Grant Recipient shall (to the extent required):-
- 12.2.1 provide TVCA with financial and operational reports in relation to the Project on the frequency and in the format specified in the Funding Particulars and as further notified by TVCA from time to time;
 - 12.2.2 provide TVCA with such further information as TVCA may reasonably require in order for TVCA to establish that the Grant has been used properly in accordance with this Agreement within fourteen (14) days; and
 - 12.2.3 on reasonable written request, permit any person authorised by TVCA such reasonable access to its employees, agents, facilities and records, for the purpose of evaluating the Grant Recipient's compliance with this Agreement within fourteen (14) days.
- 12.3 For the duration of the Approved Use Period, the Grant Recipient shall inform TVCA immediately in relation to any Disposal:-
- 12.3.1 which it intends to make seeking the prior written consent of TVCA; and
 - 12.3.2 which it has made with the prior written consent of TVCA.
- 12.4 The Grant Recipient shall provide TVCA with a final report within fourteen (14) days of the Agreed Completion Date which shall confirm whether the Project has been successfully and properly completed.
13. **INTELLECTUAL PROPERTY**
- 13.1 The Grant Recipient agrees:-
- 13.1.1 to provide TVCA with digital images in electronic format which accurately demonstrate the Project (as reasonably requested by TVCA from time to time), along with the right to use those images at any time including the right to publish and to alter the images and to share them with relevant third parties;

- 13.1.2 to hereby grant an irrevocable, perpetual and royalty-free licence to TVCA to use, copy, keep and disseminate the Digital Outputs;
- 13.1.3 obtain and maintain in force all authorisations of any kind the Grant Recipient requires to use, copy, keep and disseminate the Digital Outputs and to grant the licence in clause 13.1.2 to TVCA;
- 13.1.4 to contract to the effect that any creation made on its behalf of material which forms Digital Output is undertaken on terms which enable it to grant the licence at clause 13.1.2 to TVCA; and
- 13.1.5 ensure that Digital Outputs are held securely and are available on request by TVCA.

14. **PUBLIC RELATIONS AND MARKETING**

14.1 The Grant Recipient agrees:-

- 14.1.1 to include the statement, 'This Project is funded by Tees Valley Combined Authority' and the Logo on or in:-
 - all Site hoardings and billboards;
 - on all communications it releases relating to the Project in whatever format;
 - where the Grant is made in relation to a Fixed Asset, on a sign in a prominent position to be agreed by TVCA;and that such statement and Logo will be given equal prominence to the Grant Recipient's own branding;
- 14.1.2 that TVCA has the right to publicise the Grant and the Project;
- 14.1.3 not to publish or display any item bearing the Logo or the name of TVCA without prior TVCA approval (not to be unreasonably withheld);
- 14.1.4 to give TVCA as much advance notice as possible of announcements and milestone events (at least four (4) weeks) to allow quotes to be arranged for any media releases from the TVCA and/or HM Government as TVCA considers this appropriate;
- 14.1.5 not to issue any public statement, press release or other publicity in relation to the Project without
 - a) prior TVCA approval (not to be unreasonably withheld); and
 - b) giving TVCA and/or HM Government (where TVCA considers this appropriate) the opportunity to include a quote or comment in the statement; and

14.1.6 to meet any additional acknowledgment or publicity requirements specified in the Funding Particulars or notified by TVCA from time to time, including the requirement to use the Logo in certain contexts.

14.2 TVCA grants to the Grant Recipient a non-exclusive, non-assignable licence to use the Logo where authorised or required to do so pursuant to this clause 14 (Public Relations and Marketing) only.

15. **SUBSIDY CONTROL**

15.1 The Grant Recipient confirms it has undertaken its own independent assessment of the compatibility of the Project with Subsidy Control Law and confirms to TVCA that the Project is structured so it is compliant with Subsidy Control Law.

15.2 Where TVCA has provided its views on any aspect of Subsidy Control Law, the Grant Recipient confirms that it has considered this information alongside all other sources of Subsidy Control Law available at the time of entering into this Agreement in undertaking its own assessment of the Project's compliance. TVCA has taken into account the Grant Recipient's representations on Subsidy Control Law compliance in deciding to offer the Grant.

15.3 The Grant Recipient agrees to:-

15.3.1 procure and maintain the necessary expertise and resources to deliver the Project in accordance with Subsidy Control Law for the full term of the Project; and

15.3.2 to maintain appropriate records of compliance with Subsidy Control Law and agrees to take all reasonable steps to assist TVCA to respond to any investigation(s) instigated by a regulator of UK Subsidy Control Law into the Project.

16. **EVENTS OF DEFAULT**

16.1 TVCA may at its absolute discretion:-

16.1.1 reduce, suspend or withhold the Grant or any part of it; and/or

16.1.2 require that all or part of the Grant be repaid, and/or

16.1.3 may terminate this Agreement, or any part of it, on written notice with immediate effect;

on the occurrence of any of the Events of Default listed in clause 16.2.

16.2 The Events of Default are:-

16.2.1 the Grant Recipient:-

a) is in breach of a material term of this Agreement which (in the reasonable opinion of TVCA) is not capable of remedy;

- b) is in breach of a material term of this Agreement which is (in the reasonable opinion of TVCA) is capable of remedy and the Grant Recipient has failed to remedy the breach within fourteen (14) days of receipt of written notice from TVCA specifying the breach and requesting that it be remedied;
- c) fails to commence the Project Activities by the date which is three (3) months after the date of this Agreement;
- d) does not claim the expenditure in line with the Expenditure Profile;
- e) does not complete the Project Activities by the Agreed Completion Date;
- f) fails to meet any Key Milestone or to deliver any Agreed Output;
- g) becomes bankrupt or insolvent, makes any composition with its creditors, or has a receiver appointed under the Mental Health Act 1983;
- h) dies or becomes unable to perform their obligations under this Agreement due to incapacity for any consecutive or aggregate period of four (4) weeks in any period of twelve (12) calendar months;
- i) ceases or threatens to cease to carry on its business;
- j) is guilty of any conduct bringing itself or TVCA into serious public disrepute, including but not limited to, a breach of clause 25 (Prevention of Bribery); or
- k) in the opinion of TVCA (acting reasonably) commits any act of dishonesty or serious or persistent misconduct in connection with or referable to the Project or this Agreement (including a material breach of health and safety legislation or of the Public Contracts Regulations 2015); or

16.2.2 any Consent necessary to enable the Grant Recipient to deliver the Project is not obtained or, having been obtained, is revoked, withdrawn, suspended, modified or made subject to conditions;

16.2.3 the Approved Use of any Asset has ceased without the prior written consent of the TVCA;

16.2.4 any of the warranties given or representations made by the Grant Recipient as set out in clause 7 (Warranties) and clause 25.2 (Prevention of Bribery) prove to be untrue or incorrect;

16.2.5 a Change occurs without the prior written approval of the TVCA;

16.2.6 any event or circumstance which (in the reasonable opinion of TVCA) might materially adversely affect:-

- a) the carrying out and completion of the Project or any part of it, or
- b) the Grant Recipient's ability to achieve the Agreed Outputs; or

c) the Grant Recipient's ability to meet the Key Milestones; or

16.2.7 a finding of non-compliance with Subsidy Control Law is made in respect of the Project by a Court of competent jurisdiction (in which case, the Grant Recipient acknowledges and agrees to repay the Grant with interest in accordance with the applicable reference rates on request).

17. DISPUTE RESOLUTION PROCEDURE

17.1 Any dispute arising in relation to any aspect of this Agreement shall be resolved in accordance with this clause 17.

17.2 The Parties shall endeavour to:-

17.2.1 notify each other of any anticipated disputes so that any potential dispute can be avoided by negotiation between them; and

17.2.2 resolve any failure to agree matters or any disputes by direct negotiations between senior representatives of the Parties.

17.3 Where direct negotiations have failed to resolve the dispute, either Party may refer the matter to an adjudicator ("the Adjudicator"). The Adjudicator shall be selected by the President for the time being of the Chartered Institute of Arbitrators unless the Parties in dispute agree another appropriate person.

17.4 The Adjudicator shall be deemed not to be an arbitrator but shall render his decision as an expert, and the provisions of the Arbitration Act 1996 and the law relating to arbitration shall not apply to the Adjudicator or their determination or the procedure used to reach the determination.

17.5 The Grant Recipient shall continue to comply with, observe and perform all its obligations hereunder regardless of the nature of the dispute and notwithstanding the referral of the dispute for resolution under this clause 17.

17.6 The Parties shall give effect forthwith to every decision of the Adjudicator delivered under this clause 17.

18. SURVIVAL

18.1 The following clauses will survive termination or expiry of this Agreement:-

18.1.1 clause 5 (Obligations of the Grant Recipient) to the extent the obligations continue to be capable of being met;

18.1.2 clause 6 (Project Works);

18.1.3 clause 10 (Corrections);

18.1.4 clause 11 (Provisions Relating to Assets);

18.1.5 clause 12 (Monitoring and Reporting);

- 18.1.6 clause 13 (Intellectual Property);
- 18.1.7 clause 14 (Public Relations and Marketing);
- 18.1.8 clause **Error! Reference source not found.** (Subsidy Control);
- 18.1.9 clause 16 (Events Of Default) in relation to the right to require repayment of the Grant only;
- 18.1.10 clause 20 (Freedom of Information and Data Protection);
- 18.1.11 clause 21 (Confidentiality);
- 18.1.12 clause 22 (Limitation of Liability);
- 18.1.13 clause 23 (insurance);
- 18.1.14 clause 25 (Prevention of Bribery);
- 18.1.15 clause 28 (Severance); and
- 18.1.16 clause 31 (Law and Jurisdiction).

19. **VARIATION**

- 19.1 No variation or modification to this Agreement is valid unless it is in writing and signed by each of the Parties.

20. **FREEDOM OF INFORMATION AND DATA PROTECTION**

- 20.1 The Grant Recipient acknowledges that TVCA is required to comply with the provisions of the Freedom of Information Act 2000 and the Environmental Information Regulations 2004.
- 20.2 The Grant Recipient agrees to assist and co-operate with TVCA as required by TVCA to enable TVCA to comply with its information disclosure obligations.
- 20.3 The Grant Recipient acknowledges that the Commercially Sensitive Information is indicative only and that TVCA may be obliged to disclose it to comply with its information disclosure obligations.
- 20.4 Except where the Grant Recipient is itself required to comply with the provisions of the Freedom of Information Act 2000 and the Environmental Information Regulations 2004, the Grant Recipient will not answer any request for information itself without the prior written consent of TVCA but shall pass these on to TVCA at the earliest opportunity for TVCA to respond to.
- 20.5 Where the Grant Recipient is required to comply with the provisions of the Freedom of Information Act 2000 and the Environmental Information Regulations 2004, the Grant Recipient shall use best endeavours to consult with TVCA before responding to any request for information directly relating to the Project.

TEES VALLEY

20.6 The Grant Recipient shall (and shall procure that any of its staff involved in connection with the Project shall) comply with all applicable requirements of the Data Protection Legislation which arise in connection with this Agreement.

21. CONFIDENTIALITY

21.1 Subject to clause 20 (Freedom of Information and Data Protection), each Party shall during the Approved Use Period and thereafter keep secret and confidential all intellectual property rights (as per clause 13) or other business, technical or commercial information (including the Commercially Sensitive Information) disclosed to it as a result of the Agreement and shall not disclose the same to any person save to the extent necessary to perform its obligations in accordance with the terms of this Agreement or save as expressly authorised in writing by the other Party.

21.2 The obligation of confidentiality contained in this clause shall not apply or shall cease to apply to any intellectual property rights, or other business, technical or commercial information which:-

21.1.1 at the time of its disclosure by the disclosing Party is already in the public domain or which subsequently enters the public domain other than by breach of the terms of this Agreement by the receiving Party; or

21.1.2 is already known to the receiving Party as evidenced by written records at the time of its disclosure by the disclosing Party and was not otherwise acquired by the receiving Party from the disclosing Party under any obligations of confidence; or

21.1.3 is at any time after the date of this Agreement acquired by the receiving Party from a third party having the right to disclose the same to the receiving Party without breach of the obligations owed by that Party to the disclosing Party.

22. LIMITATION OF LIABILITY

22.1 TVCA accepts no liability for any consequences, whether direct or indirect, that may come about from the Grant Recipient running the Project, the use of the Grant or from withdrawal of the Grant. The Grant Recipient shall indemnify and hold harmless TVCA, its employees, agents, officers or Subcontractors with respect to all claims, demands, actions, costs, expenses, losses, damages and all other liabilities arising from or incurred by reason of the actions and/or omissions of the Grant Recipient in relation to the Project, the non-fulfilment of obligations of the Grant Recipient under this Agreement or its obligations to third parties.

22.2 Subject to clause 22.1, TVCA's liability under this Agreement is limited to the payment of the Grant

23. INSURANCE

23.1 The Grant Recipient shall affect and maintain with a reputable insurance company a policy or policies in respect of all risks which may be incurred by the Grant Recipient, arising out of the Grant Recipient's performance of the Agreement, including death or personal injury, loss of or damage to property (including any Assets) or any other loss (the "Required Insurances").

TEES VALLEY

23.2 The Required Insurances referred to above include (but are not limited to):-

23.2.1 public liability insurance with a limit of indemnity of not less than ten million pounds (£10,000,000) in relation to any one claim or series of claims arising from the Project; and

23.2.2 employer's liability insurance with a limit of indemnity of not less than five million pounds (£5,000,000) in relation to any one claim or series of claims arising from the Project.

23.3 The Grant Recipient shall (on request) supply to TVCA a copy of such insurance policies and evidence that the relevant premiums have been paid.

24. **NOTICES**

24.1 Any notices required to be given by either Party to the other shall be in writing and shall be served personally or sent by registered post to the address stated in this Agreement for the relevant Party.

24.2 Any notice served will be deemed to have been served as follows:-

24.2.1 notice served personally will be deemed to have been served on the day of delivery; and

24.2.2 notice sent by post will be deemed to have been served forty-eight (48) hours after it was posted.

25. **PREVENTION OF BRIBERY**

25.1 The Grant Recipient shall not, and shall procure that any employee, agent or representative of the Grant Recipient shall not, in connection with this Agreement or the Project, commit a Prohibited Act.

25.2 The Grant Recipient warrants, represents and undertakes that it is not aware of any financial or other advantage being given or promised to be given to any person working for or engaged by TVCA.

26. **THIRD PARTY RIGHTS**

26.1 This Agreement is enforceable only by the Parties to it, by their successors in title and permitted assignees. Any rights of any person to enforce the terms of this Agreement pursuant to The Contracts (Rights of Third Parties) Act 1999 are excluded.

27. **NO WAIVER**

27.1 Failure by any Party at any time to enforce any one or more of the provisions of this Agreement or to require performance by another Party of any of the provisions shall not constitute or be construed as a waiver of the provision or of the right at any time subsequently to enforce all terms and conditions of this Agreement nor affect the validity of this Agreement or any part of it or the right of the Parties to enforce any provision in accordance with its terms.

28. SEVERANCE

If any provision of this Agreement shall become or shall be declared by any court of competent jurisdiction to be invalid or unenforceable in any way, such invalidity shall not impair or affect any other provision all of which shall remain in full force and effect.

29. COSTS AND EXPENSES

29.1 The Parties have agreed to share the costs incurred by TVCA in ensuring compliance with its Assurance Framework in the proportions set out in the Funding Particulars. The Grant Recipient notes that its share of these costs are to be deducted from the Grant as specified in the Funding Profile.

29.2 Each Party shall pay their own legal and administrative costs and expenses incurred in connection with the negotiation, preparation, execution, completion and implementation of this Agreement.

30. NO AGENCY OR PARTNERSHIP

Nothing contained in this Agreement, and no action taken by the Parties pursuant to this Agreement, will be deemed to constitute a formal partnership, joint venture, principal and agent or employer and employee. No Party has, nor may it represent that it has, any authority to act or make any commitments on any other Party's behalf.

31. LAW AND JURISDICTION

This Agreement shall be governed by the laws of England and shall be subject to the exclusive jurisdiction of the English Courts.

APPENDIX 1 to Part 3 – Definitions and Interpretation

1. In this Agreement, unless the context otherwise requires:-

- 1.1. references to any individual includes his or her personal representatives and successors by law and references to 'TVCA' include its permitted assigns;
- 1.2. reference to any statute or statutory provision includes a reference to that statute or provision as from time to time amended, extended, re-enacted or consolidated and all statutory instruments or orders made pursuant to it;
- 1.3. words denoting the singular shall include the plural and vice versa, words denoting any gender include all genders, and words denoting persons shall include firms and corporations and vice versa; and
- 1.4. references to clauses and clause headings are for convenience only; and
- 1.5. the following expressions shall have the following meanings:-

“Agreed Completion Date” means the date specified in the Funding Particulars or such other date as may be agreed in writing by TVCA for the completion of the Project;

“Agreed Purpose” means the purpose for which the Grant may be used by the Grant Recipient as specified in the Funding Particulars;

“Agreed Outputs” means the outputs or deliverables that the Grant Recipient has agreed to deliver as part of the Project as specified in the Funding Particulars;

“Agreement” means the documents setting out TVCA’s agreement to provide (and the Grant Recipient’s agreement to accept) the Grant, consisting of the following documents which are to be read as one document:-

- a) Part 1 – the Offer Letter;
- b) Part 2 – the Funding Particulars;
- c) Part 3 – the Standard Funding Terms; and
- d) Part 4 – the Application Form;

and in the event of conflict between any of the Parts of the Agreement, the Parts shall have the order of precedence set out above;

“Application Form” means the application form submitted by the Grant Recipient to TVCA attached at Part 4 of this Agreement which sets out the purposes for which the Grant was sought;

“Approved Use”	means approved use of the Asset as set out in the Funding Particulars;
“Approved Use Period”	means the period set out in the Funding Particulars;
“Asset”	means any asset being funded in whole or in part by the Grant as described in the Application Form which may be either a Fixed Asset or a Major Asset as appropriate in the relevant context and Assets shall be construed accordingly;
“Asset Register Template”	means the asset register template notified by TVCA to the Grant Recipient;
“Audit”	means an inspection and verification of the accounts conducted by an independent qualified accountant;
“Certificate of Title”	means a certificate of title evidencing the Grant Recipient's title to the Site of the Works in the form required and notified by TVCA;
“Change”	means in relation to the Project as defined in the Application any of the following changes:- <ul style="list-style-type: none">a) a change in the Ownership, Control and Nature of Business of the Grant Recipient;b) a change in the nature or purpose of the Project;c) any change to the Eligible Expenditure;d) any change to the Expenditure Profile;e) any change to any of the Key Milestone Dates;f) any change to any of the Agreed Outputs; org) any material change to the Project Specifications (that is, more than merely trivial and inconsequential in its consequences);
“Claim”	means the submission of a Claim Form by the Grant Recipient to TVCA at the intervals agreed in the Funding Profile;
“Claim Form”	means the form provided to the Grant Recipient by TVCA to request draw down of the Grant;
“Claim Requirements”	means the requirements which trigger each payment of the Grant as set out in clause 8 (Payment of Grant) and the Funding Particulars (where relevant) or as subsequently notified to the Grant Recipient;

“Commercially Sensitive Information”	any information identified by the Grant Recipient in the Funding Particulars as being of a commercially sensitive nature, which, if disclosed by TVCA would cause the Grant Recipient significant commercial disadvantage or material financial loss;
“Completion”	means completion of the Project Activities to the satisfaction of the TVCA;
“Conditions”	means the terms and conditions upon which the Grant is payable as contained in clause 8 of the main body of this Capital Funding Agreement;
“Consents”	includes any approval, consent, exemption, licence, permission or registration by or from any governmental or other authority or other person to enable the Project to be lawfully commenced, carried out and/or maintained;
“Control”	control as defined by section 416 of the Income and Corporation Taxes Act 1988;
“Corrections”	means an amount to be repaid or withheld from the Grant payable as determined following the finding of an Irregularity in accordance with clause 10;
“Data Protection Legislation”	means all applicable data protection and privacy legislation in force from time to time in the UK including the UK GDPR, the Data Protection Act 2018, (and regulations made thereunder), and the Privacy and Electronic Communications Regulations 2003 (SI 2003/2426) as amended;
“Delivery Partner”	means a third party that (in collaboration with the Grant Recipient) shall deliver the Project, itself defray expenditure to be claimed and carry out the Project Activities and which third party is not intended by either Party to be providing services or works under a contract for services or works;
“Digital Outputs”	all material created in or copied into digital format by or for the Grant Recipient in connection with the Project, including all advice, reports, studies and other documentation prepared or commissioned by the Grant Recipient in connection with the Project;
“Disposal”	means the disposal, sale, transfer or the grant of any estate or interest in any Asset including a Lease Disposal, where relevant in the context, and includes the creation of any charge, legal mortgage, debenture, lien or other encumbrance and any contract for a disposal and ‘Dispose’ shall be construed accordingly;
“Eligible Expenditure”	means the costs relating to the Project that the Grant can be used to fund as set out in the Funding Particulars;
“Event of Default”	of means any of the events described in clause 16.2;

“Expenditure Profile”	means the information provided by the Grant Recipient detailing the spend forecast for the carrying out of the Project Activities and defrayal of expenditure from the Project Start Date to the Agreed Completion Date as set out in the Funding Particulars;
“Financial Year”	means 1 April to 31 March;
“Fixed Asset”	means any Asset which consists of land and/or buildings, plant, machinery or other thing that is, or is to be, fixed to any land and which is to be acquired, developed, enhanced, constructed and/or installed as part of the Project;
“Funding Particulars”	means the document detailing the specific core terms agreed between the Grant Recipient and TVCA in respect of the provision of the Grant at Part 2 of this Agreement;
“Funding Profile”	means how and when the Grant shall be defrayed to the Grant Recipient as specified in the Funding Particulars;
“Governing Body”	means the governing body of the Grant Recipient including its Directors or Trustees;
“Grant”	means the monies to be provided to the Grant Recipient by TVCA for the Agreed Purposes;
“Grant Claim”	means a claim for Grant in accordance with clause 8;
“Grant Recipient”	means the Party named in the Offer Letter;
“Irregularity”	means an infringement of a provision of applicable Law (including Subsidy Control Law and procurement requirements) resulting from an act or omission by a Grant Recipient or a Delivery Partner (and/or its agents and Subcontractors), which has, or would have, the effect of prejudicing the general budget of the TVCA by charging an unjustified item of expenditure to the general budget as determined following an Audit;
“Key Milestone”	means any milestone(s) which the Grant Recipient is required to meet with regard to delivering the Project and/or the Agreed Outputs as specified in the Funding Particulars;
“Law”	means any applicable Act of Parliament, subordinate legislation within the meaning of section 21(1) of the Interpretation Act 1978, exercise of the Royal Prerogative, bye-law, regulatory policy, guidance or industry code, judgement of a relevant court of law, or directives or requirements of any regulatory body of which the Grant Recipient is bound to comply;
“Lease Disposal”	means the grant of a Market Rent Lease;
“Logo”	means the logo for TVCA, the Tees Valley Mayor and HM Government as follows (and must be reproduced in colour):-

“Long Lease”	means a lease for a term equal to or exceeding ninety nine (99) years granted at a premium and reserving either a peppercorn or a nominal rent but which is not a Market Rent Lease;
“Losses”	all costs, actions, demands, expenses, losses, damages, claims, proceedings, awards, fines, orders and other liabilities (including reasonable legal and other professional fees and expense) whenever and however arising;
“Major Asset”	means an Asset which is not a Fixed Asset but has a value as at the date of this Funding Agreement (or date of acquisition if the acquisition is funded (in whole or in part) by this Grant) of at least £5,000.00 (five thousand pounds);
“Market Rent Lease”	means any lease which is granted at a full market rent namely not for a nominal rent or at a significant undervalue;
“Match Funding”	means the contribution to the Project to meet the balance of Eligible Expenditure not supported by Grant and the Match Funding details are contained in the Funding Particulars;
“Monitoring Period”	means the period specified in the Funding Particulars or such other period as may be agreed in writing by TVCA;
“Offer Letter”	means the letter to the Grant Recipient offering the Grant forming Part 1 of this Agreement;
“Ownership, Control and Nature of Business”	shall be construed in accordance with section 840 of the Income and Corporation Taxes Act 1988 and section 1162 of the Companies Act 2006 and for the avoidance of doubt shall include an evaluation of dominant influence and shadow directorships in the Grant Recipient from time to time;
“Party”	means the Grant Recipient or the TVCA as appropriate and ‘Parties’ means both the Grant Recipient and the TVCA;
“Payment Date”	means the date or dates given in the Funding Profile for the payment of the Grant or any part of it;
“Payment Period”	means the period from one Payment Date to the next consecutive Payment Date;
“Permitted Security”	any charge, legal mortgage, debenture, lien or other encumbrance over any Asset which has been approved by TVCA the details of which are set out fully in the Funding Particulars;
“Planned Disposal”	means a Lease Disposal of any Fixed Asset (or any part of the Fixed Asset) or the grant of a licence to occupy by the Grant Recipient which is intended as a planned step in the implementation of the Project, was referred to as such in

the Application, and the details of which are set out fully in the Funding Particulars;

“Preconditions”	means the Conditions Precedent in clause 3 and any other conditions which the Grant Recipient is required to meet prior to the Grant being paid by TVCA as set out in the Funding Particulars;
“Preliminary Expenditure”	means Eligible Expenditure in relation to site investigation works, site surveys, environmental appraisals and/or feasibility studies carried out in relation to the Project;
“Prohibited Act”	means any offence under the Bribery Act 2010 or any other legislation creating offences concerning fraudulent acts;
“Project”	means the Project in relation to which the Grant has been provided as described in the Funding Particulars;
“Project Activities”	means all activities to be carried out in implementing the Project and includes any Works to be carried out in relation to any Fixed Assets;
“Project Specifications”	means the drawings and specifications describing the Works which have been approved by TVCA in writing, or such revised drawings and specifications as TVCA may from time to time approve in accordance with the provisions of this Funding Agreement;
“Project Start Date”	means the date specified in the Funding Particulars, or such other date as may be agreed in writing by TVCA;
“Site”	means the land registered in favour of the Grant Recipient under the title numbers referred to in the Certificate of Title including any part thereof and also includes any estate or interest therein;
“Standard Funding Terms”	means TVCA’s Standard Terms and Conditions of Funding attached at this Part 3 of this Agreement;
“Subsidy Control Law”	means the rules embodied in Part 2, Title XI of the EU-UK Trade and Co-operation Agreement dated 24 th December 2020 and the Subsidy Control Act 2022;
“Sustainable Development”	means social progress which recognises the needs of everyone, effective protection of the environment, prudent use of natural resources and economic development that is inclusive, efficient and stable;
“Term”	means the period from the Project Start Date to the Agreed Completion Date;
“TVCA”	means Tees Valley Combined Authority of Teesside Airport Business Suite, Teesside International Airport, Darlington DL2 1NJ;

“UK GDPR”	has the meaning given to it in section 3(10) (as supplemented by section 205(4) of the Data Protection Act 2018;
“Value Added Tax (VAT)”	value added tax chargeable under the Value Added Tax Act 1994 and any similar replacement or additional tax; and
“Works”	means any works of construction and/or demolition and/or remediation and/or any other works in relation to a Fixed Asset which are described in the Project Specifications.

DRAFT

RESTRICTED: BUSINESS CASE

FOR TVCA USE ONLY – INITIAL GATEWAY CHECK	
TVCA Reference Number	PROJ-2371
Programme / Project Name	Linthorpe Road Highway Reinstatement
TVCA Group Company / Directorate	Infrastructure
TVCA Cabinet Allocation Amount	£2,387,585, including a proposed £2,169,699 GFA to MBC.
Cabinet Decision reference & date	TVCA 42/21 28 January 2022 DfT CRSTS Programme Delivery Plan 29 July 2022
Change required from SOBC to FBC?	This is an FBC, subject to final tendered costs by a procurement to be ran by Middlesbrough Council (MC)
Strategic Theme this project contributes to	Transport & Infrastructure
Investment Plan Theme this project delivers	Transport/Place/Business Growth
Equality Impact Assessment Signed?	Yes
Mayoral / Corporate Policy fit	Links to Middlesbrough Town Centre Development Corporation
Subsidy Control Regime Reviewed?	Yes
Outline Stakeholder Management Plan?	Yes
Contractual structure eg JV, partnership	N/A
Date of receipt of completed application	16/12/24
Priority Project?	Yes
Form signed	Yes

TVCA DISCLAIMER

Submitted Business Cases are subject to TVCA's appraisal process including appropriate due diligence. Following this process, a decision to fund or not will be made under the Local Assurance Framework.

There should be no expectation of funding assistance unless and until both parties sign a Funding Agreement. All the project sponsor's costs and charges incurred in making this Business Case shall be for the project sponsor's account and cannot be claimed as part of the Programme / Project

FOR DEVELOPMENT USE ONLY: VERSION CONTROL				
Date	Version No	Section Ref	Summary of Significant Change	Initials
16.12.24	1.0		Draft Business Case	CM
20.12.24	2.0	2	Revisions to Economic Case	CM

DRAFT

PROJECT SPONSOR DETAILS	
Lead Organisation:	Tees Valley Combined Authority
Registered Address:	
Type of Organisation:	Combined Authority
Date of Formation:	April 2016
Company Registration Number:	N/A
VAT Registration Number:	GB 232 6802 24
Are you part of a group of companies?	Yes
If so, who is the parent company?	TVCA
Lead Sponsor: name	Tom Bryant
Lead Sponsor: telephone number	07720601286
Lead Sponsor: email address	tom.bryant@teesvalley-ca.gov.uk
Lead Sponsor: position in organisation	Director - Infrastructure
Contact address if different from above:	
Project Manager: name	Craig MacLennan
Project Manager: telephone number	07386656020
Project Manager: email address	craig.maclennan@teesvalley-ca.gov.uk
Project Manager: position in organisation:	Senior Major Projects Manager
Contact address if different from above:	

PUBLICITY STATEMENT
<p style="text-align: center; font-size: 48px; opacity: 0.3; transform: rotate(-15deg);">DRAFT</p>

EXECUTIVE SUMMARY	
TVCA Ref:	PROJ-2371
Programme / Project Name:	Linthorpe Road Highway Reinstatement
Location: (delete as appropriate)	Linthorpe Road, Middlesbrough. (Ayresome Street to Borough Road)

Total Programme / Project Cost:	£2,387,585 – CRSTS1, including a £2,169,699 GFA to MBC.
TVCA funding requested: £	£2,387,585 – CRSTS1, including a £2,169,699 GFA to MBC.
Total of other funding sources: match £	0
Total of other funding sources: leverage £	0

The previous installation of the cycle lane infrastructure has generated a number of negative comments since its installation, particularly the loss of parking and loading provision, now not being adjacent to some businesses.

Considering the issues reported, it is proposed to remove the cycle lane infrastructure and reinstate the previous highway layout, including the installation of the previous parking supply. This includes the reinstatement of 47 bays to their original locations. This will support local business and the economy, and also mitigate other comments regarding impacting traffic flow (including for buses), which has been perceived as a negative consequence of the cycle lanes being introduced.

The value of the project and associated ask is £2,387,585, and it is proposed this is 100% funded from City Regional Sustainable Transport Settlement 1 (CRSTS1), with an allocation within the wider CRSTS programme to be utilised to support the Middlesbrough Development Corporation. This includes a Quantified Risk Assessment allocation, monetising foreseen risks. A cost validation exercise has been undertaken on estimates from both MC and the WSP detailed designer, based on a detailed Bill of Quants measured from the detailed design drawings.

A programme has been developed in dialogue with MC, seeking Traffic Regulation Orders to be advertised following their Executive Committee on February 5th, which would see a contractor appointed to allow commencement on site in May 2025, and completion before the end of 2025. The NEPO framework has been identified as the sourcing option to market.

The **BCR is 1.56** and has a Medium Value for Money rating and has also been tested using sensitivity tests based on parking usage, parking income and associated visitor expenditure.

CASE 1: STRATEGIC CASE

Key Messages:

Reinstatement of previous highway layout, including 47 parking bays back to their original locations, to support the local economy and businesses along Linthorpe Road, between Borough Road and Ayresome Street.

The project aligns with Local and National Economic and Regeneration policies and strategies, providing infrastructure to support the local economy. Overwhelming support has been received for the reinstatement of pay and display parking, enabled by the removal of cycle lanes, during a consultation in Summer 24.

The proposed scheme has been developed, considering a number of key success factors, including being delivered to programme and budget, and reinstatement of 47 pay and display bays generating additional parking revenue and associated visitor expenditure.

1.1 Strategic fit with Sponsor Organisation's Strategic or Local Plans

Name of Plan / Strategy / Programme	Summarise the relevant sections, and describe how this Programme / Project fits
TVCA Strategic Economic Plan (SEP)	The SEP is focused around six thematic building blocks which reflect the main priorities and areas of activities over the next ten years to unlock transformational growth: The project contributes to TVCA's policy objectives by aligning with Place Theme of the TVCA SEP. The Place Theme includes the wording "create places that attract and retain businesses and business leaders, and make the area an attractive place to live, work and visit".
Middlesbrough Development Corporation (MDC)	<p>The MDC was created to fund, manage, and accelerate regeneration in Middlesbrough town centre. The removal of the cycle lane and reinstatement of parking will support footfall and contribute to safeguarding existing and supporting new employment along Linthorpe Road, linking into the MDC area including Gresham.</p> <p>An allocation within CRSTS1 to support the MDC.</p>
Tees Valley Investment Plan	<p>This ten-year Investment Plan (2019-2029) is central to TVCA's plan to drive Teesside, Darlington and Hartlepool forward. The Devolution Deal with Government has secured hundreds of millions of pounds to stimulate economic growth and support the creation of thousands of jobs for local people. The Linthorpe Road Highway Reinstatement proposal links to three of the six growth generating themes:</p> <ul style="list-style-type: none"> Transport Business Growth Place

1.2 Strategic fit with National and Regional Policy / Strategy Documentation

Name of Strategy	Summarise relevant strategy and explain how the Programme / Project aligns
National Planning Policy Framework	<ul style="list-style-type: none"> - Building a strong and competitive economy - Ensuring the vitality of town centres
Government Cluster of Town Centre Regeneration Funds (Towns Deal, Future High Street Funding and UK Shared Prosperity Funding)	Whilst the scheme doesn't have any of these funds, it aligns to their overarching objective of supporting town centres and economic growth.
Build Back Better: Our Plan for Growth, HM Treasury (2021)	Infrastructure: Investment in roads to stimulate short-term economic activity and drive long-term productivity improvements.

1.3 Geographical Location and Summary of overall Strategic fit

The proposed project will be delivered on Linthorpe Road (B1272), between Borough Road and Ayresome Street. The length of the scheme is circa 1km. This section of Linthorpe Road is the most northern section, and links to the town centre and Middlesbrough Development Corporation. It serves both as a tertiary shopping area and an area with nightlife, with restaurants and pubs/bars along it. Despite the corridor acting as a high frequency bus corridor, there is a dependency for car parking.

The demand for parking is throughout the day, reflecting the dual nature as both a shopping and also nightlife area. Currently there is only 54 pay and display bays along the length, which is a reduction of 47%, following the installation of the cycle scheme. Feedback from businesses is that loading is difficult, with formal provision not adjacent to some businesses. Observations have shown that 'informal' loading and parking occurs in areas where it is prohibited, causing further traffic flow issues including on public transport operations.

It should be noted that the current provision of parking provision is not continuous along the whole length, and is grouped, which is thought to be linked to the parking occupancy levels decreasing from 33% in the previous layout to 18% currently. i.e. parking supply is not located as desired. This disadvantages businesses, who do not have parking and loading provision directly adjacent.

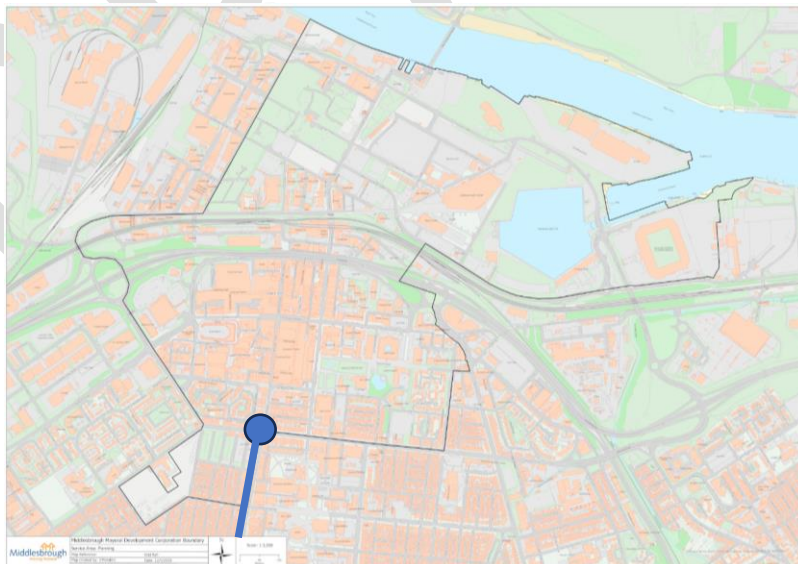
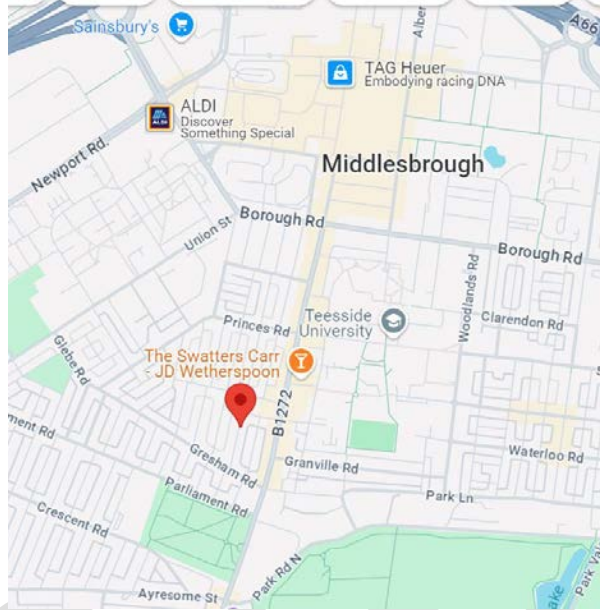
The road also serves residential streets to both the west and east of it, with a large population. Teesside University is also to the east. A significant proportion of the side roads have been closed to traffic, with footway crossovers installed as part of the original scheme. This has inconvenienced residents living in these areas, with longer journey distances.

It is proposed to reintroduce the original levels of Pay and Display parking, to support local businesses and local economy. To reintroduce the 47 Pay and Display Bays in their original location, this needs to be achieved, by removing the cycle lane infrastructure throughout. A highway cross section is not available to accommodate both the cycle lanes and the full parking reinstatement.

The reinstatement will also lead to a more linear layout, aiding the movement of buses and other highway users.

Side roads will be reopened, apart from King Edward Square and Victoria Street, to provide more convenient access for residents.

Geographical Context Plan of Linthorpe Road (B1272). Scheme proposal from Ayresome Street to Borough Road



1.4 Evidence of Need, Supply and Demand

Since the installation of the existing cycle lane in Autumn 2022 and the initial amendments to the scheme to address some of the problems with tripping on 'light segregation' features, feedback has continued from both businesses and residents related to problems and issues with the existing cycle lane infrastructure and the layout. Feedback has included:

- The remaining orcas present a trip hazard to pedestrians;
- Loading has become more difficult for businesses;
- There has been a loss of parking on Linthorpe Road, which has been deemed by the businesses to have negatively impacted their commercial operations;
- and
- Bus islands have created conflict between cyclists and bus users.

TVCA undertook a month-long consultation into the removal of the cycle lanes in August and September 2024. A total of 390 responses were received, approximately 60% from residents of Middlesbrough and 30% from Tees Valley residents living outside of Middlesbrough. Approximately 7% of responses were received from business owners or representatives, with the remainder from other local groups, schools or residents from outside the Tees Valley. The results of this consultation have shown the high levels of support for removing the cycle lane and reinstating the previous layout. The survey responses show 70.7% support for the removal of the cycle lanes on Linthorpe Road. 73% of respondents said parking and/or loading space should be reinstated.

Recurring themes that were mentioned in the responses include:

- Business owners state that the loss of car parking as a consequence of the cycle lanes, negatively affects trade, loading and deliveries;
- Comments that the cycle lanes have worsened traffic congestion. The reduction in road width and changes to bus stop designs have led to longer delays and difficulties for drivers, particularly during peak hours;
- Comments that the cycle lane's layout, and how people are informally parking/loading is said to obstruct emergency vehicles, making it harder for them to access Linthorpe Road; and
- Comments the cycle lanes are not well-used compared to the disruption they cause.

TVCA has also consulted with the local bus operators Stagecoach and Arriva over the proposal. Stagecoach strongly advocates for the removal of the cycle lane, citing delays to their services entering Middlesbrough due to the current road layout. Both operators support reinstating key bus stops, including at The Crown/King Edward Square, Granville Road, and outside One Life Medical Centre. The operators also urged a return to wider running lanes for general traffic to address safety concerns.

Cycle Flows

A comparison of average daily cycle count data for May 2022 (pre installation of the cycle lane) and May 2024 (post installation of the cycle lane) is set out below:

Comparison of Daily Cycle Flows, Before and After Installation

	Cyclists
May 2022	293
May 2024	394
Difference	101

Whilst cyclist numbers have increased with the installation of the cycle lane, this has been countered with negative perceptions of the original scheme.

Car Parking Numbers

On-street parking offers convenience, especially in areas where off-street parking options are limited. It allows drivers to park closer to their destinations, reducing the need for long walks. The location of the car parking facilities has an impact on where people choose to shop as consumers are more likely to visit stores in the close vicinity of where they parked. As a result, the reduction in car park users gives weight to the business concerns about a loss in trade. It should be noted that observations (14/12/2024) showed that the majority of vacant units along Linthorpe Road, no longer had pay and display parking or loading provision adjacent, as a consequence of the cycle lane infrastructure being introduced. Circa a dozen units were vacant along the impacted section of Linthorpe Road.

In 2018 there were 10 accidents recorded on Linthorpe Road – of these, none involved cyclists. Accidents involving pedestrians resulted in slight injuries to five pedestrians. Three of these (60%) involved pedestrians attempting to cross the carriageway from the driver's nearside and two from the drivers offside.

In the first full calendar year after installation of the cycle lane (2023), 8 accidents were recorded on Linthorpe Road – of these, one of which involved a cyclist. Accidents involving pedestrians resulted in slight injuries to four pedestrians and serious injuries to one pedestrian. Three of these (75%) involved pedestrians attempting to cross the carriageway from the driver's nearside and thus would have also been crossing the cycle lane.

Following opening there were previous news reports citing incidents, including a 78 year-old woman who suffered a broken wrist, a black eye, and concussion after tripping over one of the orcas and also a 27 year-old woman who fractured her elbow whilst crossing the cycle lane.

It should be noted the number of road accidents and casualties recorded fluctuates from year to year, and the extent to which these fluctuations represent an indication of a real underlying trend as opposed to random year to-year variation should be treated with some caution. This also considers the safety improvements implemented post opening.

Although there is no official data documenting the trip hazard issues, the consultation responses continue to raise safety concerns.

Reflecting the value of the scheme (>£2.3m), it's not viable or practical for businesses along Linthorpe Road to part or fully fund the measures. This particularly considers the businesses are small in nature, with typically less than 20 employees.

There is no S106 funding currently available, or likely to come forward, which could be utilised for the proposals.

The original scheme, which removed the parking, was funded using DfT funding. It would therefore not be felt appropriate for private sector funding, to reinstate the previous highway layout, even if such funding was available.

1.6 Constraints

Constraint	Description
Permanent Traffic Regulation Orders	<p>Traffic Regulation Orders are a legal requirement under The Road Traffic Regulation Act 1984, which are necessary when traffic restrictions are changed, as part of a proposed scheme. This is the case with this project. The TROs have to be advertised for a statutory period by MBC as the Highway Authority. This will occur following an MBC Exec Committee, intended for 5th February, subject to assurance of this Business Case. Construction can't commence until the TROs are advertised and made.</p> <p>Consultation has occurred during Summer 24 to formally gauge the opinion regarding the removal of the cycle lane infrastructure, which received overwhelming support.</p>
Highway Technical Approvals	<p>TVCA commissioned the design for the reinstatement.</p> <p>However MBC as the Highway Authority have had to formally approval the design, which occurred at their Executive Committee on 4th December 24.</p>
Road Safety Audit	<p>An RSA 1-2 has been undertaken in line with GG119 related to the proposed design.</p> <p>A RSA3 will be undertaken at the end of construction completion, ahead of opening.</p> <p>MBC as the Overseeing Organisation have the final say on recommendations from the Audits.</p>

1.7 Critical Success Factors

Critical Success Factor	Description and SMART Measures of Success
To Programme	The scheme to be constructed and open by the end of 2025
To Budget	The scheme to be delivered within the budget ask of £2,387,585
Parking reinstatement	Reinstatement of 47 Pay and Display parking spaces
Pay and Display user numbers	Increase in user numbers to previous levels – 45,805 increase, generating extra visitor expenditure.
Reduction in the number of vacant units	Reduced number of vacant units – currently circa 12.

DRAFT

CASE 2: ECONOMIC CASE

Key Messages:

Shortlisted option taken forward into the Business Case is for the full reinstatement of the previous highway layout.

Present Value of Benefits of **£1,793,024**, over a 20-year appraisal period. BCR of **1.56**, offering Medium Value for Money. Net Present Value of **£675,084**.

Benefit generation linked to increased parking levels, through the reinstatement of pay and display parking bays, with an increased income level of **£84,913.94 p.a.** And related visitor expenditure of **£229,025 p.a.** These figures before discounting.

2.1 Options Development: Long list to short list

Do Nothing

In this option the current layout, with the cycle lanes demarcated by orcas and wands, will remain in place with no changes. 54 parking bays will remain.

Do Minimum Option: Remove Orcas but Retain Cycle Lane Layout – up to circa £100k

This option sees the orcas that provide partial separation of the cycle lane from vehicles removed, to address the specific issues raised around potential trip hazards. The existing cycle lane layout would be retained, but with the removal of those vertical features. No other significant changes would be implemented. 54 parking bays would remain, with the other 47 bays not reinstated.

Do Something Option 1: Full Removal - £2.38m - retaining closures of Victoria Road and King Edward Square, and retaining 20 mph speed limit

In this option, the existing cycle lane infrastructure between Borough Road and Ayresome Street will be removed, and the road will be put back to its original layout. Although the Victoria Road and King Edwards Square closures will remain in place, as will the retention of the current 20mph speed limit. As part of the removal of the cycle lane, 47 short-stay parking spaces will be reinstated, taking the total number to 101. All bus stops will also be put back to their original design and layout. All other side road closures will be removed allowing direct access to/from Linthorpe Road from adjacent residential areas.

Do Something Option 2: Full Removal – circa £2.5m

This option reflects the above, but also reopens the King Edward Square and Victoria Road closures, as well as reinstating the previous 30mph speed limit.

Long List to Short List

The **Do Nothing Option** is **discounted** from the Long List as doesn't resolve any of the issues raised during consultation.

The **Do Minimum Option** would address any pedestrian trip hazard issues with the existing orcas, referenced in the consultation. However the issues raised in the consultation by local businesses about the reduction in trade and difficulties with servicing would also remain. As a result, this option is **discounted** from the Long List.

Short List Prioritisation

The **Do Something 1** and **2 Options** are comparable in the economic outputs they provide, in terms of pay and display bays reinstated, and related visitor expenditure.

Do Something 2 is not consistent with an approved planning application from Wetherspoons for an outside table area at the junction with Victoria Street. **Do Something 2** is also not consistent with a potential future pedestrian crossing linking over Linthorpe Road from the Gresham development. It should be noted however this is subject to separate processes, including related planning applications.

Do Something 1 also provides retains a lower speed limit, which will assist with cyclists riding within traffic lanes with general traffic, with the proposed existing light segregation removed.

Therefore **Do Something Option 1** has been taken forward as the shortlisted option to test via an economic appraisal.

2.2 Shortlisted Option: Monetary Costs and Benefits

Removing the existing cycle lanes and restoring on-street car parking will generate additional parking revenue, over and above what would be received if the cycle lane infrastructure was to remain in place.

To calculate the value of this additional revenue, the following assumptions have been made:

- Calculations are based on on-street car parking income figures obtained from Middlesbrough Council for 2019/20 and 2023/24;

- It is assumed that with the cycle lane still in place, the parking revenue would remain at 2023/24 levels;

- With the cycle lane removed, parking demand would return to 2019/20 levels, but with an uplift to reflect increases in parking charges. This considers parking is to be reinstated in its original locations;

- The value of this additional car parking revenue has been estimated in 2026/27 prices for a 20-year appraisal period, commencing from 2026;

- Average parking cost would increase in line with historic levels at a rate of 1.028 per annum, informing an average cost of £1.85 in 26/27. To provide a conservative and robust assessment, the parking cost is held flat from 26/27;

- This gives a value of the additional car parking revenue over a 20 year appraisal period of **£484,974**, discounted to 2010. Before discounting this equates to **£84,914** extra parking revenue per annum;

- Visitor expenditure has been applied at a conservative £5 per car over an appraisal period of 20 years. This provides an expenditure figure of **£1,308,409**, discounted to 2010. Before discounting this equates to **£229,025** extra visitor expenditure per annum on Linthorpe Road, supporting local businesses and the economy.

- With sunken development/design costs, inflation and the QRA value removed, but with Optimism Bias of 20% applied, the **Present Value of Costs in 2010**

- Market Prices** is **£1,146,405** over the appraisal period;

- The Net Present Value is **£646,614**;

The Benefit Cost Ratio is **1.56**, reflecting **Medium Value for Money**; Maintenance Costs haven't been applied, as qualitatively it is felt there will be a maintenance saving, compared to the current layout. This considers there will be less infrastructure within the carriageway.

Parking Demand and Revenues – Before and After Introduction of the Cycle Lane Infrastructure

	2019/20	2023/24
	Linthorpe Road (previous layout – without cycle lane infrastructure)	Linthorpe Road (with cycle lane infrastructure)
No. spaces	101	54
19/20 income (£)	£108,315.00	£43,260.00
No. customers	71,453	25,648
Average cost (£)	£1.52	£1.70
Average stay (hours)	1.2	1.2
Max Annual income (100% efficiency of spaces used based on average use/income) (£)	£327,724.80	£242,611.20
% of actual vs maximum income	33%	18%

Do Nothing versus Do Something Scenarios, with Car Parking Prices Uplifted

	2026/27	2026/27
	Linthorpe Road Do Nothing – cycle lane infrastructure in place	Linthorpe Road Do Something – cycle lane infrastructure removed
No. spaces	54	101
19/20 income (£)	£47,192.32	£132,106.26
No. customers	25,648	71,453
Average cost (£)	£1.85	£1.85
Average stay (hours)	1.2	1.2
Max Annual income (100% efficiency of spaces used based on average use/income) (£)	£262,590.95	£399,709.15
% of actual vs maximum income	18%	33%
	Increased parking revenue of £84,913.94 per annum x 20 years Benefits before discounting = £1,698,279 Present Value of Benefits, after discounting to 2010 = £484,974	

Direct Construction Costs

Base Construction Cost (Q2 2021 Prices)	£891,445
Allowance for traffic signals	£150,000

Indirect Construction Costs

Main Contractors preliminaries and traffic management	£325,000
Overheads and Profits	£136,644
Fees (construction PM, CDM, PM)	£122,127
Benefits Realisation Report	£30,000

Total excl. Risk and Inflation **£1,655,216**

Optimism Bias	£331,043
Inflation	£283,768

Total **£614,811**

Present Value of Costs

£2,270,027

£1,146,045

Present Value of Costs (discounted to 2010 market prices)

An additional 45,805 vehicles are projected per annum. Based on a conservative expenditure per car of £5, this equates to £229,025 additional expenditure per annum, before discounting. With discounting to 2010 prices, this equates to £1,308,049, over a 20 year appraisal period.

Present Value of Benefits (parking revenue) = £484,974

Present Value of Benefits (visitor expenditure) = £1,308,045

Above Divided by Present Value of Costs = £1,146,045

BCR = 1.56

The Centre of Cities Visitor economy data tool (November 2024) finds that the average spend per trip of a day visitor to Middlesbrough was £43. For the purpose of this business case, the average local spend of those visitors parking on Linthorpe Road has been conservatively estimated at £5 for an average stay of 1.2 hours.

2.3 Shortlisted Option: Sensitivity Analysis

Parking demand (no. of customers) would need to decrease by 2,953 customers per annum, from the projected level, for the Value for Money rating to go from a Medium to Low rating (<1.5). This would represent a 4.2% reduction in the number of users, compared to the projected level.

Sensitivity Analysis – Parking Demand – Medium to Low Value for Money

	2026/27	2026/27
	Linthorpe Rd (On carriageway) - Do Nothing	Linthorpe Rd (On carriageway) (2026 Do Something)
No. spaces	54	101
19/20 income (£)	£47,192.32	£126,646.59
No. customers	25648	68,500
Average cost (£)	£1.85	£1.85
Average stay (hours)	1.2	1.2
Max Annual income (100% efficiency of spaces used based on average use/income) (£)	£262,590.95	£383,190.03
% of actual vs maximum income	18%	33%
	Increased parking revenue of £79,454.27 per annum x 20 years Increased visitor expenditure of £214,260 p.a. Total Present Value of Benefits discounted to 2010 over a 20 year appraisal period (£1,677,509) / Present Value of Costs (£1,146,405) = BCR = 1.46	

Alternatively if the parking demand did not change from projected levels, the Present Value of Costs (discounted to 2010) would need to increase from £1,146,405 to £1,195,347 for the Value for Money rating to go from Medium to Low (<1.5). This would represent a 4.1% increase in costs.

Parking demand (no. of customers) would need to decrease by circa 16,453 customers per annum, from the projected level, for the Value for Money rating to go from Medium to Poor (<1.5). This would represent a 23.11% reduction in the number of users, compared to the projected level.

Sensitivity Analysis – Parking Demand – Medium to Poor Value for Money

	2026/27	2026/27
--	---------	---------

	Linthorpe Rd (On carriageway) - Do Nothing	Linthorpe Rd (On carriageway) (2026 Do Something)
No. spaces	54	101
19/20 income (£)	£47,192.32	£101,687.04
No. customers	25648	55,000
Average cost (£)	£1.85	£1.85
Average stay (hours)	1.2	1.2
Max Annual income (100% efficiency of spaces used based on average use/income) (£)	£262,590.95	£307,670.82
% of actual vs maximum income	18%	25%
	Increased parking revenue of £54,494.72 per annum x 20 years Increased visitor expenditure of £146,760 Total Present Value of Benefits discounted to 2010 over a 20 year appraisal period (£1,146,405) / Present Value of Costs (£1,146,405) = BCR = 1	

There are also other monetised benefits, which could have been attainable linked to the scheme. However for robustness for the BCR these haven't been quantified. These include:

- Some benefit to bus journey times/delays; and
- Reduced maintenance liabilities for the infrastructure removed.

The increased parking numbers and visitor expenditure are also likely to support new jobs and safeguard existing jobs along Linthorpe Road, as well as leading to a decrease in vacant units.

It should also be noted that the discounting year is to change from the DfT in May 2025, from 2010 to 2023, which would further boost the economics.

It can therefore be seen with confidence that the BCR has a Medium Value for Money rating.

2.4 Shortlisted Option: Non-Monetary Costs and Benefits

This section considers TAG Appraisal Summary Table qualitative impacts.

2.4.1 Economy

Business Users and Transport Providers

The realignment of the highway, and removal of the cycle lanes, will enable the easier movement of buses along Linthorpe Road. **Slight Beneficial**

Reliability Impacts on Business Users and Transport Providers

As above, an assessment of **Slight Beneficial** has been concluded for the scheme.

Regeneration

A moderate beneficial impact for regeneration is expected, with increased parking and associated expenditure, safeguarding and creating employment. **Moderate Beneficial**

Wider Impacts

The removal of the cycle lane infrastructure will remove an active mode route on one the main radial routes into the town centre. However this will be offset by other active mode links, to be delivered by LUF2 and CRSTS1, including on Newport Road, Longlands Road and Marton Road. **Negligible**

2.4.2 Society

Commuting and Other Users

The impact on commuters and other users is expected to be a **Negligible** impact. Whilst the cycle lanes will be removed, as well as pedestrian priority measures over side streets (side streets proposed to be reopened), the removal of the cycle lanes will allow easier movement for buses along Linthorpe Road.

Physical Activity

Physical activity benefits primarily refer to the uptake of cycling and walking. With the scheme involving the removal of cycle lane infrastructure, there will be a **Moderate Adverse** impact, offset to some extent by the proposed introduction of active mode schemes linking to the town centre via LUF 2 and CRSTS1 funding.

Journey Quality

Journey quality is defined as a “measure of the real and perceived physical and social environment experienced while travelling” according to TAG Unit A4.1 (Social Impact Appraisal). There are three distinct aspects of journey quality:

- Traveller Care: Dedicated provision for cyclists will be removed, potentially detracting from traveller care.
- Travellers' Views: Consultation has shown the majority of people want the cycle lanes removed.
- Traveller Stress: Easier movement for buses and other vehicles along Linthorpe Road, with the cycle lanes removed.

Overall **Negligible** impact.

Accidents

The number of road accidents and casualties recorded fluctuates from year to year and the extent to which these fluctuations represent an indication of a real underlying trend as opposed to random year to-year variation needs to be treated with caution.

However, issues of concern regarding the safety were raised in the Summer 2024 consultation.. **Negligible**

Personal Safety

Dedicated segregated cycle provision is proposed to be removed, which would potentially be perceived as a disadvantage by some less confident cyclists.

Natural surveillance related to the perception of crime would be unchanged.

Perceived tripping hazards related to orcas would be removed.

Overall **Negligible** impact.

Affordability

The scheme will have **No Impact** on affordability.

Severance

The proposal will largely remove side road priority crossing measures for pedestrians, but will reopen the majority of side roads, providing direct access from adjacent residential areas onto Linthorpe Road. Overall **Negligible** impact.

Access to Services

Removal of active mode measures will reduce the attractiveness of one modal choice to access services. **Slight Adverse** impact.

Option Values

TAG states the monetisation of option values should be limited to the opening / closure of rail stations or the introduction / loss of good quality local bus services. Therefore, **No Impact** is expected.

2.4.3 Environment

Noise

The removal of the cycle lanes, may lead to a lessened number of cyclists and potentially an increase of private car journeys, albeit some of the mode shift may go to buses. The removal of the cycle lanes will ease movements of buses along Linthorpe Road. **Slight Adverse** impact on noise in the area.

Local Air Quality

A decrease in mode share for cyclists may have a **Slight Adverse** impact on local air quality as the number of car journeys may increase slightly. Albeit some of the mode shift may go to buses. The removal of the cycle lanes will ease movements of buses along Linthorpe Road.

Greenhouse Gases

The scheme may increase the mode share for private car travel and reduce it for cyclists, leading to some greenhouse gas increases. **Slight Adverse**. Albeit some of the mode shift may go to buses. The removal of the cycle lanes will ease movements of buses along Linthorpe Road. There would be some carbon impacts during construction, albeit efforts would be made to minimise this.

Landscape and Townscape

No impact is anticipated.

Cultural Heritage

No impact is anticipated.

Biodiversity

No impact is expected.

Water Environment

No impact is anticipated.

2.4.4 Other Benefits

There are also other monetised benefits (Level 1 and 2) which could have been attainable linked to the scheme. However for robustness for the BCR these haven't been quantified. These include:

- Some benefit to bus journey times/delays; and
- Reduced maintenance liabilities for the infrastructure removed.

The increased parking numbers and visitor expenditure are also likely to support new jobs and safeguard existing jobs along Linthorpe Road, as well as leading to a decrease in vacant units.

2.5 Shortlisted Option: Delivery Risks

The strategic risk and potential showstopper to the scheme, outside of the Business Case not being approved and funding being released, are the TROs not being approved. Relevant objections would need to be received during the advertisement period, which would need to be upheld by the Highway Authority, meaning the construction couldn't occur. It however should be noted that the proposal received widespread support during consultation earlier in 2024.

Whilst the scheme is in a sensitive location on a radial route at the edge of the town centre, including as a bus route and a tertiary shopping/nightlife area, the construction risks are considered standard for a scheme of this nature. MBC have a thorough knowledge of the locality and risks following the installation of the original scheme, including the location and depths of utilities. Elements of the original Construction Phase Plan are thought likely to be able to be used, to inform the sequencing of the works.

A robust cost estimate has been put together, including with an appropriate QRA value, including an allowance for any uplift in inflation associated with factors outside of the control of TVCA or MBC.

The NEPO framework will be utilised to appoint a contractor, with market testing to be considered, following the appraisal of the business case.

2.6 Value for Money Statement

The VfM assessment of Linthorpe Road Highway Improvements has highlighted the scheme is expected to be of **Medium VfM**. The overall core **BCR is 1.56** and, which does not include other potential monetised benefits, which potentially could have improved the BCR.

Sensitivity testing has been undertaken which has shown there would need to be a reduction in projected parking demand or an increase in costs, providing confidence to the Medium VfM rating. Parking demand (no. of customers) would need to decrease by circa 2,953 customers per annum, from the projected level, for the Value for Money rating to go from a Medium to Low rating (<1.5). This would represent a 4.2% reduction in the number of users, compared to the projected level.

The strategic motivation for the scheme further strengthens the VfM. The promotion of the town centre, as a retail and employment destination, via increased parking in suitable locations is evident in the policy and strategy review presented in the Strategic Case.

To offset the loss of the cycle lane infrastructure, TVCA and MBC are looking to deliver active mode schemes along suitable corridors into the Town Centre, including Marton Road, Longlands Road and Newport Road. These designs avoid impacting upon parking provision and any negative associated impacts upon businesses and residents. These schemes are proposed to be delivered in the LUF and CRSTS 1 period, ahead of March 27, with opening shortly after the proposed reinstatement of the original Linthorpe Road Highway. However, it should be noted these schemes are subject to their own processes, including TROs and Business Case assurance.

CASE 3: COMMERCIAL CASE

Key Messages:

The construction contract to be tendered via the NEPO Framework by MBC. NEC Option A Contract between MBC and appointed contractor, but TVCA to approve compensation events raised.

No subsidy control issues, but TROs needed to be advertised (by MBC) and made, before construction can commence.

3.1 Subsidy Control

It is considered that the funding does not conflict with subsidy control issues as the funding is for infrastructure (within the adopted highway), which will not be commercially exploited and will be open for general public use by all. The parking bays will be available to be used by all, and not just by selective businesses or individuals, and will not involve a selective advantage which would distort or threaten to distort competition. The wider amendments to the highway will also be available to be used by all highway users. Construction will be tendered competitively via the NEPO framework, so will not give assistance to one or more undertakings over others. TVCA and MBC are carrying out a non-economic activity as public authorities in promoting and introducing infrastructure on the adopted highway.

MBC will receive a projected increased level of parking revenue, related to the reinstatement of parking, previously removed as part of the installation of the cycle scheme. 100% of this increased parking income will be utilised for revenue within their Parking Service, including enforcement and maintenance of Pay and Display Bays, upon an adopted highway in their role of Highway Authority. A private sector parking operator will not operate or benefit from the additional parking bays and related forecast income.

3.2 Delivery Mechanism

TVCA have utilised WSP for the detailed design of the reinstatement scheme, with them being their CRSTS1 framework partner consultancy. WSP also produced the design for the original cycle lane installation, and have a thorough knowledge of the local highway, including holding various relevant surveys such as drainage and topographical.

Middlesbrough Traffic Signals will be utilised for the traffic signals detailed design and commissioning, as the framework Traffic Signals design team, utilised by Local Authorities across the Tees Valley.

TVCA will fully fund the proposal, without a need for match funding, from CRSTS1 funds. This is subject to assurance of this business case and advertisement of the Traffic Regulation Orders (TROs).

An RSA 1-2 was commissioned by TVCA, utilising Nicholson Sloan Consultancy, with MBC responsible for Overseeing Organisation responsibilities, as the Highway Authority.

MBC are also responsible for the approval of the detail design, which occurred at their Executive Committee on 4th December 2024.

MBC are responsible for advertising TROs, and making subsequent decisions on feedback/objections received.

Ahead of this TVCA and MBC have collaboratively ran a consultation over summer 2024, to inform the detailed design and subsequent TROs.

Without a Section 8 (Highways Act 1980) in place to allow the option of TVCA of procuring the works onto site, this will be undertaken by MBC, who will procure the works via the NEPO Framework.

MBC will adopt the highway alterations at the end of construction, as part of their Highways Assets Management register.

3.3 Procurement Approach

3.3.1 Sourcing Options

This explains the options for sources of provision of services to meet the business need with rationale for selected preferred sourcing option.

Design

The civils detailed design for Linthorpe Road Highway Reinstatement has been undertaken by WSP, commissioned by TVCA, as TVCA's framework consultant for CRSTS1. This is via an existing overarching call off contract.

The traffic signals design and commissioning will be undertaken by Middlesbrough Traffic Signals, as the signals design body representing all the Highway Authorities in the Tees Valley.

Construction

With regards to the sourcing options for construction, this section considered the following: (i) existing frameworks, (ii) competitive tender process, and (iii) internal building construction services.

MBC will be responsible for arranging the construction procurement, with TVCA not holding a Section 8 to procure works onto the adopted highway directly.

Existing Frameworks

An appropriate framework exists for MBC to use for the construction and handover of built schemes, which could alleviate some degree of risk and reduce the timescale for delivery. This is the North East Procurement Organisation (NEPO) Building Construction Works Framework. As MBC is already a member of this framework, this option could be utilised. There are a number of regional and national contractors on the NEPO framework.

The use of this existing framework would require a contained tender period, and the Contractor would not have sight of the emerging designs, as they would not have been appointed in advance.

Competitive Tender Process

The competitive tender process includes several options: (i) Open, (ii) Restricted, (iii) Accelerated Restricted and (iv) Competitive Dialogue. The advantages of a competitive tender process are that it provides a robust procurement route that follows legal regulations as the tender is open to a range of contractors, competitive prices should be received, MBC could choose which form of contract the work is awarded under, and the route provides a clear, auditable procurement and award process. However, the disadvantages include the increased level of MBC resource required to undertake the procurement process and the longer procurement timescales compared to alternative options.

MBC Highway Operations

A team does exist within MBC to deliver projects similar this proposal. The use of internal resources allows involvement of the team from the early phases of design through to handover, working closely with the Designer. Once the project is complete on site, the team continue to work with MBC to ensure that all relevant compliance and maintenance requirements are in place.

Chosen Sourcing Option

Following a review of the sourcing options, the chosen option has been to procure via the NEPO Framework. This decision was made for the following reasons:

- Reduces procurement timescales, associated with a competitive tender;
- Competitive tender disproportionate to the value of the works;
- MBC do not have internal construction resources available;
- Multiple contractors on the NEPO framework, will provide a competitive cost comparison proving best value for money;
- NEPO framework typically consists of North East based contractors, so therefore using local resources. Other social value requirements can be prescribed in the tender;
- Pre-qualification process to join the NEPO framework reduces the administration resource requirements, if MBC had ran a competitive tender;
- NEPO has been developed for the delivery of civils/highway schemes, which demonstrate bidder's experience delivering similar infrastructure schemes; and
- Following the original installation scheme, MBC have prior knowledge of the construction phasing, to act as early contractor involvement and help inform the tender.

This is consistent with MBC's internal procurement regulations, which require four tenders via NEPO, for construction contracts between £1,000,001 and £4,477,174.17 (exclusive of VAT). This also complies with TVCA's procurement regulations, as the funder of the project.

3.3.2 Contract Type and Payment Mechanism

Control of costs throughout scheme development will be achieved through a New Engineering and Construction (NEC) Contract NEC4 Option A (Priced Contract with Activity Schedule). The contract will be between MBC Council and the appointed Contractor, but the Grant Funding Agreement will be between TVCA and MBC, detailing a collaborative open book process reflecting TVCA as the funder. The

Contractor will be paid following the submission of monthly claims. The Contractor will submit monthly claims to the Construction Project Manager/Quantity Surveyor. They will validate the claims, comparing progress on site, and make a recommendation to MBC to proceed. Following approval, payment will be made to the Contractor directly via MBC's finance system within 28 days of issues of the initial valuation. Quarterly claims will then be made from MBC to TVCA.

3.3.3 Risk Management

In order to identify and devise a strategy to manage, or avoid risk, and also to understand the residual risk, a QRA was undertaken in December 2024. The process adopted the four-stage development process of:

- Risk identification;
- Risk impact assessment;
- Risk likelihood assessment; and
- Risk management.

The QRA sets out how the risk mitigation to minimise the likelihood of the risk event occurring. The QRA has been produced in line WebTAG Unit A1.2, producing a risk-adjusted cost estimate. The QRA is in the form a 5x5 calculation, with the value to go in the estimate, based on a median mitigated risk value. The QRA considers all foreseen risks identified during the development and design of the project.

If risks are realised on site and need to be drawn down against the QRA value, then these will be reported to TVCA for approval. Any unutilised element of the QRA allowance will be returned to TVCA.

TVCA's funding terms are that where it provides a grant to anyone who deliver a scheme, it cannot take responsibility for cost overruns over and above a quantified risk allowance which has formed part of the business case. However any exceptional risks and related financial implications realised during construction, which could not have been identified as part of the development of the scheme and quantified risk assessment process, can be reported back to the Tees Valley Combined Authority for consideration.

Any new risks not identified in the QRA, but can be contained within the original QRA value, can be claimed subject to TVCA approval.

3.3.4 Contract Length

The construction period will be informed by the tenders returned and appointment of the contractor. However based on the programme for the original scheme installation, it is anticipated a 7 month construction period may be required, running from May to December 2025. This reflects the sensitivities of Linthorpe Road, in terms of being a key radial route to/from the Town Centre, a high frequency bus corridor and need to provide loading and parking access wherever possible during construction.

3.3.5 Contract Management

An NEC4 Option A contract will be agreed between MBC and the contractor, which is the standard form for UK construction works. A Construction Project Manager and Quantity Surveyor will administer the contract, working client side to MBC, but will operate in an open and collaborative manner with TVCA, as the funders of the project including approval of reported Compensation Events. The NEC Construction Project Manager will be responsible for the following:

- Monitoring progress;
- Issuing Project Manager instructions;
- Accepting subcontractors;
- Assessing Compensation Events;
- Issuing Early Warnings;
- Maintaining the Risk Register;
- Assessing amounts due for payment;
- Ensuring that the works are constructed in line with the specification;
- Ensuring any required quality testing is undertaken;
- Reporting any major issues to the Project Manager;
- Keeping daily logs and a site diary;
- Keeping photographic records of construction for future reference;
- Producing monthly progress reports; and
- Searching for and arranging the correction of defects.

3.3.6 Value for Money

Utilising the NEPO Framework for the construction procurement, will allow multiple contractors to tender, and help prove best value for money.

WSP and MBC's detailed design estimates show a close comparison, with only a marginal difference, giving confidence to the Finance and Economic Cases.

The construction estimate tender will be via a detailed Bill of Quants, providing granularity which will allow MBC and potentially TVCA to review relevant rates as required.

3.4 External Permissions and Studies

Consultation was held during August 2024, with the majority of respondents wanting the removal of the cycle lanes. This has informed the detailed design to return to the previous highway layout. The detailed design has now been complete and approved by MBC Committee on 4th December 2024. The TROs for this proposal will now be advertised from February, subject to the assurance of this Business Case by TVCA. Should the proposed TROs be made, the construction will then commence.

3.4.1 Feasibility Studies

Consultation on the proposals, to inform the detailed design and subsequent TROs, held in August 2024.

3.4.2 Land Ownership

Not Applicable – all within the adopted highway

3.4.3 Planning Permission

Not Applicable – permitted development rights, with MBC procuring the works. The Town and Country Planning (General Permitted Development) (England) Order 2015. Schedule 2. Part 9.

Permitted development

A. The carrying out by a highway authority -

(a) on land within the boundaries of a road, of any works required for the maintenance or improvement of the road, where such works involve development by virtue of section 55(2)(b)(1) of the Act; or

(b) on land outside but adjoining the boundary of an existing highway of works required for or incidental to the maintenance or improvement of the highway.

As described elsewhere permanent Traffic Regulation Orders will need to be advertised by MBC as the Highway Authority, related to The Road Traffic Regulation Act 1984.

3.4.4 Detailed Design

The detailed design has been approved by MBC, in their role as Highway Authority, at their Executive Committee on 4th December 2024.

CASE 4: FINANCIAL CASE

Key Messages:

The capital investment is to develop (within 24/25) and deliver (25/26) the highway reinstatement of Linthorpe Road as per the previous layout, removing the cycle infrastructure and reinstating previous pay and display car parking. The capital investment will be inclusive of all design and development fees, full construction costs, and also subsequent monitoring. The scheme is to be 100% fully funded from CRSTS1 from the TVCA, without any other match or leverage from other sources. MBC will pay the contractor on a monthly basis in line with NEC contract regulations and will make quarterly claims in arrears to TVCA.

4.1 Summary of Expenditure and Funding

The Financial Case considers the affordability of the project, funding arrangements and technical accounting issues, in line with DfT Transport Business Cases Guidance (2013). The Financial Case considers the costs and accounting issues, while the Value for Money (VfM) is dealt with separately in the Economic Case. This section,

therefore, sets out how much the scheme will cost and how this has been calculated, the risks that may affect the costs, the anticipated profile of expenditure and how the scheme will be paid for.

4.1.1 Base Capital Costs

The estimated base cost of the scheme from MBC, in Q3 2021 prices, excluding risk and future inflation is **£1,796,130**. This is made up of:

Base Construction Cost – £891,445
Allowance for Traffic Signals – £150,000
Main Contractors preliminaries and traffic management - £325,000
Overheads and Profits - £136,644
Fees (Design, Construction PM, CDM, Other PM Costs) - £263,041
Benefits Realisation Report - £30,000

The base construction costs have been informed, after a detailed design process. The detailed design is fully complete for the scheme, bar the traffic signal design which is at a preliminary design stage. Measurements and off takes have been produced by the designer in the form of Bill of Quantities, with MBC then producing a Bill of Rates, from their most recent library which is 2021.

Following dialogue with Traffic Signals Engineers, an allowance of £150,000 has been provided for the traffic signals elements, ahead of the detailed design being complete.

Appropriate indirect construction costs, including for preliminaries and traffic management; overheads and profits; fees (design, construction PM, CDM) and a benefits realisation report have then been applied.

- Preliminaries and traffic management has an allowance of £325,000, which represents 32% of the direct construction costs;
- Overheads and Profits has an allowance of £136,644, which represents 10% of the direct construction costs plus preliminaries and traffic management;
- Fees (design, construction project management, CDM and other project management) has an allowance of £263,041, which represents 17.5% of direct construction costs, plus overheads and profits and plus preliminaries and traffic management

4.1.2 Risk

The risk value, via an agreed Quantified Risk Assessment, is valued at £285,518.

4.1.3 Inflation

Middlesbrough have provided the base construction costs in Q3 2021 prices, which is their latest library of rates, reflecting the date of the installation of the original scheme. The costs have been updated to Q3 2025 prices (BCIS Indices), which reflects the mid-point of construction. This reflects an inflation uplift of 20.35%, which equates to £305,937.

4.1.4 Cost Comparison for Detailed Design Estimates

The comparison between MBC's and WSP's overall detailed design estimates show a minimal cost difference of £42,201, providing confidence in both the Financial Case and for costs to be carried forward into the Economic Case. MBC's cost is the higher one, and has therefore been carried forward into the Economic Appraisal, to provide the most robust approach.

MBC Detailed Design Cost Estimate

Direct Construction Costs

Base Construction Cost (Q2 2021 Prices) £891,445

Allowance for traffic signals £150,000

Indirect Construction Costs

Main contractors preliminaries and traffic management £325,000

Overheads and Profits £136,644

Fees (design, construction PM, CDM and other project management) £263,041

Benefits Realisation Report 30,000

Total excl. Risk and Inflation £1,796,130

Risk £285,518

Inflation (taken forward from Q3 21 to Q3 25 – mid point of construction – 20.35%)
£305,937

Total £591,455

Total Cost £2,387,585

WSP Detailed Design Cost Estimate

Direct Construction Costs

Base Construction Cost inclusive of Traffic Signals allowance (2024 Q3 Prices)
£1,209,325

Indirect Construction Costs

Main contractors preliminaries and traffic management £325,000

Overheads and Profits £153,433

Fees (design, construction PM, CDM and other project management) £295,358

Benefits Realisation Report £30,000

Total excl. Risk and Inflation £2,013,115

Risk £285,518

Inflation (taken forward from Q4 24 to Q3 25 – mid point of construction – 2.77%)
£46,751

Total £332,269

Total cost £2,345,384

4.1.5 Construction Tender

The final estimate is subject to a tender to be organised via MBC and procured via the NEPO Framework. Multiple private sector contractors on the framework will provide a competitive comparison, ensuring best value for money. It should be noted that any significant variance would need to be discussed with TVCA and may require submission of a Project Change Request and a recalculated BCR which would require internal assessment and approval. The construction tender will run from April to May 25, with appointment planned to be made in May 25.

No on-costs will be applied by MBC, over and above hourly wages, for the procurement to be arranged by them.

4.1.6 Accountancy Implications

The works are 100% capital investment, and to be completely funded from CRSTS funding, with funding devolved to the TVCA from Central Government. No other match is to be utilised, or leverage gained. No 'commuted' sum will be provided from this capital investment, towards future maintenance, which will be undertaken by Middlesbrough Borough Council as part of their Highways Assets Management register.

No income will be gained by the TVCA related to the schemes' operation once opened. Middlesbrough Borough Council will receive a projected increased level of parking revenue (£84,281 p.a.), related to the reinstatement of parking, previously removed as part of the installation of the cycle scheme. 100% of this increased parking income will be utilised for revenue within their Parking Service, including enforcement and maintenance of Pay and Display Bays.

Project Expenditure

All CRSTS1	24/25	25/26	26/27	27/28	28/29	29/30	30/31	Total
Design and Development Costs, including Project Management	£140,914 (67%)	£46,972						£187,886
Construction Costs		£1,809,026						£1,809,026
QRA (construction costs)		£285,518						£285,518
NEC Project Management/QS/Site Supervision Costs		£75,155 (100%)						£75,155
Evaluation and Monitoring Costs		£5,000 (16.67%)	£5,000 (16.67%)	£5,000 (16.67%)	£5,000 (16.67%)	£5,000 (16.67%)	£5,000 (16.67%)	£30,000
Total	£140,914	£2,221,671	£5,000	£5,000	£5,000	£5,000	£5,000	£2,387,585

4.2 Programme / Project Match Funding Sources

No other match or leverage is needed or to be utilised for the project. 100% funded from CRSTS.

Ongoing revenue funding in the form of maintenance, will be funded by Middlesbrough Borough Council, and managed as part of their Assets Management Register. This is likely to be Local Transport Plan Highways Maintenance Block, or

other Council Capital funding. No 'commuted' sum will be allocated from CRSTS for this.

4.3 Leverage Funding

No other match or leverage is needed or to be utilised for the project. 100% funded from CRSTS.

Ongoing revenue funding in the form of maintenance, will be funded by Middlesbrough Borough Council, and managed as part of their Assets Management Register. This is likely to be Local Transport Plan Highways Maintenance Block, or other Council Capital funding. No 'commuted' sum will be allocated from CRSTS for this.

4.4 Operating Model: projected income and expenditure

Ongoing revenue funding in the form of maintenance, will be funded by Middlesbrough Borough Council, and managed as part of their Assets Management Register. This is likely to be Local Transport Plan Highways Maintenance Block, or other Council Capital funding. No 'commuted' sum will be allocated from CRSTS for this.

It should be noted that the maintenance costs over the 20-year appraisal period will likely be less for the proposed layout, than what currently exists, with a much-simplified highway layout. This will include the removal of 'within carriageway' infrastructure such as WANDS bollards and orcas.

No income will be gained by the TVCA related to the schemes' operation once opened. MBC will receive a projected increased level of parking revenue, related to the reinstatement of parking, previously removed as part of the installation of the cycle scheme. 100% of this increased parking income will be utilised for revenue within their Parking Service, including enforcement and maintenance of Pay and Display Bays.

4.5 What will the TVCA funding be paying for?

Middlesbrough's detailed design cost estimate for the scheme is as follows:

Direct Construction Costs

Base Construction Cost (Q2 2021 Prices) £891,445
Allowance for traffic signals £150,000

Indirect Construction Costs

Main Contractors preliminaries and traffic management £325,000
Overheads and Profits £136,644
Fees (design, construction PM, CDM) £263,041
Benefits Realisation Report £30,000

Total excl. Risk and Inflation £1,796,130

Risk £285,518
Inflation £305,937

Total £591,455

Total cost £2,387,585

Should the tendered costs vary significantly to the above, it is understood that it would need to be discussed with TVCA and may require submission of a Project Change Request and recalculated BCR for assessment.

The proposal will fund:

- the removal on unidirectional on carriageway cycles lanes between Ayresome Street and Borough Road;
- the removal of WANDS bollards and orcas;
- the reinstatement of 47 on street parking bays;
- bus stops are located back to their original positions – kerb side, where as currently there are boarding islands, with cycle lanes located between them and
- the footway;
- realignment of the carriageway to provide a more 'linear' layout, removing the meandering layout, related to the cycle lanes;
- retaining the current 20mph speed limit;
- retaining the closure of King Edwards Square;
- retaining the closure of Victoria Road; and
- removing the current closures of all other side streets, between Borough Road and Ayresome Street.

4.6 Details of other funding routes explored

No other funding routes are available for this project. No private sector funding is available for a such a reinstatement project, which was originally funded using DfT funding secured from the TVCA.

No public sector contribution is available from MBC. This includes a constrained Local Transport Plan allocation from Central Government, and other MBC Capital Funding, which is committed to other funding requirements across the Borough.

There has been a previous commitment from the TVCA Mayor to fully fund the removal of the cycle lane project, which subject to processes and requirements, including the assurance of a Business Case by TVCA and advertisement and approval of Traffic Regulation Orders by MBC as the Highway Authority.

Projected increased parking revenue from the scheme, would not allow for an 'invest to save' type project, with this funding fully required for revenue purposes within MBC's Parking Service.

4.7 Type of financial assistance sought

Approval of £2,387,585 CRSTS capital funding, with relevant elements of this overall cost estimate (£2,169,699), forming a Grant Funding Agreement between TVCA and MBC.

4.8 Financial Return to TVCA

No financial return to TVCA to be generated.

The highway scheme will be adopted by MBC and maintained as part of their Highway Asset Management register, in their role as Highway Authority.

No income will be gained by the TVCA related to the schemes' operation once opened. MBC will receive a projected increased level of parking revenue, related to the reinstatement of parking, previously removed as part of the installation of the cycle scheme. 100% of this increased parking income will be utilised for revenue within their Parking Service, including enforcement and maintenance of Pay and Display Bays.

4.9 Financial Risks

A Quantified Risk Assessment has been undertaken, involving transport officers from both TVCA and MBC, and Highway Design/Engineering Officers from MBC.

Risk management is integral to all projects, as risks can have a considerable effect on the timescale, cost and overall impact of initiatives. The production of a risk register forms an integral task associated with standard project management procedures that are followed by TVCA. The risk register for the Linthorpe Road Highway Reinstatement project will be reviewed regularly throughout the remaining stages of the project by the Delivery team. Any unresolved or significant issues, which are identified, will be escalated through the appropriate governance mechanisms in place outlined in the Management Case.

Risks to delivery to the project were considered within the following categories: (i) preparation and supervision, (ii) land, (iii) strategic and legal, (iv) engineering highway, (v) environmental mitigations, (vi) ground conditions, (vii) construction / contractual, (viii) other work, (ix) delays in commencing construction, (x) delays during construction and (xi) capacity.

The production of a risk register will enable the effective management and communication of potential conflicts, ensuring appropriate mitigation is incorporated. The Risk Register identifies the potential causes and consequences of each risk and will be a 'live' document that is maintained and owned by the Project Manager throughout.

The monetised value of the risks has been quantified by considering the likelihood and probability scores. The scoring system for the risk assessment is shown below:

Key:		Impact					
Extreme Risk		Very Low	Low	Medium	High	Very High	
High Risk							
Moderate Risk							
Low Risk							
Very Low Risk							
		1	2	3	4	5	
Likelihood	Very High (>80%)	5	M	M	H	E	E
	High (51-80%)	4	L	M	H	H	E
	Medium (21-50%)	3	L	L	M	H	H
	Low (6-20%)	2	V	L	L	M	M
	Very Low (0-5%)	1	V	V	L	L	M

Probability Score	1-2	3-5	6-10	11-20	21-25
Probability Percentage	2.5%	12.5%	35.0%	65.0%	90.0%

The main financial risks to the scheme are listed below, with all risks detailed in the accompanying QRA:

- A01 Contractor resource availability - £78,000
- D01 Bill of Quantities incorrect from designer, when measured by the tenderer – £50,000
- D03 All ready defective measures (drainage/structures/street lighting etc), not related to our construction, but found on site - £90,000
- D04 Other unexpected underground apparatus encountered - £60,000
- D05 Unforeseen utilities encountered - £80,000
- D06 Increase in rates, due to events outside of our control – Middle East/Ukraine/etc - £60,000

4.10 Cashflow of Programme / Project

CRSTS funding is devolved to the TVCA from Central Government, with Business Cases assurance undertaken by the TVCA. Therefore, funding is readily available, subject to appraisal and assurance processes.

£140,914 is projected to be spent on design and other preparation costs by the end of 24/25. This also includes surveys and a Road Safety Audit. This activity will be funded directly by the Tees Valley Combined Authority to respective consultants.

£46,971 will be spent in 25/26 on Project Management costs.

Please note the above two costs will be paid directly by TVCA and will not form part of the GFA to MBC.

£2,094,544 is projected to be spent on construction related activities in 25/26, with the construction fully delivered in 25/26. Control of costs throughout scheme development will be achieved through a New Engineering and Construction (NEC) Contract NEC4 Option A (Priced Contract with Activity Schedule). The contract will be between Middlesbrough Borough Council and the appointed Contractor. The Contractor will be paid following the submission of monthly claims. The Contractor will submit monthly claims to the Construction Project Manager and a Quantity Surveyor. They will validate the claims, comparing progress on site, and make a recommendation to MBC to proceed. Following approval, payment will be made to the Contractor directly via MBC's finance system within 28 days of issue of the initial valuation. The tender returns and subsequent contract will detail a monthly spend profile, which will be then managed.

MBC will be responsible for cashflow and will make quarterly claims in arrears to TVCA in line with TVCA's standard claims process.

£75,155 is projected to be spent on construction project management fees in 25/26. An external Construction Project Manager will be appointed via Middlesbrough, in the form of a NEC Professional Services Contract. The contract will detail monthly costs, which will be paid directly by MBC's finance system within 28 days of the respective claim. Quarterly claims will be made from MBC to TVCA.

£5,000 per annum (six years) will be spent on monitoring the project from 25/26 to 30/31. This will include a scheme opening report in 25/26, and then annual reports from 26/27, reflecting this will be the first full operational year. This activity will be funded directly by the Tees Valley Combined Authority.

The monitoring costs will be funded directly by TVCA and will not form part of the GFA to MBC.

The detailed financial profile will be available upon appointment of the Contractor, but it is anticipated the works will likely take circa 7 months from May to December 25, with likely consistent monthly payments across this period. This reflects the consistent nature of the works, along the length of Linthorpe Road, between Borough Road and Ayresome Street. The referenced works duration considers the programme for the original installation of the cycle lane infrastructure.

The proposed total GFA amount to MBC will therefore be £2,169,699.

Compensation Event Process

A QRA has been produced in line WebTAG Unit A1.2, producing a risk-adjusted cost estimate. The QRA is in the form a 5x5 calculation, with the value to go in the estimate, based on a median mitigated risk value. The QRA considers all foreseen risks identified during the development and design of the project.

If risks are realised on site and need to be drawn down against the QRA value, then these will be reported to TVCA for approval. Any unutilised element of the QRA allowance will be returned to TVCA.

TVCA's funding terms are that where it provides a grant to anyone who deliver a scheme, it cannot take responsibility for cost overruns over and above a quantified risk allowance which has formed part of the business case. However any exceptional risks and related financial implications realised during construction, which could not have been identified as part of the development of the scheme and quantified risk assessment process, can be reported back to the Tees Valley Combined Authority for consideration.

Any new risks not identified in the QRA, but can be contained within the original QRA value, may be claimed subject to TVCA approval.

CASE 5: MANAGEMENT CASE

Key Messages:

Governance structure put in place allowing upwards reporting and identifying responsibilities of individual officers within both MBC and TVCA, and identifying the interfaces between the two organisations.
A workable and sequential programme in place.
Issue and risk, stakeholder communication and benefits monitoring processes identified.

5.1 Delivery Partners

TVCA the promotor and funder of the scheme.

MBC to undertake Highway Authority processes, including technical approvals, Overseeing Organising duties for RSA 1-2, advertiser of TROs and procuring the works onto site.

Grant Funding Agreement between TVCA and MBC, detailing terms and conditions of funding to deliver the proposals

5.2 Formal Partnership Arrangements

Grant Funding Agreement between TVCA and MBC, detailing terms and conditions of funding to deliver the proposals

5.3 Details of Programme / Project Management Structure

Tom Bryant (Director for Infrastructure) is the TVCA Senior Responsible Owner. Tom will be responsible for making final decisions on Compensation Event claims from MBC and their respective Contractor. Tom is also ultimately responsible for the assurance of the Business Case and production of the Grant Funding Agreement.

Craig MacLennan (Senior Major Projects Manager) has been responsible for ensuring the detailed design has been produced. Craig will receive Compensation Events claims from Middlesbrough/their Contractor and make recommendations to Tom for the approval or not. Craig will be also responsible for producing Highlight and

Exception Reports, based on information from Middlesbrough and their NEC Project Manager, and report upwards to Tom and the TVCA Projects Board as required.

An NEC Project Manager and Quantity Surveyor will work client side to MBC, and assess progress on site and be the point of contact for the contractor. This person will be the site representative, dealing with design and construction interfaces. They will also receive and assess Compensation Events and Claims from the contractor, and make recommendations on whether they should be paid. These will be reported to TVCA for approval.

Chris will be both the internal point of contact within Middlesbrough and also the interface with TVCA. Chris will be responsible for producing both Highlight Reports and Exception Reports on a monthly basis to TVCA, working alongside the NEC Project Manager, which will also be reported to Middlesbrough's Capital programme Board. Chris will be responsible for ensuring the Traffic Regulation Orders happen to schedule, and also producing the Executive Committee Report, to initiate the TROs.

Chris will be responsible for dealing with any processes and reports associated with TRO objections.

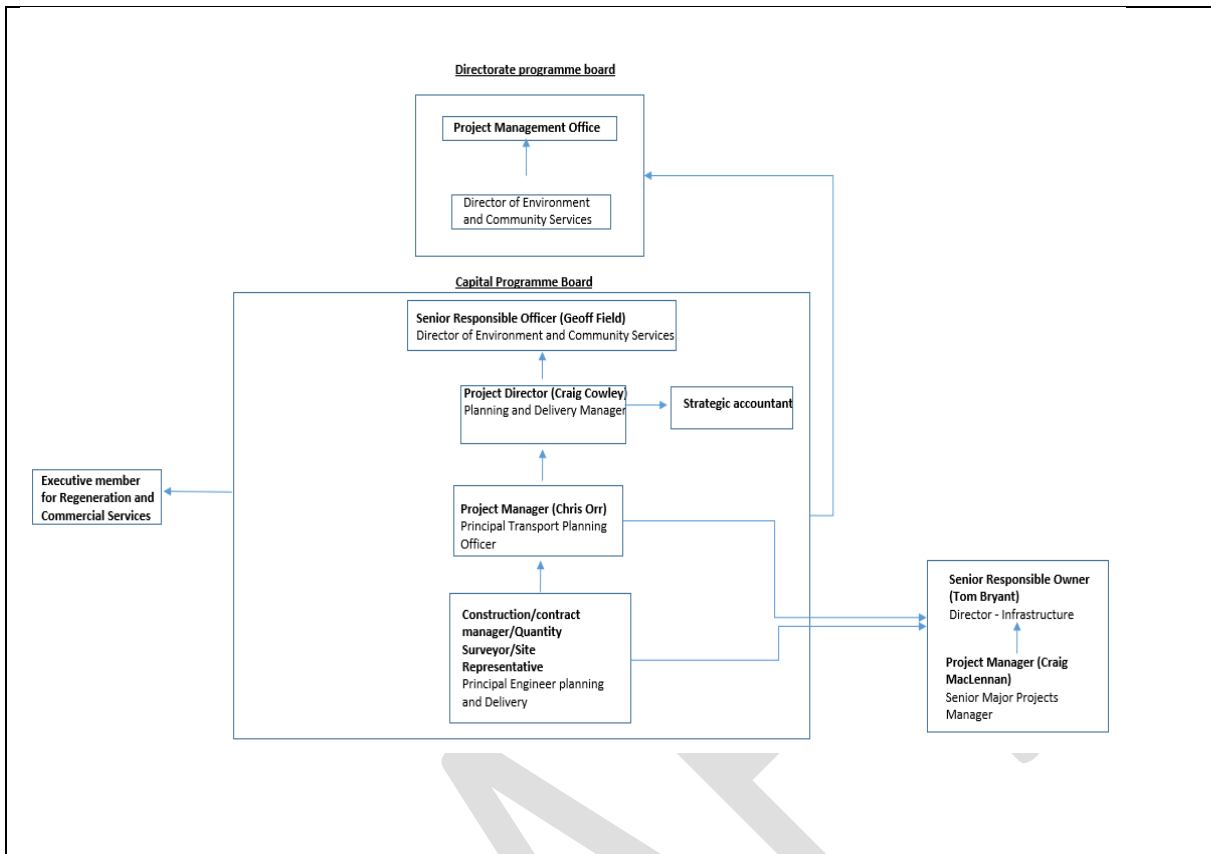
Craig Cowley is the Project Director, and will be responsible for decision making, which can't be made by the Project Manager and NEC Construction Manager. This will be either be decisions taken by himself, or as required in dialogue with the Strategic Accountant, Executive Member or Senior Responsible Officer. Craig will ensure briefings are held on a regular basis as required with these representatives.

Geoff Field is the Senior Responsible Officer, who is the chair of the Capital Programme Board, and is ultimately responsible for the delivery of the project to programme and budget. Geoff will make decisions, which are unable to be taken at a Project Manager or Project Director level.

A Directorate Programme Board will also have oversight of the project, which the Director of Regeneration is a member of.

The Capital Programme Board and Directorate Programme Board will both meet monthly.

The governance arrangements put in place enable a clear upwards reporting and decision making process, but also clearly identifies the roles of individual officers within both TVCA and MBC, and how they interface with each other.



5.4 Issue and Risk Management Processes

A QRA has been produced in line WebTAG Unit A1.2, producing a risk-adjusted cost estimate. The QRA is in the form a 5x5 calculation, with the value to go in the estimate, based on a median mitigated risk value. The QRA considers all foreseen risks identified during the development and design of the project.

If risks are realised on site and need to be drawn down against the QRA value, then these will be reported to TVCA for approval. Any unutilised element of the QRA allowance will be returned to TVCA.

TVCA's funding terms are that where it provides a grant to anyone who deliver a scheme, it cannot take responsibility for cost overruns over and above a quantified risk allowance which has formed part of the business case. However any exceptional risks and related financial implications realised during construction, which could not have been identified as part of the development of the scheme and quantified risk assessment process, can be reported back to the Tees Valley Combined Authority for consideration.

Any new risks not identified in the QRA, but can be contained within the original QRA value, may be claimed subject to TVCA approval.

5.5 Key Milestones	
Key Milestone Description	Expected Completion Date
Project start date	1st July 2024
Civils design complete	October 2024 - complete
Traffic signals design complete	February 2025
Appraisal of Business Case	December 2024
Consideration of Business Case recommendation by Tees Valley Management Group	January 2025
Approval to proceed with Permanent Traffic Regulation Orders by MBC Executive Committee	5 th February 2025
Traffic Regulation Order Advertisement	17 th February to 30 th March 2025
Tender	14 th April to 23 rd May 2025
Contract let	30 th May 2025
Work starts on site	End of May 2025
Work completes on site	December 2025
Site open for end user	December 2025
Financial completion	March 2026
Year 1 Monitoring Report	31 st March 2027
Year 2 Monitoring Report	31 st March 2028
Year 3 Monitoring Report	31 st March 2029
Year 4 Monitoring Report	31 st March 2030
Year 5 Monitoring Report	31 st March 2031
5.6 Stakeholder Management, Communications and Marketing Plan	
<p>Consultation occurred during August 2024, Led by MBC. TVCA promoted and shared the consultation in a variety of ways to encourage local communities to respond and feedback:</p> <ul style="list-style-type: none"> • Consultation information and survey available online via the Tees Valley Combined Authority website • Social media content shared by TVCA • Political Briefing Notes • Bus Operator Briefing Notes; and • Stakeholder briefings held (where requested) <p>A significant number of responses were received – 390.</p> <p>TVCA shared a briefing note outlining the proposal to remove the cycle lanes on Linthorpe Road with both Stagecoach and Arriva, the operators of bus services along this route. Stagecoach strongly advocated for the removal of the cycle lane, citing significant delays to their services entering Middlesbrough due to the current road layout.</p>	

The operators support reinstating key bus stops, including at The Crown/King Edward Square, Granville Road, and outside One Life Medical Centre. They also urged a return to a wider highway gauge to address safety concerns.

The same stakeholder groups and frontages will be contacted during the TRO advertisement period.

Required construction phasing will be developed into the tender by MBC. This will take into consideration access and loading requirements, as well as bus operations.

A website will be developed collaboratively between MBC and TVCA Comms and Marketing Team advising of construction progress. MBC and TVCA will also provide regular updates via social media and the press.

The NEC project manager will be the point of contact for stakeholders, with a dedicated email address and telephone number available.

5.7 Social Value Generation: Environmental and Social Impacts

NEPO works in partnership with our members, suppliers, and other key stakeholders to deliver positive social value outcomes. NEPO's social value programme enables the suppliers to use their skills, knowledge, opportunities and resources to make a difference for the communities, residents and environment across the North East region.

Whilst the Procurement Act 2023 has brought into effect a new regime and ways of working, NEPO has continued to work with Members to leverage tender processes to get the added benefits and impacts needed to support local economic and environmental challenges. NEPO's new ESG model will be finalised in 2024 and will enable a stronger partnership with our Member Authorities and suppliers to maximise social value benefits.

TVCA and MBC Procurement teams will identify a list of social value outcomes it wishes to take forward into the procurement and will work with NEPO to embed these within the tender documents.

Themes will include:

Economic prosperity:

Directly contributing to economic growth including suppliers, local spend, new businesses, small-to-medium-size enterprises, micro, voluntary, charitable and social enterprises/mutuals;

Contributing to apprenticeships, skills and education, and regional economic prosperity

Improving the environment:

Net Zero considerations in construction and materials;

Respecting neighbouring residences by reducing our impact on air quality, noise and water quality.

Community wellbeing

Improving community amenities;
Supporting community education and heritage programmes

Equality, diversity and inclusion

Contributing to employment and opportunities for women; ethnic minorities; the long-term unemployed; veterans; people with disabilities; the LGBTQ+ community; care givers; school leavers; young people not engaged in education, employment or training; ex-offenders; the homeless; maternity returners; new to industry; flexible workers; Apprenticeship and training opportunities.

5.8 Monitoring and Evaluation Plan

An allocation has been made within the cost estimate, to monitor and evaluate the benefits of the project. This will allow SMART benefits to be measured upon completion and during operation. The TVCA Project Manager, working alongside MBC will be responsible for this.

	Out turn Cost v Budget	Completion v Programme	On-street parking bay usage and revenue	Retail Unit occupancy	Pay and Display Bay User numbers	Cyclist numbers	Bus journey times/delays
Upon Completion	X	X					
1 Year after Opening			X	X	X	X	X
2 Years after Opening			X	X	X	X	X
3 Years after Opening			X	X	X	X	X
4 Years after Opening			X	X	X	X	X
5 Years after Opening			X	X	X	X	X

5.9 Exit Strategy

The proposed highway changes will be adopted by MBC, as part of their adopted highway. Ongoing revenue funding in the form of maintenance, will be funded by Middlesbrough Borough Council, and managed as part of their Assets Management Register. This is likely to be Local Transport Plan Highways Maintenance Block, or other Council Capital funding. No 'commuted' sum will be allocated from CRSTS for this.

MBC will receive a projected increased level of parking revenue, related to the reinstatement of parking, previously removed as part of the installation of the cycle scheme. 100% of this increased parking income will be utilised for revenue within their Parking Service, including enforcement and maintenance of Pay and Display Bays.

APPLICANT SIGN OFF

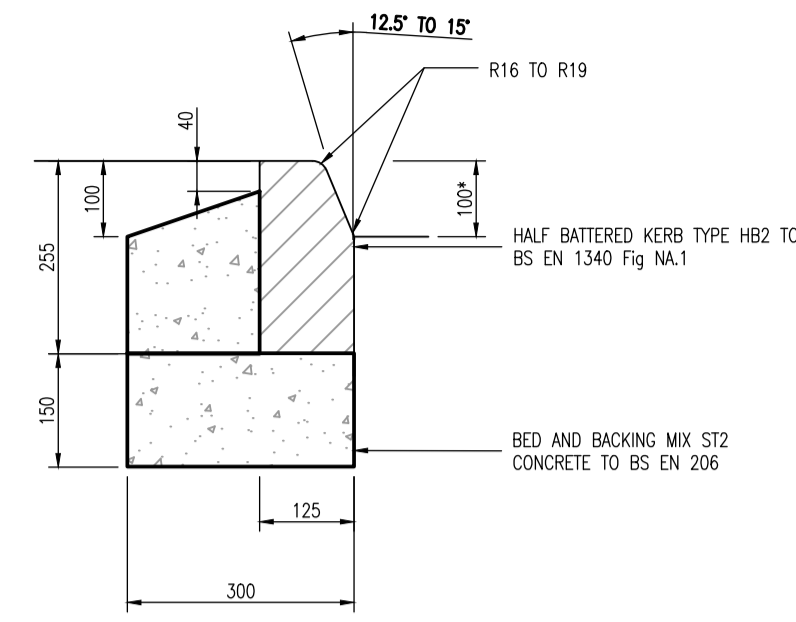
TO BE SIGNED BY PROJECT SPONSOR PRIOR TO SUBMISSION FOR APPRAISAL

I declare that to the best of my knowledge and belief, the information given within this application form is correct. I understand that acceptance of this application form does not in any way signify that Tees Valley Combined Authority has agreed to invest. I declare I am an authorised signatory of the lead organisation, with the authority to sign off this application form and have notified the necessary senior finance officers.

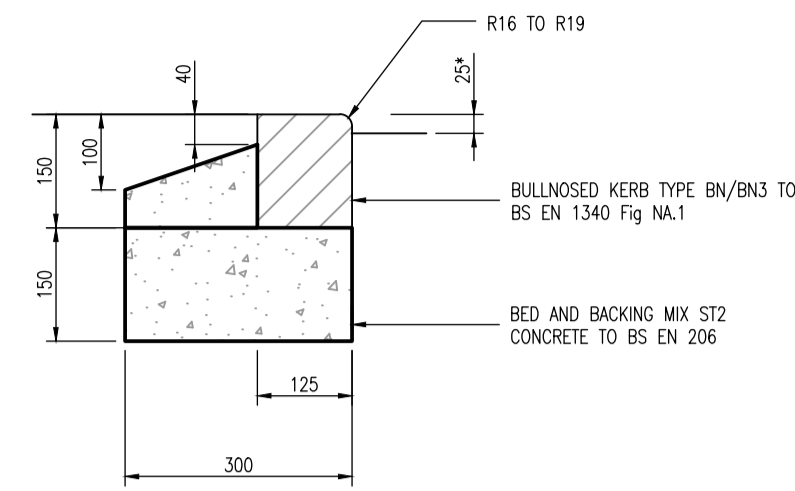
FREEDOM OF INFORMATION

I understand the requirements of the Freedom of Information Act 2000. I understand that Tees Valley Combined Authority is subject to the provisions of the Freedom of Information Act 2000 ("the Act"). The Act provides for information to be exempt from the general right of access if its disclosure would, or would be likely to, prejudice the commercial interests of any person. A Project Sponsor may request that certain information in their submission and any subsequent Agreement is treated as covered by this exemption. However, if the information is requested the availability of this exemption will be subject to a test of whether the public interest lies in disclosing the information or keeping it confidential. We understand that while Tees Valley Combined Authority will endeavour to take into account the Project Sponsor's views as to the keeping of information confidential, it reserves the right to disclose information if required to do so.

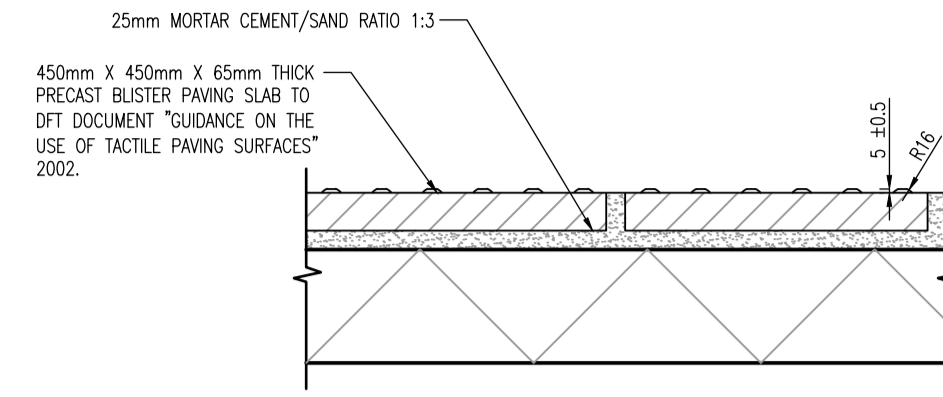
Signature:	<i>Tom Bryant</i>
Name:	Tom Bryant
Position / Title:	Director - Infrastructure
Company / Organisation:	Tees Valley Combined Authority
Date:	15/12/2024



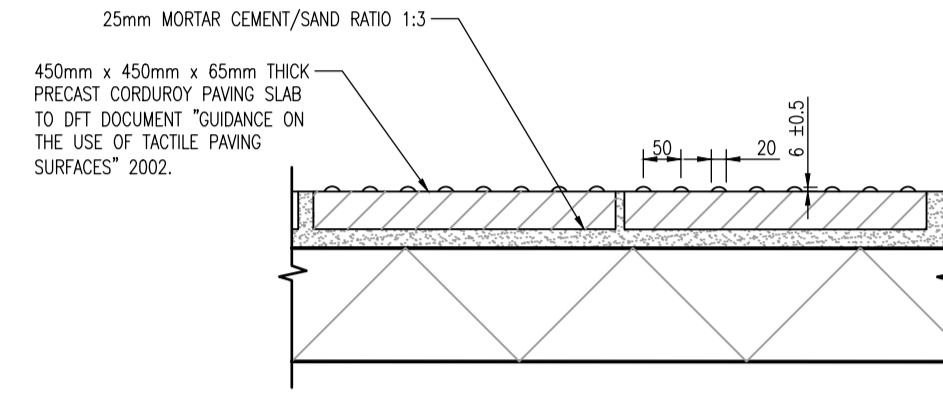
HB2 TYPE KERB
1:10 SCALE
(*UPSTAND HEIGHT IS SUBJECT TO CHANGE)



BN3 TYPE KERB
1:10 SCALE
(*UPSTAND HEIGHT IS SUBJECT TO CHANGE)

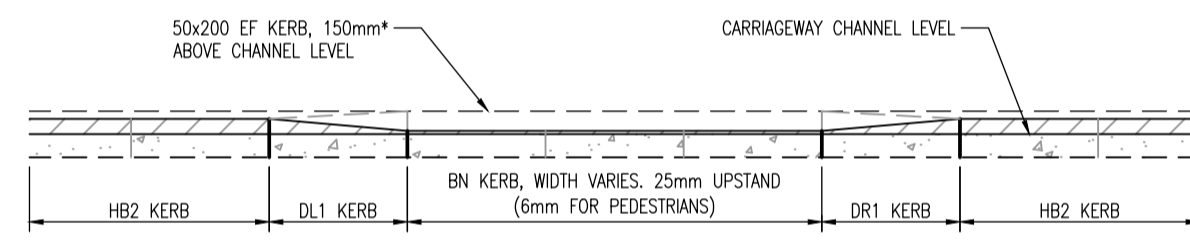


TACTILE PAVING - BLISTER SLAB UNITS
1:10 SCALE

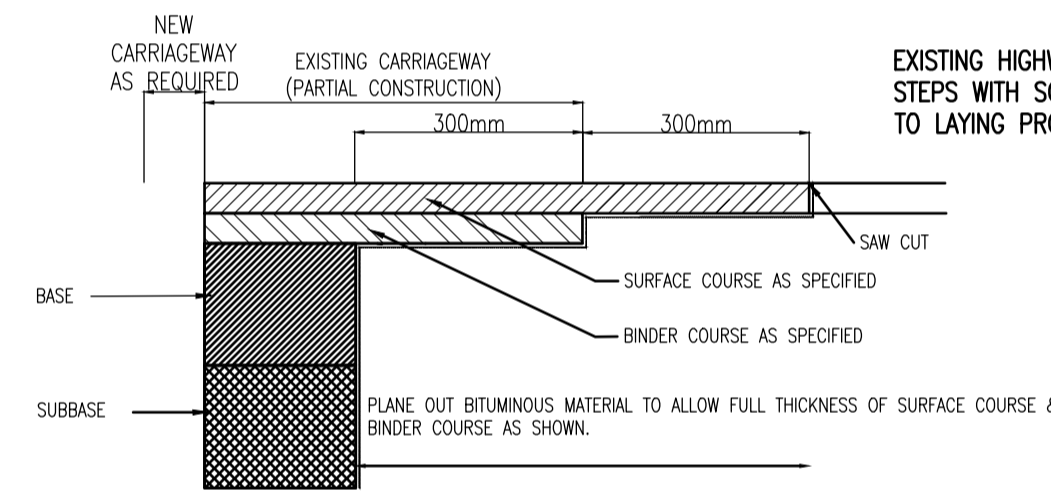


TACTILE PAVING - CORDUROY SLAB UNITS (HAZARD WARNING)
1:10 SCALE

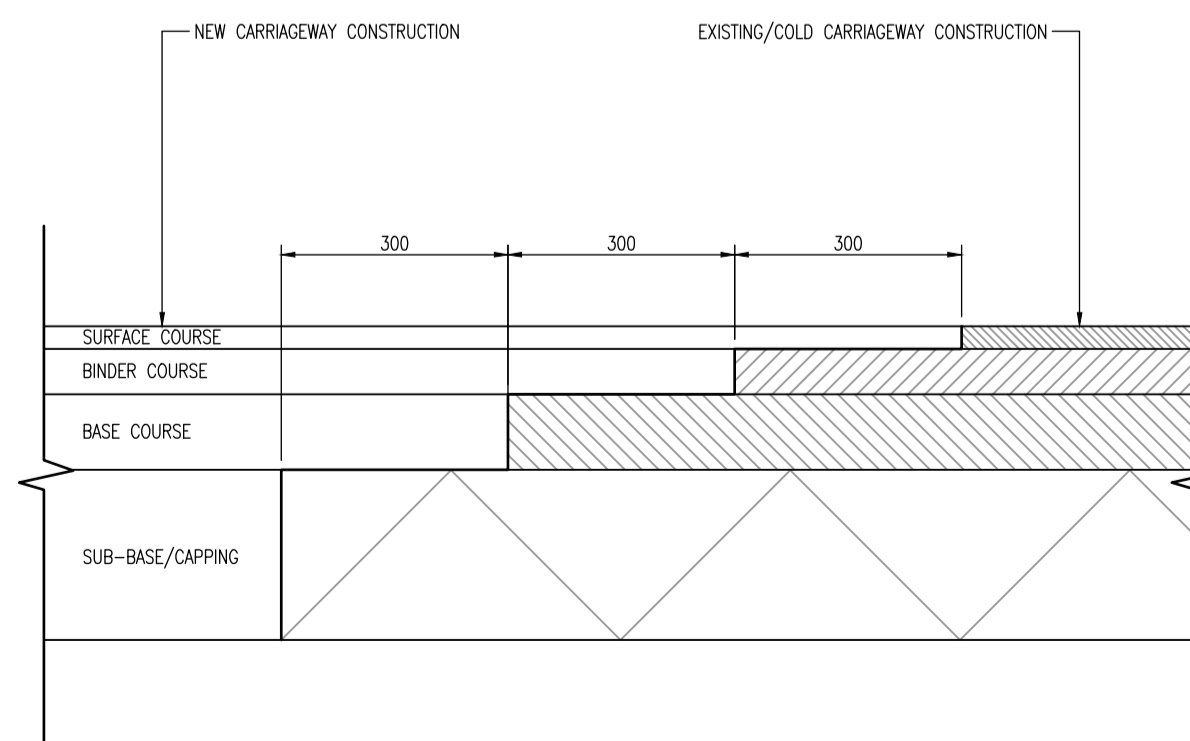
PRIOR TO LAYING SUBSEQUENT LAYERS OF BITUMINOUS MATERIAL THE SURFACE SHALL BE THOROUGHLY CLEANED AND A TACK COAT OF BITUMEN EMULSION CLASS K1-40 TO BS434 SHALL BE APPLIED AT A RATE OF 0.4 TO 0.6 LITRES PER SQUARE METRE TO ANY BITUMINOUS SURFACE THAT HAS BEEN LEFT UNCOVERED FOR MORE THEN THREE DAYS



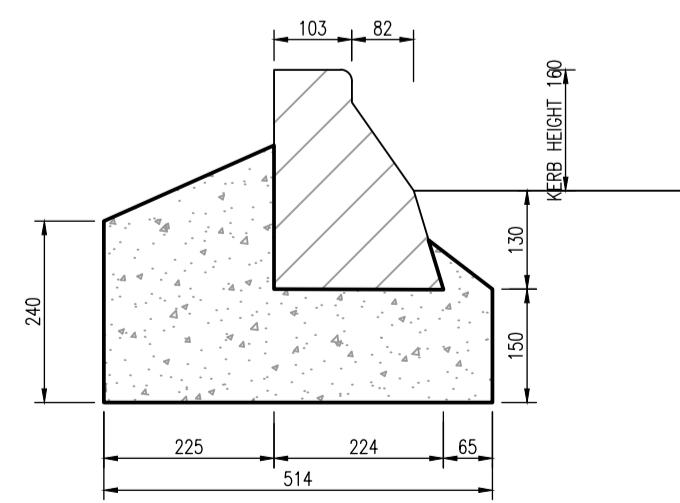
ELEVATION TYPICAL DROP KERB ARRANGEMENT
1:50 SCALE
(*DIMENSIONS TO BE ADJUSTED TO SUIT SITE LAYOUT/DESIGN. REFER TO EXTERNAL WORKS DESIGN LAYOUTS AND SPECIFICATION)



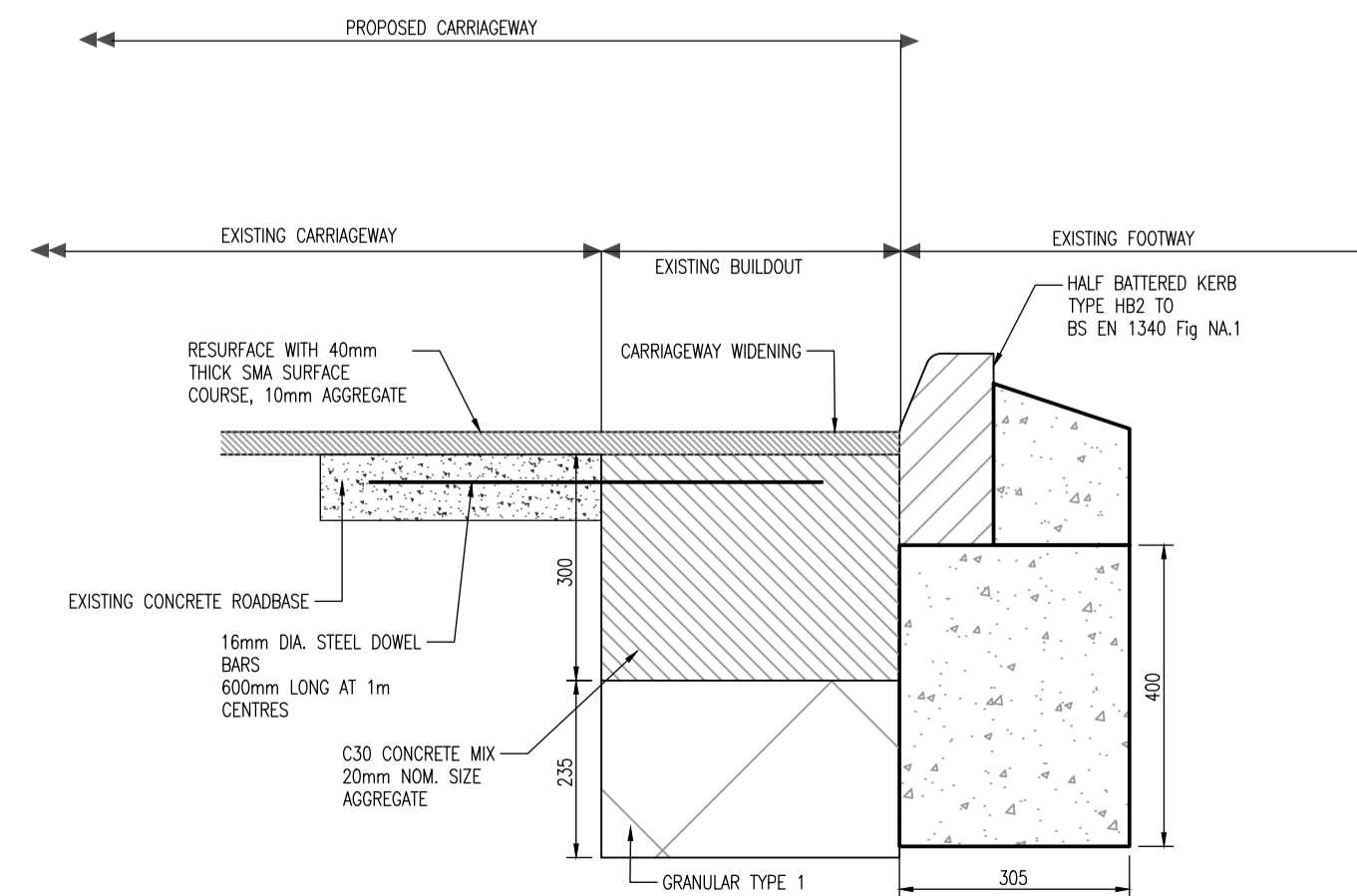
ROAD PAVEMENT TIE-IN CONSTRUCTION
NARROW WIDTHS
(SCALE 1:10)



LONGITUDINAL CARRIAGEWAY JOINT
1:10 SCALE
VERTICAL FACES OF COLD BITUMINOUS MATERIALS TO BE PAINTED WITH HOT BITUMINOUS BINDER OF NOT LESS THAN 40/60 PEN. ALL HORIZONTAL OR INCLINED EXISTING SURFACES TO ACCEPT BITUMINOUS MATERIALS TO BE PREPARED WITH A BOND COAT IN ACCORDANCE WITH BS 594987



160MM ACCESS KERB BY CHARCON
STANDARD PROFILE WITHOUT MARKER BUMP
(SIDE VIEW)
1:10 SCALE



TYPICAL CROSS SECTION
1:10 SCALE

NOTES

- THIS DRAWING SHOULD BE READ IN CONJUNCTION WITH ALL RELATED DOCUMENTATION AND DRAWINGS.
- ALL DIMENSIONS ARE IN MILLIMETRES UNLESS STATED OTHERWISE.
- THE WORKS SHALL BE CARRIED OUT IN ACCORDANCE WITH THE DFT MCHW, DMRB AND MIDDLESBROUGH COUNCIL REQUIREMENTS.
- REFER TO THE KERBS, FOOTWAYS AND PAVED AREAS DRAWINGS FOR SITE SPECIFIC DETAILS OF KERBING AND SURFACING
- KERBS TO BE BUTT JOINTED. WHERE KERB ALIGNMENTS INCUR MINOR OPEN JOINT TAPERS IN EXCESS OF 5mm BETWEEN FORMED SURFACES, MORTAR JOINTING SHALL BE USED.
- FULL LENGTH PCC KERBS ARE TO BE MECHANICALLY HANDLED.
- ALL PRECAST KERBS, CHANNELS AND EDGINGS SHALL BE TO BS EN 1340 AND LAID TO BS 7533-6 AND ALL SETTS OF NATURAL STONE SHALL BE TO BS EN 1342 AND LAID TO BS 7533-7.
- ALL KERBS, CHANNELS AND EDGINGS BED & BACKING SHALL BE ST2 CONCRETE WELL COMPACTED. UNLESS OTHERWISE STATED, THIS SHALL BE A MINIMUM BED THICKNESS OF 150mm FOR KERBS AND CHANNELS, 100mm FOR EDGINGS.
- THE MINIMUM CUT OF ANY LENGTH OF KERB SHALL BE 450mm.
- THE CORRECT TRANSITION SHALL BE USED FOR ALL DROP KERBS AND UPSTAND VARIATION LOCATIONS.
- KERBS SHALL BE LAID ON BEDDING WHILST CONCRETE IS STILL "GREEN", WHERE THE BEDDING CONCRETE HAS CURED PRIOR TO INSTALLING THE FINISHED KERBING, 20mm NOMINAL THICKNESS CEMENT MORTAR BED DESIGNATION (f) SHALL BE USED. N.B. THE MINIMUM BEDDING THICKNESS OF 150mm SHALL BE MAINTAINED.

DO NOT SCALE

- TACTILE PAVING SHALL BE COLOURED RED AT CONTROLLED CROSSING POINTS AND BUFF AT UNCONTROLLED CROSSINGS.
- MORTAR FOR TACTILE PAVING BE AS PER THAT SPECIFIED FOR BLOCK PAVING. CHLORIDE ION CONTENT OF MORTAR SHALL BE IN ACCORDANCE WITH BS EN 1744-1 AND SHALL NOT EXCEED 0.3% MASS OF CEMENT (0.2% FOR SULPHATE RESISTANT CEMENT. CALCIUM CHLORIDE IS NOT ACCEPTABLE).
- NO FROST-SUSCEPTIBLE MATERIALS TO BE USED WITHIN 450mm OF THE FINISHED SURFACE UNLESS EXPRESSLY REQUIRED BY THE ENGINEER.
- WHERE THE TRIMMING OF BLOCKS IS NECESSARY, PORTIONS OF LESS THAN ONE THIRD OF AN ENTIRE BLOCK SHALL NOT BE LAID. INSTEAD THE PATTERN SHOULD BE ALTERED TO ENABLE LARGER PIECES OF BLOCK TO BE USED.
- IF THE DEPTH OF AN ACCESS COVER FRAME PROHIBITS BLOCKS FROM BEING LAID UP CLOSE TO THE FRAME IT SHOULD BE SURROUNDED IN CONCRETE GRADE ST2 UP TO FINISHED GROUND LEVEL.
- JOINTING MATERIAL SHALL BE 0.5mm DRIED FREE FLOWING SILICA TO BS 7533-3 TABLE D.4. JOINT FILLING SHALL TAKE PLACE IMMEDIATELY AFTER THE BLOCK PAVERS HAVE BEEN COMPACTED INTO THE LAYING COURSE, AND COMPLETED WITH A FINAL COMPACTION OF THE FINISHED SURFACE.
- ASPHALTIC CONCRETE MATERIALS ARE TO BE IN ACCORDANCE WITH THE RELEVANT SECTION WITHIN BS EN 13108 PARTS 1-21, WITH ADDITIONAL GUIDANCE PROVIDED WITHIN PD 6691.
- INSTALLATION OF ASPHALTIC SURFACES TO BE IN ACCORDANCE WITH BS 594987.
- APPLICATION OF BOND COATS SHALL BE APPLIED IN COMPLIANCE WITH BS 594987 CL3.5 AND APPLIED AT A UNIFORM RATE OF SPREAD. TACK COATS SHALL NOT BE ACCEPTED, UNLESS AGREED IN ADVANCE WITH THE ENGINEER.
- COLD SURFACES OF ASPHALT AT TIE-INS TO BE CLEANED PRIOR TO APPLICATION OF BOND COAT TO ENSURE ADEQUATE COHESION OF NEW MATERIALS.
- IN THE AREAS OF NEW CARRIAGEWAY CONSTRUCTION, WHERE THE WIDTH OF CARRIAGEWAY IS LESS THAN 1.0M, CONCRETE FILL TYPE ST1 / ST2 OR OTHER SIMILAR SPECIFICATION AS APPROVED BY MIDDLESBROUGH COUNCIL TO BE USED AS A REPLACEMENT FOR TYPE 1 SUBBASE.
- CONFLICTING INFORMATION SHOWN ON THE ENGINEER'S DRAWINGS OR DISCREPANCIES BETWEEN THE INFORMATION GIVEN BY THE ENGINEER AND THAT PROVIDED BY OTHERS MUST BE REFERRED TO THE ENGINEER BEFORE THE WORKS COMMENCE.

UNTIL TECHNICAL APPROVAL HAS BEEN OBTAINED FROM THE RELEVANT LOCAL AUTHORITIES OR STATUTORY BODIES, IT SHOULD BE UNDERSTOOD THAT ALL DRAWINGS ARE ISSUED AS PRELIMINARY AND NOT FOR CONSTRUCTION. SHOULD THE CONTRACTOR AND / OR EMPLOYER COMMENCE WORK PRIOR TO APPROVAL BEING GIVEN, IT IS ENTIRELY AT THEIR OWN RISK

PO2	31/07/24	NR	ISSUED FOR CLIENT APPROVAL	JS	PWW
PO1	23/07/24	NR	ISSUE FOR COSTING	JS	PWW
DR	16/07/24	NR	DRAFT ISSUE	JS	PWW
REV	DATE	BY	DESCRIPTION	CHK	APP

DRAWING STATUS:
S2 - FOR INFORMATION

Amber Court, William Armstrong Drive, Newcastle upon Tyne, NE4 7YQ, UK
T+ 44 (0) 191 226 2000, F+ 44 (0) 191 226 2104
wsp.com

CLIENT:

SITE/PROJECT:
LINTHORPE ROAD RESTORATION

TITLE:
TYPICAL HIGHWAY DETAILS

SCALE @ AT:	CHECKED:	APPROVED:
NTS	JS	PWW
PROJECT NO:	DESIGNED:	DRAWN:
2024UK293315	NR	NR
	DATE:	
	JULY 2024	

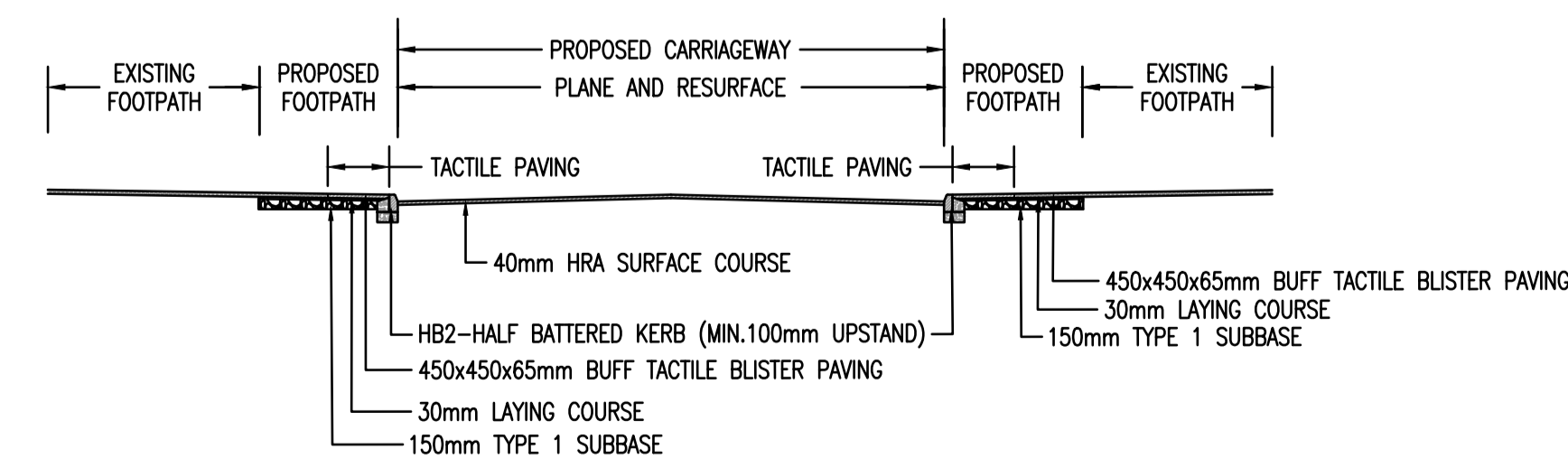
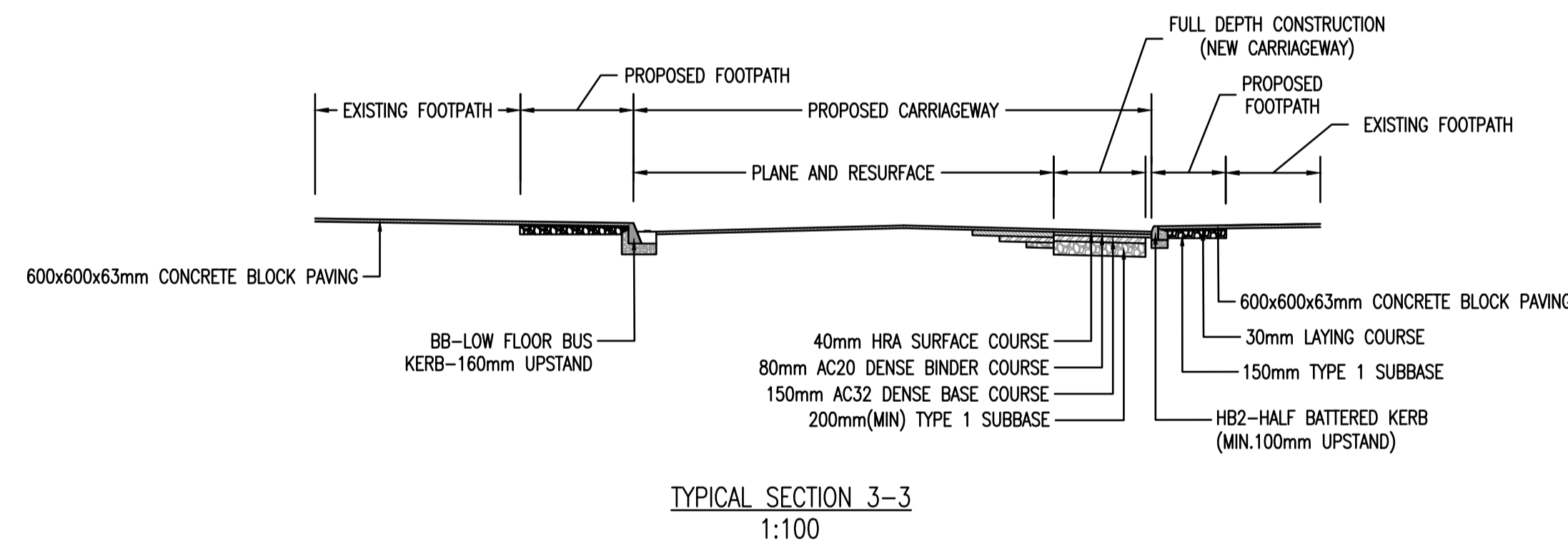
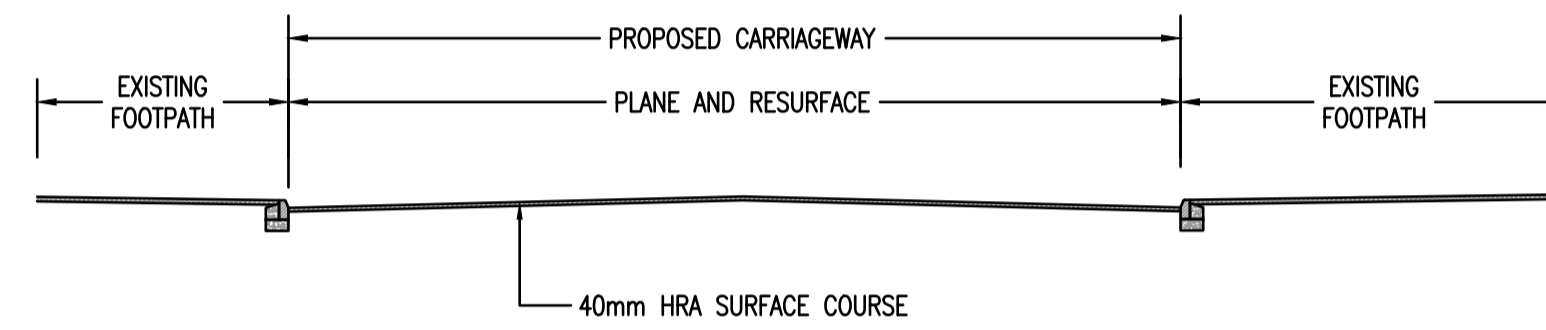
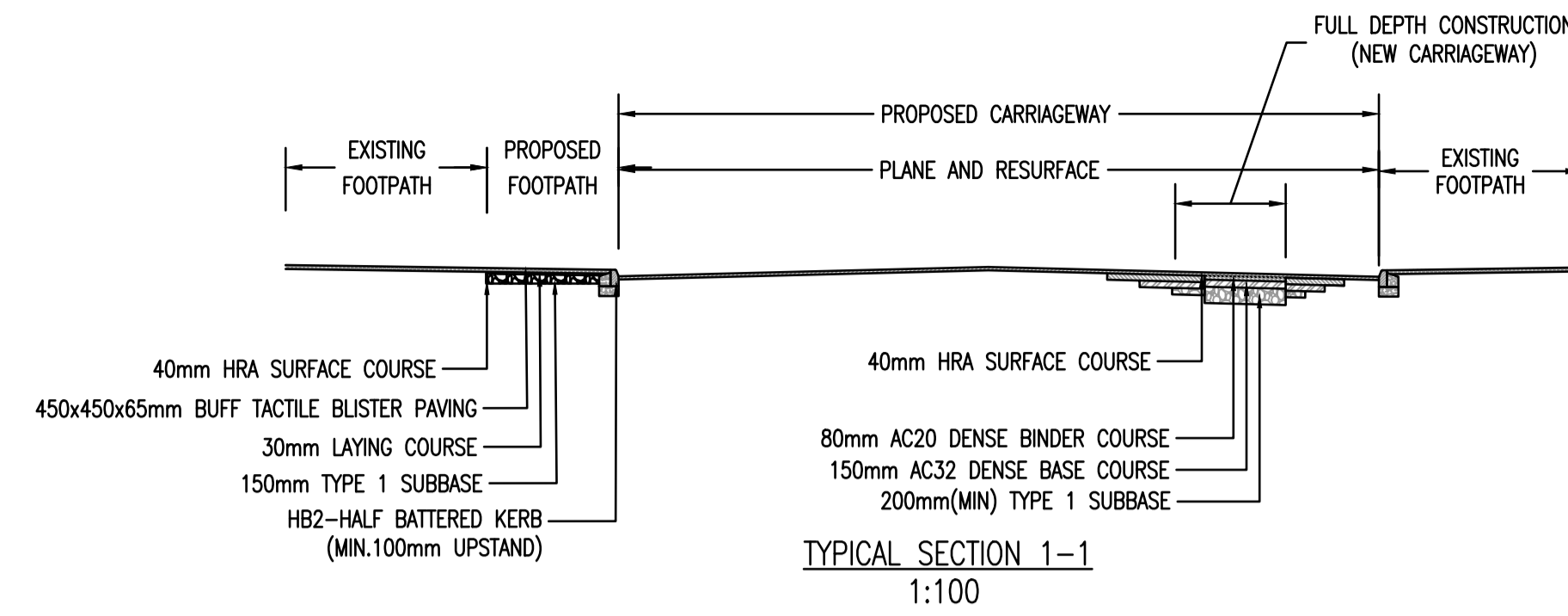
DRAWING NO:	REV:
288933-WSP-XX-XX-DR-C-1111	P02

© WSP UK Ltd

DO NOT SCALE

NOTES

1. ALL DIMENSIONS ARE IN METRES UNLESS NOTED OTHERWISE.
2. FOR SECTION LOCATIONS, REFER DRAWING NO. 288933-WSP-XX-XX-DR-C-000101 - 000104.
3. THIS DRAWING SHOULD BE READ IN CONJUNCTION WITH ALL RELEVANT DOCUMENTATION DRAWINGS AND STANDARD DETAILS BUT WITH PARTICULAR REFERENCE TO THE FOLLOWING 288933-WSP-XX-XX-DR-C-1101.



UNTIL TECHNICAL APPROVAL HAS BEEN OBTAINED FROM THE RELEVANT LOCAL AUTHORITIES OR STATUTORY BODIES, IT SHOULD BE UNDERSTOOD THAT ALL DRAWINGS ARE ISSUED AS PRELIMINARY AND NOT FOR CONSTRUCTION. SHOULD THE CONTRACTOR AND / OR EMPLOYER COMMENCE WORK PRIOR TO APPROVAL BEING GIVEN, IT IS ENTIRELY AT THEIR OWN RISK

PO2	31/07/24	NR	ISSUED FOR CLIENT APPROVAL	JS	PWW
PO1	23/07/24	NR	ISSUE FOR COST ESTIMATE	JS	PWW
DR	10/07/24	NR	DRAFT ISSUE	JS	PWW
REV	DATE	BY	DESCRIPTION	CHK	APP

DRAWING STATUS:



Amber Court, William Armstrong Drive, Newcastle upon Tyne, NE4 7YQ, UK
T+ 44 (0) 191 226 2000, F+ 44 (0) 191 226 2104
wsp.com

CLIENT:



SITE/PROJECT:

LINTHORPE ROAD RESTORATION

TITLE:

TYPICAL HIGHWAY SECTIONS

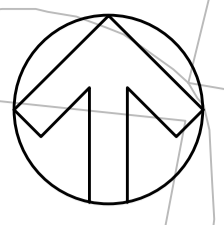
SCALE @ A1: AS SHOWN CHECKED: JS APPROVED: PWW

PROJECT NO: 2024UK293315 DESIGNED: NR DRAWN: NR DATE: JULY 2024

DRAWING NO: 288933-WSP-XX-XX-DR-C-1112 REV: P02



File name: C:\USERS\JIN\S20760\WSP\0367024\288933\TYPICAL SECTIONS\DWG\DOCUMENTS\DWG\MPCV\CIVIL\ENGINEERING\03\DRAWINGS\288933-WSP-XX-XX-DR-C-001112 - TYPICAL SECTIONS.DWG, printed on 31 Jul 2024 16:45:54, by Shanghai_Tup



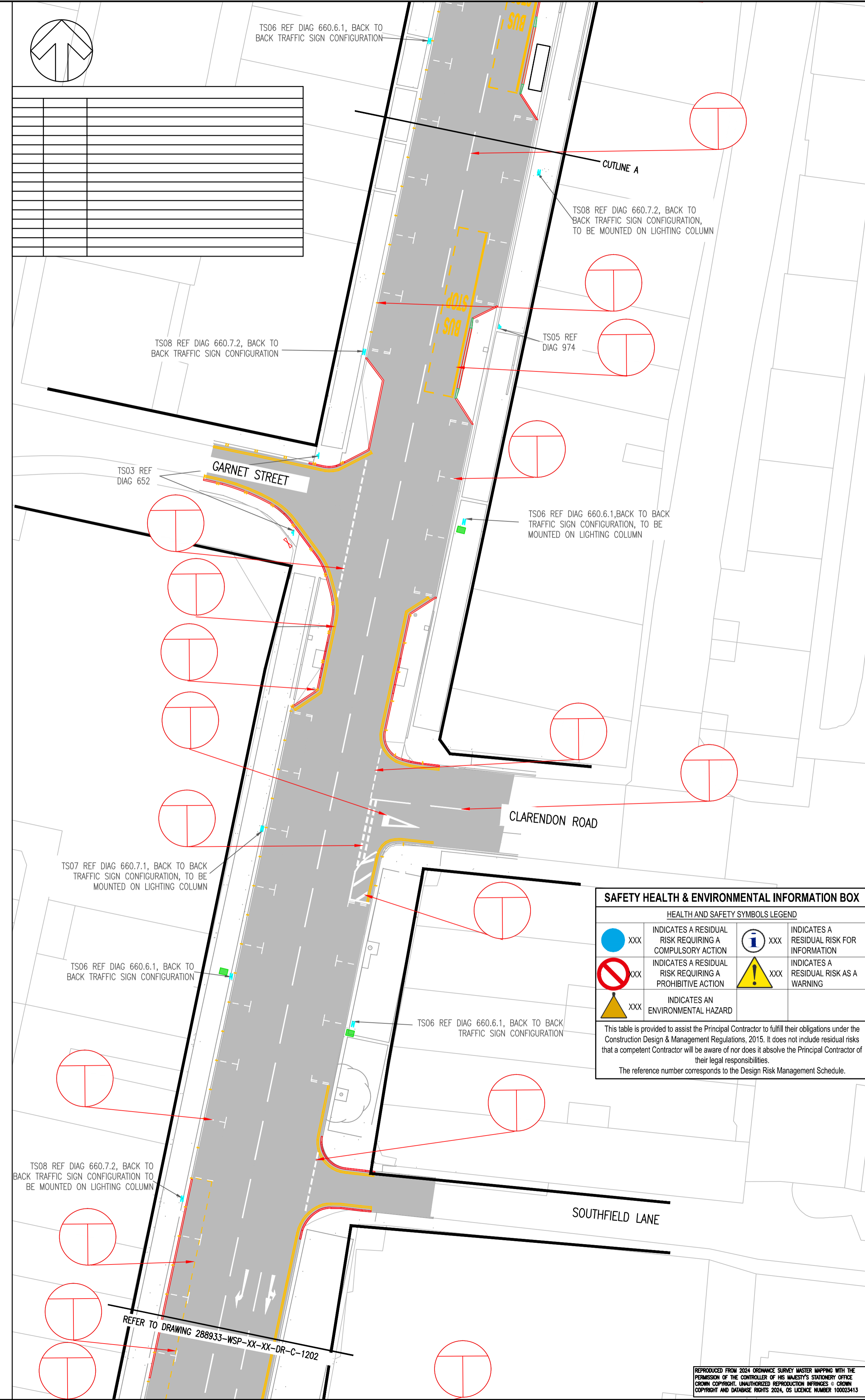
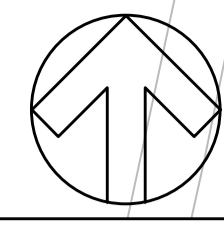
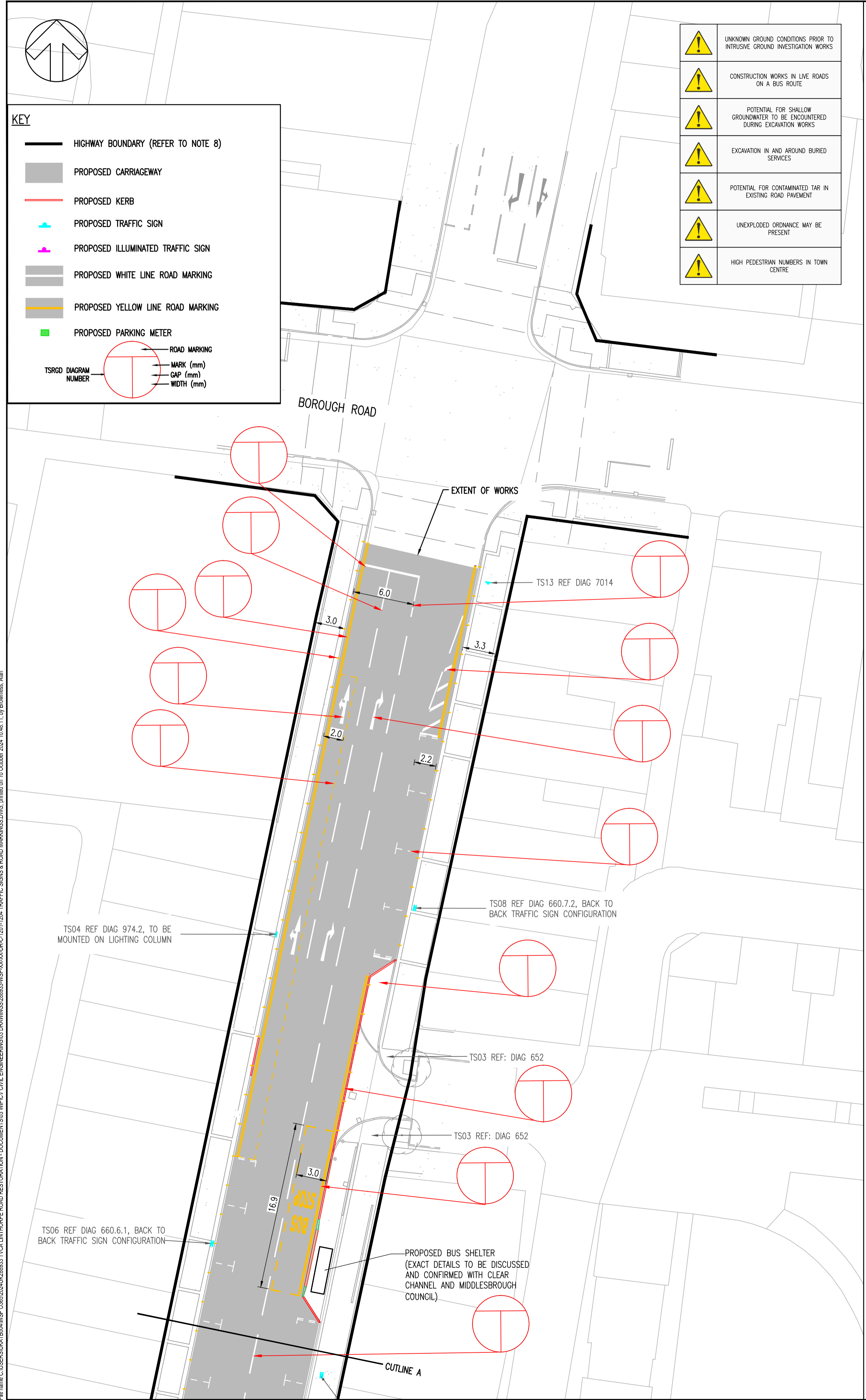
KEY

- HIGHWAY BOUNDARY (REFER TO NOTE 8)
- PROPOSED CARRIAGEWAY
- PROPOSED KERB
- PROPOSED TRAFFIC SIGN
- PROPOSED ILLUMINATED TRAFFIC SIGN
- PROPOSED WHITE LINE ROAD MARKING
- PROPOSED YELLOW LINE ROAD MARKING
- PROPOSED PARKING METER

ROAD MARKING
 MARK (mm)
 GAP (mm)
 WIDTH (mm)

TSRGD DIAGRAM
 NUMBER

- ⚠ UNKNOWN GROUND CONDITIONS PRIOR TO INTRUSIVE GROUND INVESTIGATION WORKS
- ⚠ CONSTRUCTION WORKS IN LIVE ROADS ON A BUS ROUTE
- ⚠ POTENTIAL FOR SHALLOW GROUNDWATER TO BE ENCOUNTERED DURING EXCAVATION WORKS
- ⚠ EXCAVATION IN AND AROUND BURIED SERVICES
- ⚠ POTENTIAL FOR CONTAMINATED TAR IN EXISTING ROAD PAVEMENT
- ⚠ UNEXPLODED ORDNANCE MAY BE PRESENT
- ⚠ HIGH PEDESTRIAN NUMBERS IN TOWN CENTRE



SAFETY HEALTH & ENVIRONMENTAL INFORMATION BOX

HEALTH AND SAFETY SYMBOLS LEGEND

ⓘ XXX	INDICATES A RESIDUAL RISK REQUIRING A COMPULSORY ACTION	ⓘ XXX	INDICATES A RESIDUAL RISK FOR INFORMATION
⊘ XXX	INDICATES A RESIDUAL RISK REQUIRING A PROHIBITIVE ACTION	⚠ XXX	INDICATES A RESIDUAL RISK AS A WARNING
⚠ XXX	INDICATES AN ENVIRONMENTAL HAZARD		

This table is provided to assist the Principal Contractor to fulfill their obligations under the Construction Design & Management Regulations, 2015. It does not include residual risks that a competent Contractor will be aware of nor does it absolve the Principal Contractor of their legal responsibilities.

The reference number corresponds to the Design Risk Management Schedule.

DO NOT SCALE

- NOTES**
- THIS DRAWING SHOULD BE READ IN CONJUNCTION WITH ALL RELEVANT DOCUMENTATION DRAWINGS AND STANDARD DETAILS BUT WITH PARTICULAR REFERENCE TO THE FOLLOWING -288933-WSP-XX-XX-DR-C-1211-1212
 - ALL DIMENSIONS ARE IN METRES UNLESS STATED OTHERWISE.
 - THE ORIGINAL OF THIS DRAWING WAS PRODUCED IN COLOUR - A MONOCHROME COPY CANNOT BE RELIED ON.
 - INFORMATION REGARDING THE LOCATION AND DEPTH OF EXISTING SERVICES CANNOT BE GUARANTEED BY THE STATUTORY UNDERTAKER AND SHOULD BE INVESTIGATED. THE CONTRACTOR SHALL ESTABLISH THE PRECISE LINE AND DEPTH OF ALL EXISTING UTILITIES PRIOR TO COMMENCING ANY EXCAVATION WORK.
 - THE DESIGN IS BASED ON TOPOGRAPHICAL SURVEY REFERENCE ASS 2963 BY ADH SITE SURVEYS LIMITED RECEIVED FEBRUARY 2021.
 - CONFLICTING INFORMATION SHOWN ON THE ENGINEER'S DRAWINGS OR DISCREPANCIES BETWEEN THE INFORMATION GIVEN BY THE ENGINEER AND THAT PROVIDED BY OTHERS MUST BE REFERRED TO THE ENGINEER BEFORE THE WORKS COMMENCE.
 - ALL TRAFFIC SIGNS AND ROAD MARKINGS REFERENCES CONFORM TO TRAFFIC SIGNS REGULATIONS AND GENERAL DIRECTIONS (TSRGD) 2016 AND THE TRAFFIC SIGNS MANUAL (TSM).
 - ALL PROPOSED ROAD MARKINGS ARE TO TIE INTO EXISTING AT SCHEME EXTENTS.
 - THE USE OF TRAFFIC SIGN DIAGRAM REF 7014 IS RESTRICTED TO A PERIOD OF 3 MONTHS AFTER THE COMPLETION OF THE WORKS AND A 'REMOVE BY' DATE MUST BE INCLUDED AT THE BACK OF THE SIGN CONFORMING TO TSRGD 2016.
 - ALL POST SIZES ASSUME LEVEL GROUND CONDITIONS.
 - SIGN LOCATIONS ARE SHOWN INDICATIVELY AND CAN BE ADJUSTED TO EASE BUILDABILITY / AVOID UNDERGROUND SERVICES
 - THIS DRAWING IS BASED IN A WHOLE OR IN PART ON INFORMATION AND/OR DATA APPROVED BY A THIRD PARTY INCLUDING MIDDLESBROUGH COUNCIL AND AS SUCH IN CONTINGENT UPON THE ACCURACY AND COMPLETENESS OF THAT INFORMATION AND/OR DATA. WSP UK LIMITED SHALL NOT BE LIABLE IN RELATION TO ANY INACCURACY OR AMBIGUITY WITH ANY OTHER PROJECT DOCUMENTS SHOULD ANY DATA AND OR INFORMATION UPON WHICH RELIANCE HAS BEEN BASED BE INCORRECT OR OUT OF DATE OR HAVE BEEN CONCEALED, WITHHELD OR BE INCOMPLETE, MISREPRESENTED OR OTHERWISE NOT FULLY DISCLOSED TO WSP UK LIMITED.

UNTIL TECHNICAL APPROVAL HAS BEEN OBTAINED FROM THE RELEVANT LOCAL AUTHORITIES OR STATUTORY BODIES, IT SHOULD BE UNDERSTOOD THAT ALL DRAWINGS ARE ISSUED AS PRELIMINARY AND NOT FOR CONSTRUCTION. SHOULD THE CONTRACTOR AND / OR EMPLOYER COMMENCE WORK PRIOR TO APPROVAL BEING GIVEN, IT IS ENTIRELY AT THEIR OWN RISK

REV	DATE	BY	DESCRIPTION	CHK	APP
P04	10/10/24	AB	UPDATED FOLLOWING CLIENT REQUIREMENTS	JS	PWW
P03	03/09/24	NR	UPDATED FOLLOWING CLIENT REVIEW	JS	PWW
P02	31/07/24	NR	ISSUED FOR CLIENT APPROVAL	JS	PWW
P01	23/07/24	NR	ISSUE FOR COSTING	JS	PWW
DR	12/07/24	NR	DRAFT ISSUE	JS	PWW

wsp

Amber Court, William Armstrong Drive, Newcastle upon Tyne, NE4 7YQ, UK
 T+ 44 (0) 191 226 2000, F+ 44 (0) 191 226 2104
 wsp.com

TEES VALLEY

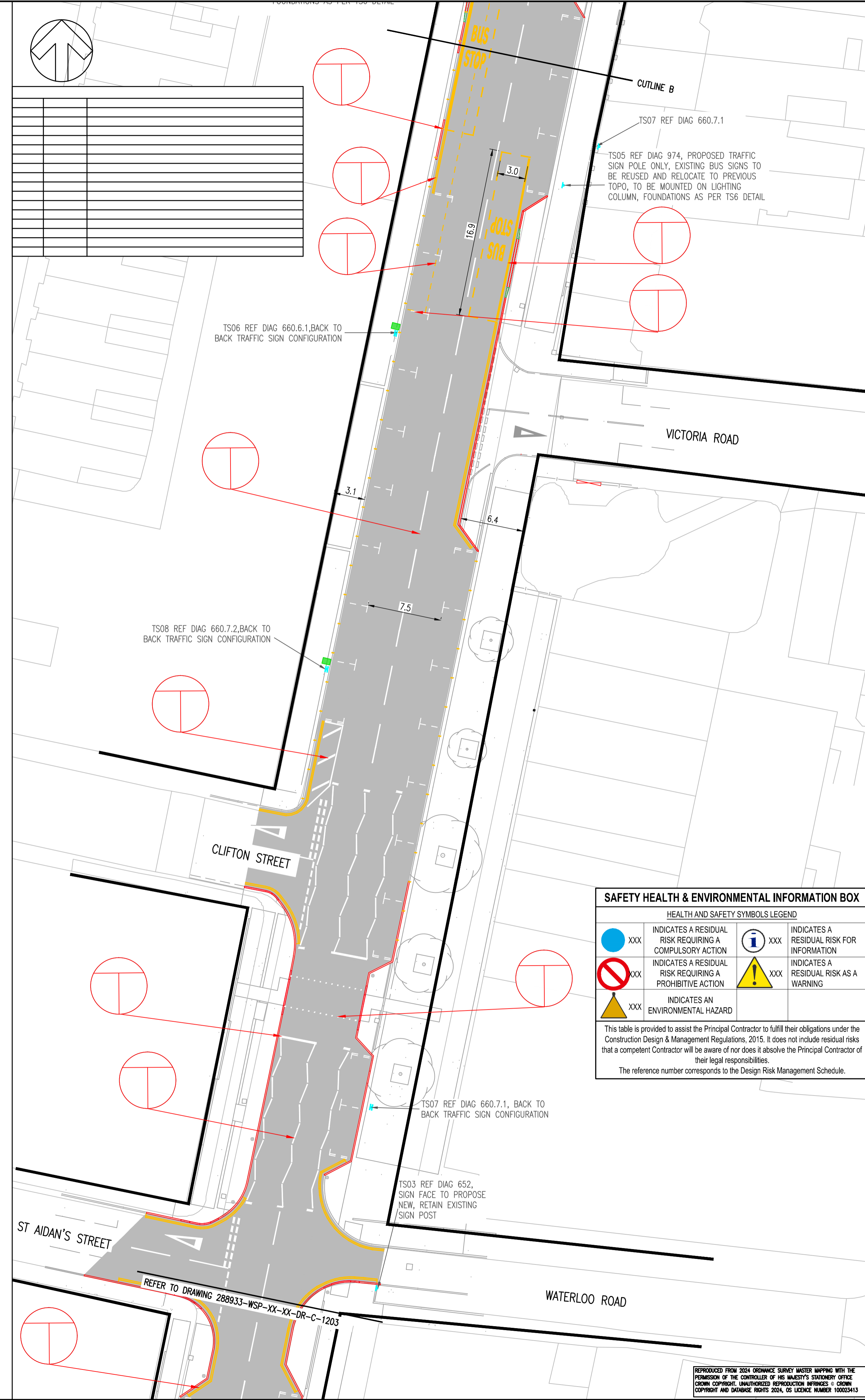
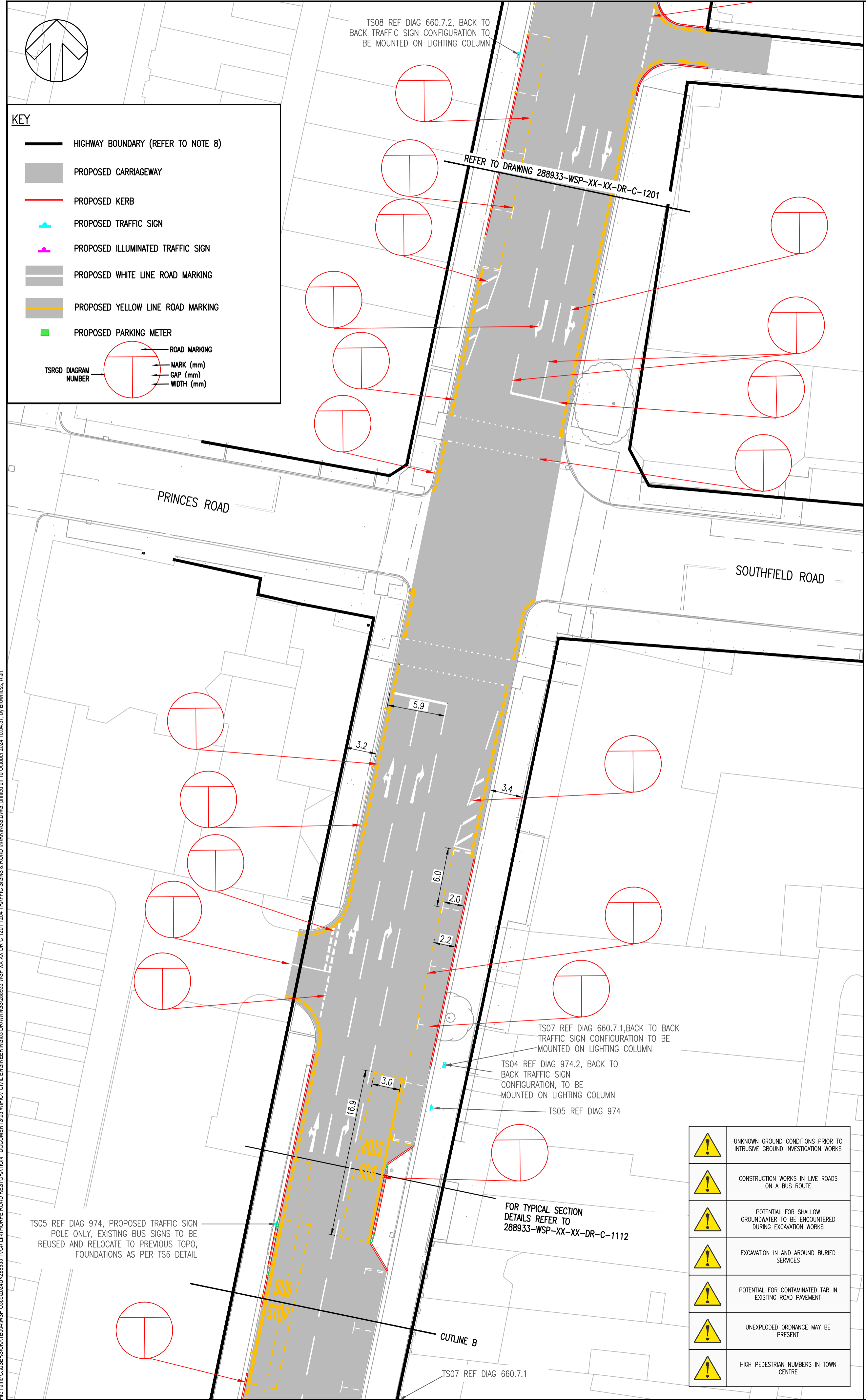
SITE/PROJECT: LINTHORPE ROAD RESTORATION

TITLE: TRAFFIC SIGNS AND ROAD MARKINGS (SHEET 1 OF 4)

SCALE @ AT:	CHECKED:	APPROVED:
1:250	JS	PWW
PROJECT NO:	DESIGNED:	DRAWN:
2024UK293315	NR	NR
DATE:	JULY 2024	
DRAWING NO:	288933-WSP-XX-XX-DR-C-1201	
REV:	P04	

REPRODUCED FROM 2024 ORDNANCE SURVEY MASTER MAPPING WITH THE PERMISSION OF THE CONTROLLER OF HIS MAJESTY'S STATIONERY OFFICE. COPYRIGHT, UNAUTHORIZED REPRODUCTION INFRINGES CROWN COPYRIGHT AND DATABASE RIGHTS 2024, OS LICENSE NUMBER 10003413

File name: C:\Users\WATBOW\OneDrive\WSP\2024\2024UK293315\TVC\LINTHORPE ROAD RESTORATION - DOCUMENTS\03_MPC\01_CIVIL_ENGINEERING\03_DRAWINGS\TRAFFIC SIGNS & ROAD MARKINGS.DWG, printed on 10 October 2024 11:48:11, by: Brownless, Alan



DO NOT SCALE

NOTES

- THIS DRAWING SHOULD BE READ IN CONJUNCTION WITH ALL RELEVANT DOCUMENTATION DRAWINGS AND STANDARD DETAILS BUT WITH PARTICULAR REFERENCE TO THE FOLLOWING - 288933-WSP-XX-XX-DR-C-1211-1212
- ALL DIMENSIONS ARE IN METRES UNLESS STATED OTHERWISE.
- THE ORIGINAL OF THIS DRAWING WAS PRODUCED IN COLOUR - A MONOCHROME COPY CANNOT BE RELIED ON.
- INFORMATION REGARDING THE LOCATION AND DEPTH OF EXISTING SERVICES CANNOT BE GUARANTEED BY THE STATUTORY UNDERTAKER AND SHOULD BE INVESTIGATED. THE CONTRACTOR SHALL ESTABLISH THE PRECISE LINE AND DEPTH OF ALL EXISTING UTILITIES PRIOR TO COMMENCING ANY EXCAVATION WORK.
- THE DESIGN IS BASED ON TOPOGRAPHICAL SURVEY REFERENCE ASS 2963 BY ADH SITE SURVEYS LIMITED RECEIVED FEBRUARY 2021.
- CONFLICTING INFORMATION SHOWN ON THE ENGINEER'S DRAWINGS OR DISCREPANCIES BETWEEN THE INFORMATION GIVEN BY THE ENGINEER AND THAT PROVIDED BY OTHERS MUST BE REFERRED TO THE ENGINEER BEFORE THE WORKS COMMENCE.
- ALL TRAFFIC SIGNS AND ROAD MARKINGS REFERENCES CONFORM TO TRAFFIC SIGNS REGULATIONS AND GENERAL DIRECTIONS (TSRGD) 2016 AND THE TRAFFIC SIGNS MANUAL (TSM).
- ALL PROPOSED ROAD MARKINGS ARE TO TIE INTO EXISTING AT SCHEME EXTENTS.
- THE USE OF TRAFFIC SIGN DIAGRAM REF 7014 IS RESTRICTED TO A PERIOD OF 3 MONTHS AFTER THE COMPLETION OF THE WORKS AND A 'REMOVE BY' DATE MUST BE INCLUDED AT THE BACK OF THE SIGN CONFORMING TO TSRGD 2016.
- ALL POST SIZES ASSUME LEVEL GROUND CONDITIONS.
- SIGN LOCATIONS ARE SHOWN INDICATIVELY AND CAN BE ADJUSTED TO EASE BUILDABILITY / AVOID UNDERGROUND SERVICES
- THIS DRAWING IS BASED IN A WHOLE OR IN PART ON INFORMATION AND/OR DATA APPROVED BY A THIRD PARTY INCLUDING MIDDLESBROUGH COUNCIL AND AS SUCH IN CONTINGENT UPON THE ACCURACY AND COMPLETENESS OF THAT INFORMATION AND/OR DATA. WSP UK LIMITED SHALL NOT BE LIABLE IN RELATION TO ANY INACCURACY PRESENTED WITHIN THE DRAWING AND/OR IN RELATION TO ANY DISCREPANCY OR AMBIGUITY WITH ANY OTHER PROJECT DOCUMENTS SHOULD ANY DATA AND OR INFORMATION UPON WHICH RELIANCE HAS BEEN BASED BE INCORRECT OR OUT OF DATE OR HAVE BEEN CONCEALED, WITHHELD OR BE INCOMPLETE, MISREPRESENTED OR OTHERWISE NOT FULLY DISCLOSED TO WSP UK LIMITED.

UNTIL TECHNICAL APPROVAL HAS BEEN OBTAINED FROM THE RELEVANT LOCAL AUTHORITIES OR STATUTORY BODIES, IT SHOULD BE UNDERSTOOD THAT ALL DRAWINGS ARE ISSUED AS PRELIMINARY AND NOT FOR CONSTRUCTION. SHOULD THE CONTRACTOR AND / OR EMPLOYER COMMENCE WORK PRIOR TO APPROVAL BEING GIVEN, IT IS ENTIRELY AT THEIR OWN RISK

PO4	10/10/24	AB	UPDATED FOLLOWING CLIENT REQUIREMENTS	JS	PWW
PO3	03/09/24	NR	UPDATED FOLLOWING CLIENT REVIEW	JS	PWW
PO2	31/07/24	NR	ISSUED FOR CLIENT APPROVAL	JS	PWW
PO1	23/07/24	NR	ISSUE FOR COSTING	JS	PWW
DR	12/07/24	NR	DRAFT ISSUE	JS	PWW
REV	DATE	BY	DESCRIPTION	CHK	APP

DRAWING STATUS:

wsp

Amber Court, William Armstrong Drive, Newcastle upon Tyne, NE4 7YQ, UK
T+ 44 (0) 191 226 2000, F+ 44 (0) 191 226 2104
wsp.com

CLIENT:

TEES VALLEY

SITE/PROJECT:

LINTHORPE ROAD RESTORATION

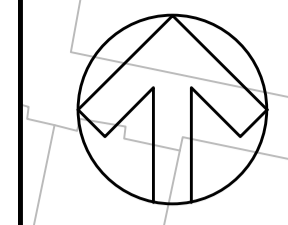
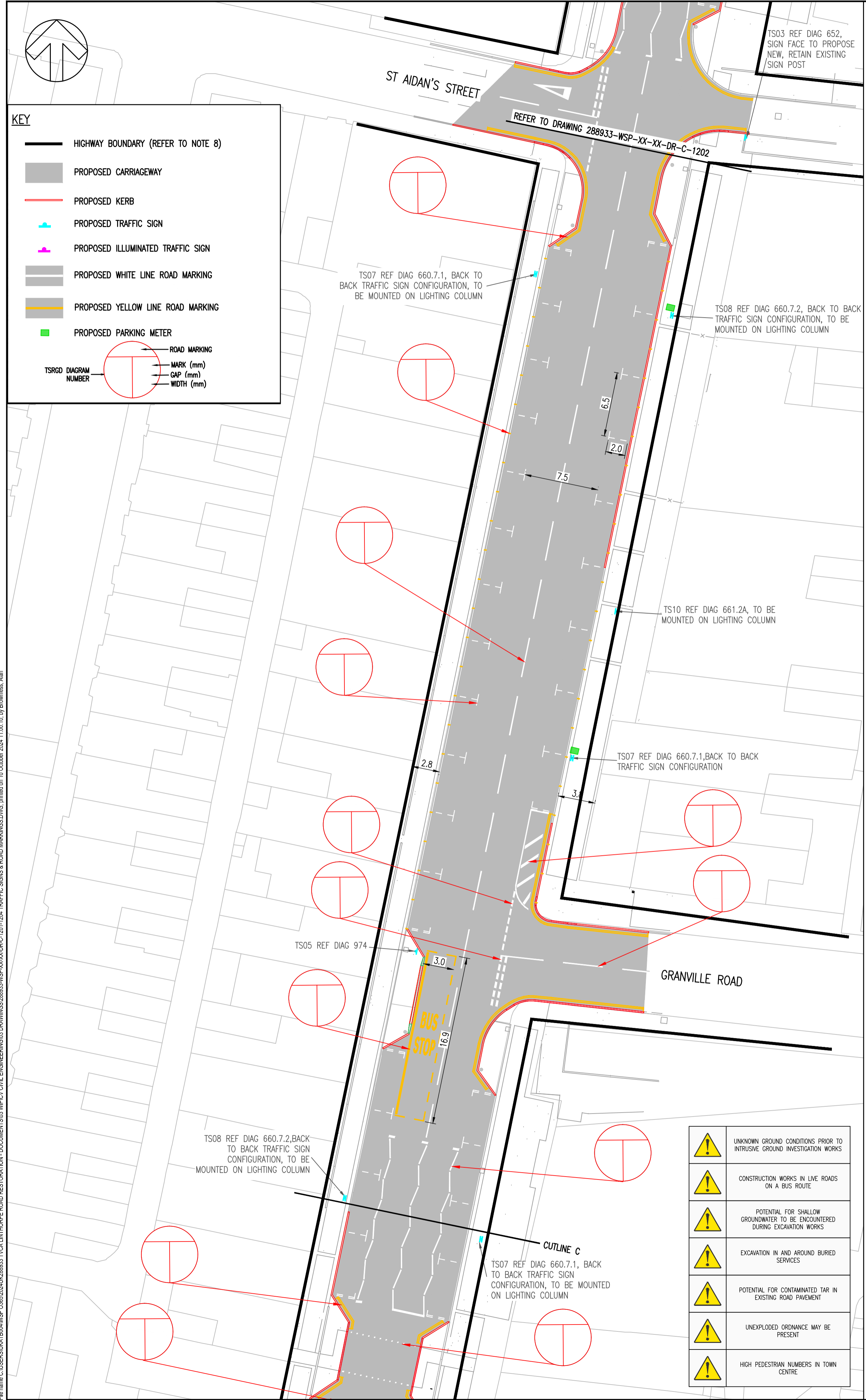
TITLE:

TRAFFIC SIGNS AND ROAD MARKINGS
(SHEET 2 OF 4)

SCALE @ A1:	1:250	CHECKED:	JS	APPROVED:	PWW
PROJECT NO:	2024UK293315	DESIGNED:	NR	DATE:	JULY 2024
DRAWING NO:	288933-WSP-XX-XX-DR-C-1202	DRAWN:	NR	REV:	P04

REPRODUCED FROM 2024 ORDNANCE SURVEY MASTER MAPPING WITH THE PERMISSION OF THE CONTROLLER OF HIS MAJESTY'S STATIONERY OFFICE
CROWN COPYRIGHT. UNAUTHORIZED REPRODUCTION INFRINGES © CROWN COPYRIGHT AND DATABASE RIGHTS 2024. OS LICENSE NUMBER 10003413

File name: C:\Users\WATBOW\OneDrive\Documents\WSP\Civil Engineering\Drawings\288933-WSP-XX-XX-DR-C-1201-1204\TRAFFIC SIGNS & ROAD MARKINGS.DWG, printed on 10 October 2024 10:54:47 by Brownless, Alan

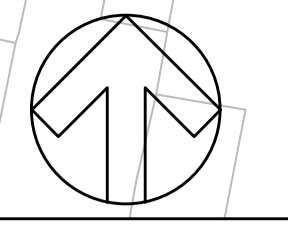
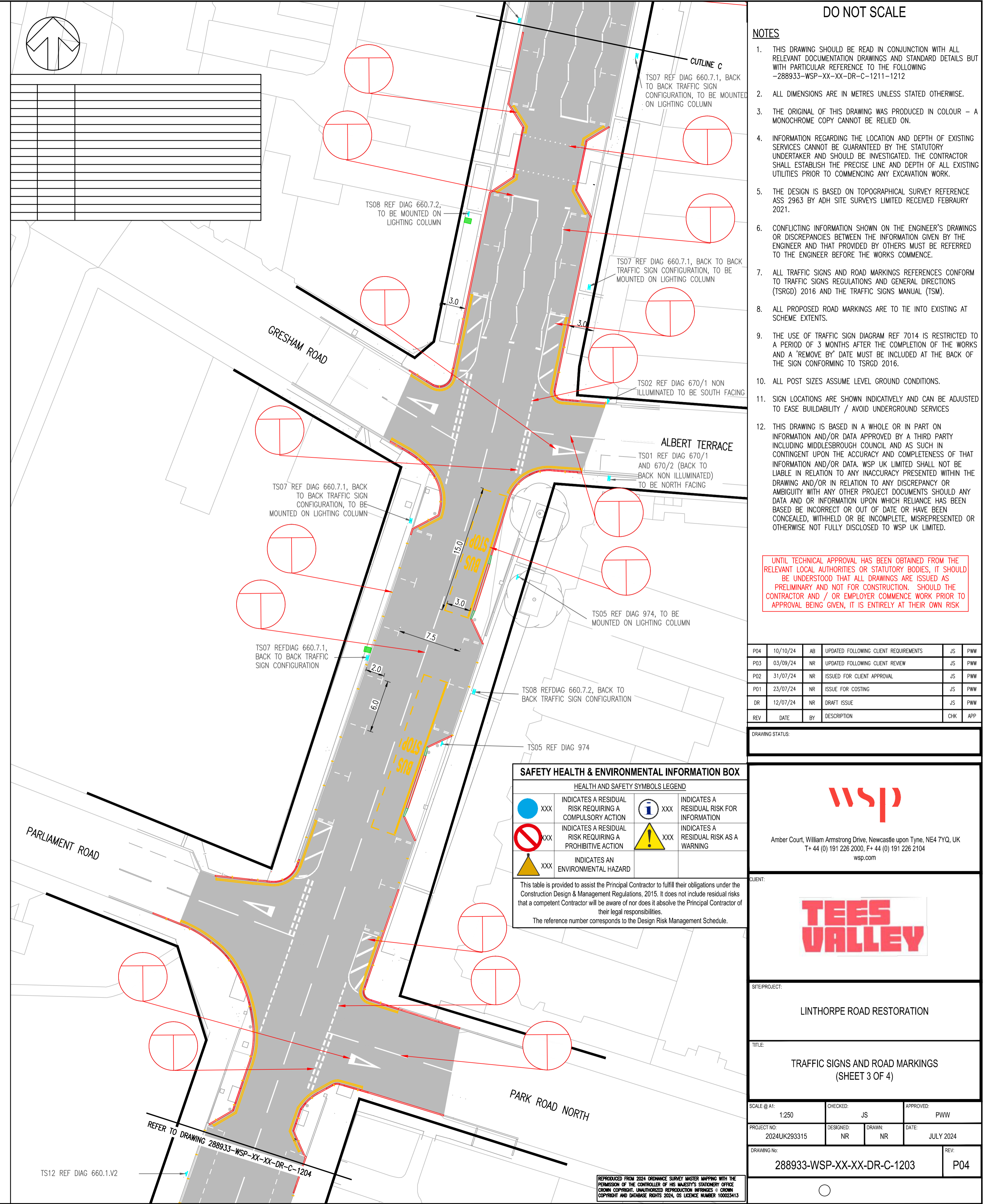


KEY

- HIGHWAY BOUNDARY (REFER TO NOTE 8)
- PROPOSED CARRIAGEWAY
- PROPOSED KERB
- PROPOSED TRAFFIC SIGN
- PROPOSED ILLUMINATED TRAFFIC SIGN
- PROPOSED WHITE LINE ROAD MARKING
- PROPOSED YELLOW LINE ROAD MARKING
- PROPOSED PARKING METER

TSRGD DIAGRAM NUMBER	ROAD MARKING
1	MARK (mm)
2	GAP (mm)
3	WIDTH (mm)

⚠	UNKNOWN GROUND CONDITIONS PRIOR TO INTRUSIVE GROUND INVESTIGATION WORKS
⚠	CONSTRUCTION WORKS IN LIVE ROADS ON A BUS ROUTE
⚠	POTENTIAL FOR SHALLOW GROUNDWATER TO BE ENCOUNTERED DURING EXCAVATION WORKS
⚠	EXCAVATION IN AND AROUND BURIED SERVICES
⚠	POTENTIAL FOR CONTAMINATED TAR IN EXISTING ROAD PAVEMENT
⚠	UNEXPLODED ORDNANCE MAY BE PRESENT
⚠	HIGH PEDESTRIAN NUMBERS IN TOWN CENTRE



SAFETY HEALTH & ENVIRONMENTAL INFORMATION BOX

HEALTH AND SAFETY SYMBOLS LEGEND

ⓘ	INDICATES A RESIDUAL RISK REQUIRING A COMPULSORY ACTION	ⓘ	INDICATES A RESIDUAL RISK FOR INFORMATION
⊘	INDICATES A RESIDUAL RISK REQUIRING A PROHIBITIVE ACTION	⚠	INDICATES A RESIDUAL RISK AS A WARNING
⚠	INDICATES AN ENVIRONMENTAL HAZARD		

This table is provided to assist the Principal Contractor to fulfill their obligations under the Construction Design & Management Regulations, 2015. It does not include residual risks that a competent Contractor will be aware of nor does it absolve the Principal Contractor of their legal responsibilities.

The reference number corresponds to the Design Risk Management Schedule.

DO NOT SCALE

- NOTES**
- THIS DRAWING SHOULD BE READ IN CONJUNCTION WITH ALL RELEVANT DOCUMENTATION DRAWINGS AND STANDARD DETAILS BUT WITH PARTICULAR REFERENCE TO THE FOLLOWING - 288933-WSP-XX-XX-DR-C-1211-1212
 - ALL DIMENSIONS ARE IN METRES UNLESS STATED OTHERWISE.
 - THE ORIGINAL OF THIS DRAWING WAS PRODUCED IN COLOUR - A MONOCHROME COPY CANNOT BE RELIED ON.
 - INFORMATION REGARDING THE LOCATION AND DEPTH OF EXISTING SERVICES CANNOT BE GUARANTEED BY THE STATUTORY UNDERTAKER AND SHOULD BE INVESTIGATED. THE CONTRACTOR SHALL ESTABLISH THE PRECISE LINE AND DEPTH OF ALL EXISTING UTILITIES PRIOR TO COMMENCING ANY EXCAVATION WORK.
 - THE DESIGN IS BASED ON TOPOGRAPHICAL SURVEY REFERENCE ASS 2963 BY ADH SITE SURVEYS LIMITED RECEIVED FEBRUARY 2021.
 - CONFLICTING INFORMATION SHOWN ON THE ENGINEER'S DRAWINGS OR DISCREPANCIES BETWEEN THE INFORMATION GIVEN BY THE ENGINEER AND THAT PROVIDED BY OTHERS MUST BE REFERRED TO THE ENGINEER BEFORE THE WORKS COMMENCE.
 - ALL TRAFFIC SIGNS AND ROAD MARKINGS REFERENCES CONFORM TO TRAFFIC SIGNS REGULATIONS AND GENERAL DIRECTIONS (TSRGD) 2016 AND THE TRAFFIC SIGNS MANUAL (TSM).
 - ALL PROPOSED ROAD MARKINGS ARE TO TIE INTO EXISTING AT SCHEME EXTENTS.
 - THE USE OF TRAFFIC SIGN DIAGRAM REF 7014 IS RESTRICTED TO A PERIOD OF 3 MONTHS AFTER THE COMPLETION OF THE WORKS AND A "REMOVE BY" DATE MUST BE INCLUDED AT THE BACK OF THE SIGN CONFORMING TO TSRGD 2016.
 - ALL POST SIZES ASSUME LEVEL GROUND CONDITIONS.
 - SIGN LOCATIONS ARE SHOWN INDICATIVELY AND CAN BE ADJUSTED TO EASE BUILDABILITY / AVOID UNDERGROUND SERVICES
 - THIS DRAWING IS BASED IN A WHOLE OR IN PART ON INFORMATION AND/OR DATA APPROVED BY A THIRD PARTY INCLUDING MIDDLESBROUGH COUNCIL AND AS SUCH IN CONTINGENT UPON THE ACCURACY AND COMPLETENESS OF THAT INFORMATION AND/OR DATA. WSP UK LIMITED SHALL NOT BE LIABLE IN RELATION TO ANY INACCURACY PRESENTED WITHIN THE DRAWING AND/OR IN RELATION TO ANY DISCREPANCY OR AMBIGUITY WITH ANY OTHER PROJECT DOCUMENTS SHOULD ANY DATA AND OR INFORMATION UPON WHICH RELIANCE HAS BEEN BASED BE INCORRECT OR OUT OF DATE OR HAVE BEEN CONCEALED, WITHHELD OR BE INCOMPLETE, MISREPRESENTED OR OTHERWISE NOT FULLY DISCLOSED TO WSP UK LIMITED.

UNTIL TECHNICAL APPROVAL HAS BEEN OBTAINED FROM THE RELEVANT LOCAL AUTHORITIES OR STATUTORY BODIES, IT SHOULD BE UNDERSTOOD THAT ALL DRAWINGS ARE ISSUED AS PRELIMINARY AND NOT FOR CONSTRUCTION. SHOULD THE CONTRACTOR AND / OR EMPLOYER COMMENCE WORK PRIOR TO APPROVAL BEING GIVEN, IT IS ENTIRELY AT THEIR OWN RISK

REV	DATE	BY	DESCRIPTION	CHK	APP
P04	10/10/24	AB	UPDATED FOLLOWING CLIENT REQUIREMENTS	JS	PWW
P03	03/09/24	NR	UPDATED FOLLOWING CLIENT REVIEW	JS	PWW
P02	31/07/24	NR	ISSUED FOR CLIENT APPROVAL	JS	PWW
P01	23/07/24	NR	ISSUE FOR COSTING	JS	PWW
DR	12/07/24	NR	DRAFT ISSUE	JS	PWW

DRAWING STATUS:

wsp

Amber Court, William Armstrong Drive, Newcastle upon Tyne, NE4 7YQ, UK
T +44 (0) 191 226 2000, F +44 (0) 191 226 2104
wsp.com

TEES VALLEY

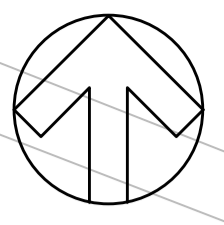
SITE/PROJECT: LINTHORPE ROAD RESTORATION

TITLE: TRAFFIC SIGNS AND ROAD MARKINGS (SHEET 3 OF 4)

SCALE @ A1: 1:250	CHECKED: JS	APPROVED: PWW
PROJECT NO: 2024UK293315	DESIGNED: NR	DATE: JULY 2024
DRAWING NO: 288933-WSP-XX-XX-DR-C-1203	REV: P04	

File name: C:\USERS\WATBOW\WSP_03602024\288933\TVC\LINTHORPE ROAD RESTORATION - DOCUMENTS\03 MP\01 CIVIL ENGINEERING\03 DRAWINGS\288933-WSP-XX-XX-DR-C-1203-1204 TRAFFIC SIGNS & ROAD MARKINGS.DWG, printed on 10 October 2024 11:00:01, by Brownless, Alan

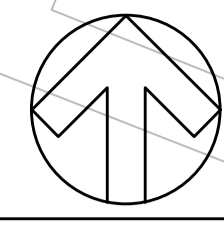
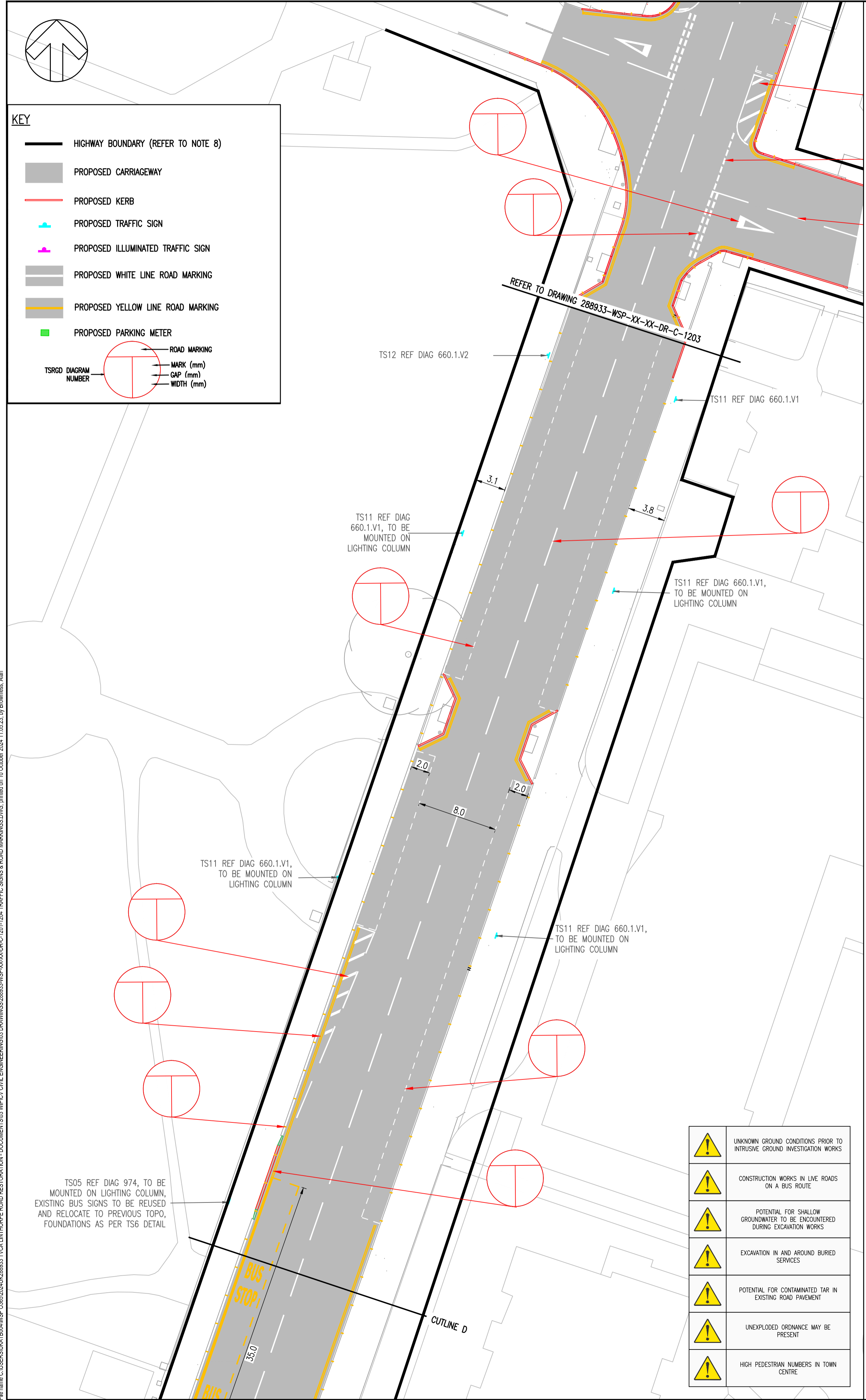
REPRODUCED FROM DATA OBTAINED FROM THE PERMISSION OF THE CONTROLLER OF HIS MAJESTY'S STATIONERY OFFICE. COPYRIGHT AND DESIGN RIGHTS 2024, BY LICENSE NUMBER 10003413

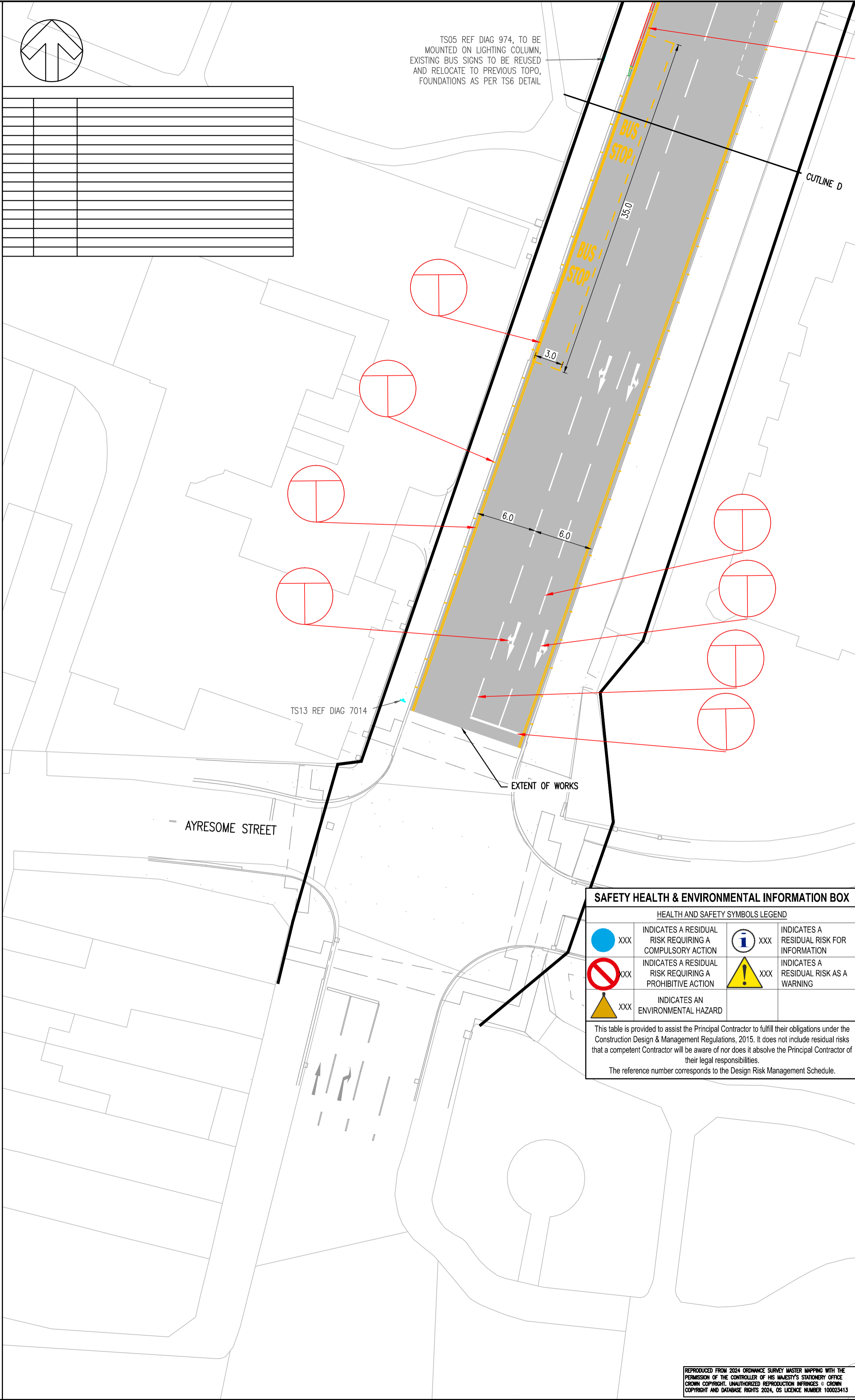


KEY

- HIGHWAY BOUNDARY (REFER TO NOTE 8)
- PROPOSED CARRIAGEWAY
- PROPOSED KERB
- PROPOSED TRAFFIC SIGN
- PROPOSED ILLUMINATED TRAFFIC SIGN
- PROPOSED WHITE LINE ROAD MARKING
- PROPOSED YELLOW LINE ROAD MARKING
- PROPOSED PARKING METER

TSRGD DIAGRAM NUMBER	ROAD MARKING
(Symbol)	MARK (mm)
(Symbol)	GAP (mm)
(Symbol)	WIDTH (mm)





DO NOT SCALE

- NOTES**
- THIS DRAWING SHOULD BE READ IN CONJUNCTION WITH ALL RELEVANT DOCUMENTATION DRAWINGS AND STANDARD DETAILS BUT WITH PARTICULAR REFERENCE TO THE FOLLOWING - 288933-WSP-XX-XX-DR-C-1211-1212
 - ALL DIMENSIONS ARE IN METRES UNLESS STATED OTHERWISE.
 - THE ORIGINAL OF THIS DRAWING WAS PRODUCED IN COLOUR - A MONOCHROME COPY CANNOT BE RELIED ON.
 - INFORMATION REGARDING THE LOCATION AND DEPTH OF EXISTING SERVICES CANNOT BE GUARANTEED BY THE STATUTORY UNDERTAKER AND SHOULD BE INVESTIGATED. THE CONTRACTOR SHALL ESTABLISH THE PRECISE LINE AND DEPTH OF ALL EXISTING UTILITIES PRIOR TO COMMENCING ANY EXCAVATION WORK.
 - THE DESIGN IS BASED ON TOPOGRAPHICAL SURVEY REFERENCE ASS 2963 BY ADH SITE SURVEYS LIMITED RECEIVED FEBRUARY 2021.
 - CONFLICTING INFORMATION SHOWN ON THE ENGINEER'S DRAWINGS OR DISCREPANCIES BETWEEN THE INFORMATION GIVEN BY THE ENGINEER AND THAT PROVIDED BY OTHERS MUST BE REFERRED TO THE ENGINEER BEFORE THE WORKS COMMENCE.
 - ALL TRAFFIC SIGNS AND ROAD MARKINGS CONFORM TO TRAFFIC SIGNS REGULATIONS AND GENERAL DIRECTIONS (TSRGD) 2016 AND THE TRAFFIC SIGNS MANUAL (TSM).
 - ALL PROPOSED ROAD MARKINGS ARE TO TIE INTO EXISTING AT SCHEME EXTENTS.
 - THE USE OF TRAFFIC SIGN DIAGRAM REF 7014 IS RESTRICTED TO A PERIOD OF 3 MONTHS AFTER THE COMPLETION OF THE WORKS AND A 'REMOVE BY' DATE MUST BE INCLUDED AT THE BACK OF THE SIGN CONFORMING TO TSRGD 2016.
 - ALL POST SIZES ASSUME LEVEL GROUND CONDITIONS.
 - SIGN LOCATIONS ARE SHOWN INDICATIVELY AND CAN BE ADJUSTED TO EASE BUILDABILITY / AVOID UNDERGROUND SERVICES
 - THIS DRAWING IS BASED IN A WHOLE OR IN PART ON INFORMATION AND/OR DATA APPROVED BY A THIRD PARTY INCLUDING MIDDLESBROUGH COUNCIL AND AS SUCH IN CONTINGENT UPON THE ACCURACY AND COMPLETENESS OF THAT INFORMATION AND/OR DATA. WSP UK LIMITED SHALL NOT BE LIABLE IN RELATION TO ANY INACCURACY PRESENTED WITHIN THE DRAWING AND/OR IN RELATION TO ANY DISCREPANCY OR AMBIGUITY WITH ANY OTHER PROJECT DOCUMENTS SHOULD ANY DATA AND OR INFORMATION UPON WHICH RELIANCE HAS BEEN BASED BE INCORRECT OR OUT OF DATE OR HAVE BEEN CONCEALED, WITHHELD OR BE INCOMPLETE, MISREPRESENTED OR OTHERWISE NOT FULLY DISCLOSED TO WSP UK LIMITED.

UNTIL TECHNICAL APPROVAL HAS BEEN OBTAINED FROM THE RELEVANT LOCAL AUTHORITIES OR STATUTORY BODIES, IT SHOULD BE UNDERSTOOD THAT ALL DRAWINGS ARE ISSUED AS PRELIMINARY AND NOT FOR CONSTRUCTION. SHOULD THE CONTRACTOR AND / OR EMPLOYER COMMENCE WORK PRIOR TO APPROVAL BEING GIVEN, IT IS ENTIRELY AT THEIR OWN RISK

REV	DATE	BY	DESCRIPTION	CHK	APP
P04	10/10/24	AB	UPDATED FOLLOWING CLIENT REQUIREMENTS	JS	PWW
P03	03/09/24	NR	UPDATED FOLLOWING CLIENT REVIEW	JS	PWW
P02	31/07/24	NR	ISSUED FOR CLIENT APPROVAL	JS	PWW
P01	23/07/24	NR	ISSUE FOR COSTING	JS	PWW
DR	12/07/24	NR	DRAFT ISSUE	JS	PWW

DRAWING STATUS:



Amber Court, William Armstrong Drive, Newcastle upon Tyne, NE4 7YQ, UK
T+ 44 (0) 191 226 2000, F+ 44 (0) 191 226 2104
wsp.com



SITE PROJECT:
LINTHORPE ROAD RESTORATION

TITLE:
**TRAFFIC SIGNS AND ROAD MARKINGS
(SHEET 4 OF 4)**

SCALE @ A1:	CHECKED:	APPROVED:
1:250	JS	PWW
PROJECT NO:	DESIGNED:	DATE:
2024UK293315	NR	JULY 2024
DRAWING NO:	REV:	
288933-WSP-XX-XX-DR-C-1204	P04	

SAFETY HEALTH & ENVIRONMENTAL INFORMATION BOX

HEALTH AND SAFETY SYMBOLS LEGEND

	INDICATES A RESIDUAL RISK REQUIRING A COMPULSORY ACTION		INDICATES A RESIDUAL RISK FOR INFORMATION
	INDICATES A RESIDUAL RISK REQUIRING A PROHIBITIVE ACTION		INDICATES A RESIDUAL RISK AS A WARNING
	INDICATES AN ENVIRONMENTAL HAZARD		

This table is provided to assist the Principal Contractor to fulfill their obligations under the Construction Design & Management Regulations, 2015. It does not include residual risks that a competent Contractor will be aware of nor does it absolve the Principal Contractor of their legal responsibilities.
The reference number corresponds to the Design Risk Management Schedule.

	UNKNOWN GROUND CONDITIONS PRIOR TO INTRUSIVE GROUND INVESTIGATION WORKS
	CONSTRUCTION WORKS IN LIVE ROADS ON A BUS ROUTE
	POTENTIAL FOR SHALLOW GROUNDWATER TO BE ENCOUNTERED DURING EXCAVATION WORKS
	EXCAVATION IN AND AROUND BURIED SERVICES
	POTENTIAL FOR CONTAMINATED TAR IN EXISTING ROAD PAVEMENT
	UNEXPLODED ORDNANCE MAY BE PRESENT
	HIGH PEDESTRIAN NUMBERS IN TOWN CENTRE

REPRODUCED FROM 2024 ORDNANCE SURVEY MASTER MAPPING WITH THE PERMISSION OF THE CONTROLLER OF HIS MAJESTY'S STATIONERY OFFICE
CROWN COPYRIGHT. UNAUTHORIZED REPRODUCTION INFRINGES © CROWN COPYRIGHT AND DATABASE RIGHTS 2024. OS LICENCE NUMBER 10003413

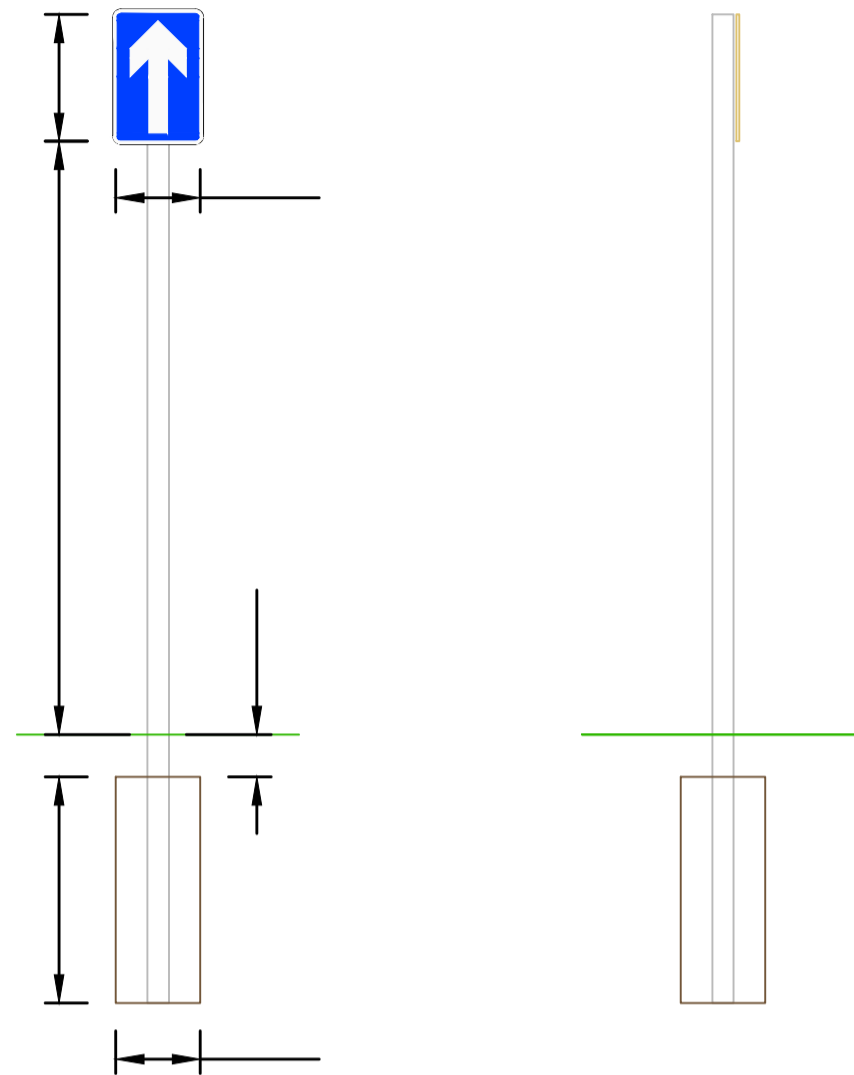
File name: CLUSERS\MATBOWNSP_03602024\288933-TVCLINTHORPE ROAD RESTORATION - DOCUMENTS\00 MP\CIVIL ENGINEERING\00 DRAWINGS\TRAFFIC SIGNS & ROAD MARKINGS.DWG, printed on 10 October 2024 11:05:23, by Brownless, Alan

DO NOT SCALE

NOTES

- ALL DIMENSIONS ARE IN METRES UNLESS OTHERWISE STATED.
- ALL LEVELS ARE IN METRES ABOVE ORDNANCE DATUM.
- WORKS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE RELEVANT B.S, MIDDLESBROUGH COUNCIL HIGHWAY DESIGN GUIDE AND DEPARTMENT OF TRANSPORTS 'SPECIFICATION FOR HIGHWAY WORKS', PUBLISHED BY HMSO. ALL SPECIFICATION CLAUSES REFER TO THIS DOCUMENT UNLESS OTHERWISE STATED.
- ALL SETTING OUT IS TO BE AGREED WITH THE ENGINEER PRIOR TO THE COMMENCEMENT OF THE WORKS. THIS DRAWING IS TO BE READ IN CONJUNCTION WITH ALL OTHER RELEVANT DRAWINGS, SCHEDULES AND SPECIFICATIONS.
- CONFLICTING INFORMATION SHOWN ON THE ENGINEER'S DRAWINGS OR DISCREPANCIES BETWEEN THE INFORMATION GIVEN BY THE ENGINEER AND THAT PROVIDED BY OTHERS MUST BE REFERRED TO THE ENGINEER BEFORE THE WORKS COMMENCE.
- TRAFFIC SIGNS & ROAD MARKING REFERENCES CONFORMS TO TSRGD 2016.
- SIGN POSITIONING AND SIZES ARE NOT TO SCALE, EXACT LOCATION AND ORIENTATION OF SIGNS TO BE CONFIRMED WITH ENGINEERS REPRESENTATIVE PRIOR TO ERECTION ON SITE.
- POST FOUNDATIONS SUBJECT TO FINAL DESIGN BY CONTRACTOR TO AVOID BURIED SERVICES.
- SIGNAGE LOCATED WITHIN FOOTWAYS TO HAVE A MOUNTING HEIGHT OF 2.3m;
- ALL ILLUMINATED SIGNS TO BE MOUNTED ON GALVANISED WIDE BASED POSTS WITH G1 FINISH KCC COASTAL SPECIFICATION AS MANUFACTURED BY STANTON FABRIKAT.
- ALL SIGN LIGHTING UNITS TO BE LED LVA RANGE (TO INCLUDE PHOTOCELL); 3W FOR SMALL PLATES (600MM AND UNDER), 10W FOR LARGE PLATES (OVER 600MM) AS MANUFACTURED BY SIMMONSIGNS.
- ALL ILLUMINATED AND NON-ILLUMINATED SIGN FACES TO BE CLASS 2 REFLECTIVE.
- NON-ILLUMINATED TRAFFIC BOLLARDS TO BE METRO PLUS ANTI-TWIST (REFLEX BOLLARD) AS MANUFACTURED BY TMP.

Signing Project	Signing Project	Width (mm)	Height (mm)	Area (sq.m)	Background	Border	Legend
Sign Reference	TS03	300	450	0.14	Blue	White	White
x Height							
Material	Class RA2						



TS03 - 652

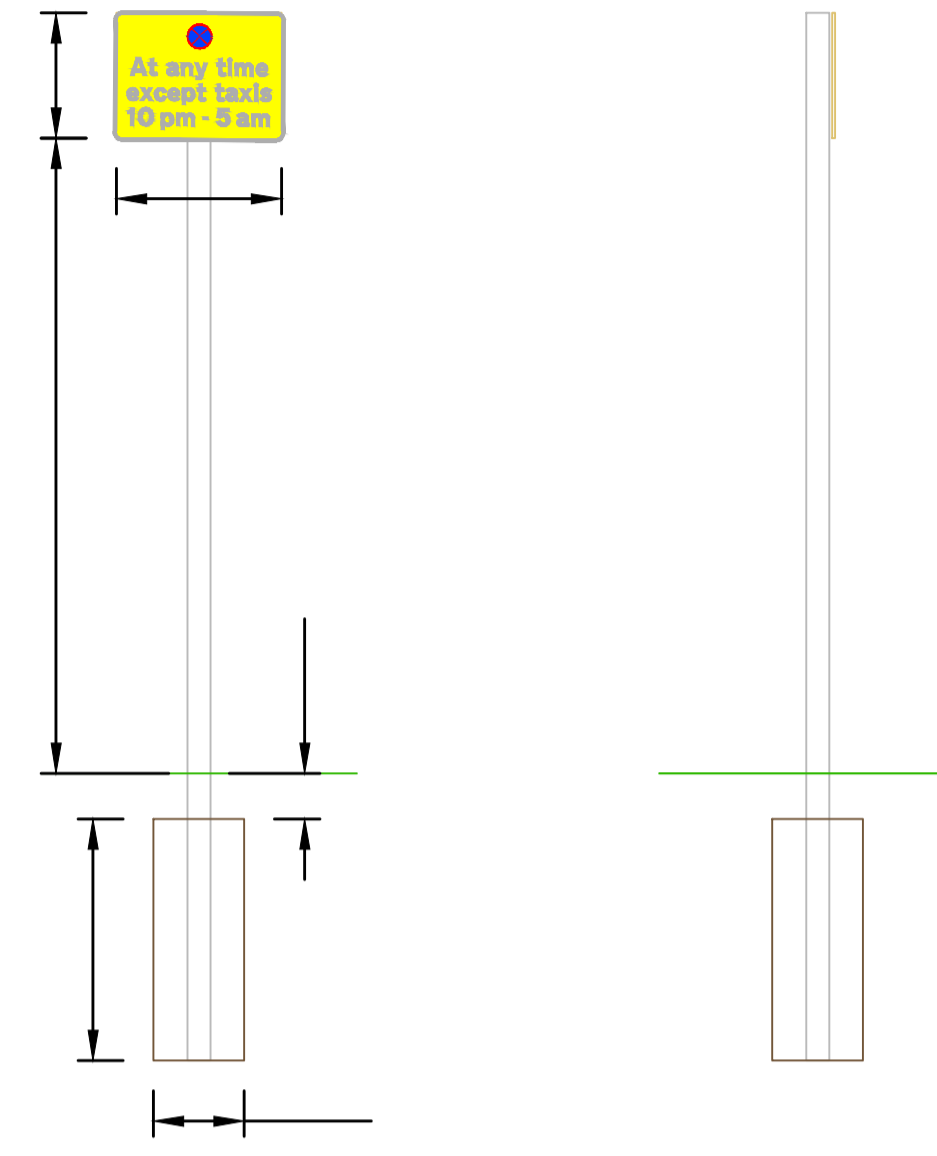
Structural Calculation Details

Signs:
 Reference: TS03
 Width: 300mm, Height: 450mm
 Shape: Rectangle
 Orientation: Facing front
 Mounted on posts: 1
 Mounting height: 2100mm, at post

Posts:
 Quantity: 1
 Type: 76.1 x 3.2 CHS (S355)
 Spacing: N/A
 Post 1 Length: 3500mm

Base:
 Type: Planted
 Diameter: 300mm
 Depth: 800mm
 Volume: 0.06cum
 Earth cover: 150mm

Signing Project	Signing Project	Width (mm)	Height (mm)	Area (sq.m)	Background	Border	Legend
Sign Reference	TS04	545	415	0.23	Yellow	Black	Black
x Height							
Material	Class RA2						



TS04 - 974.2

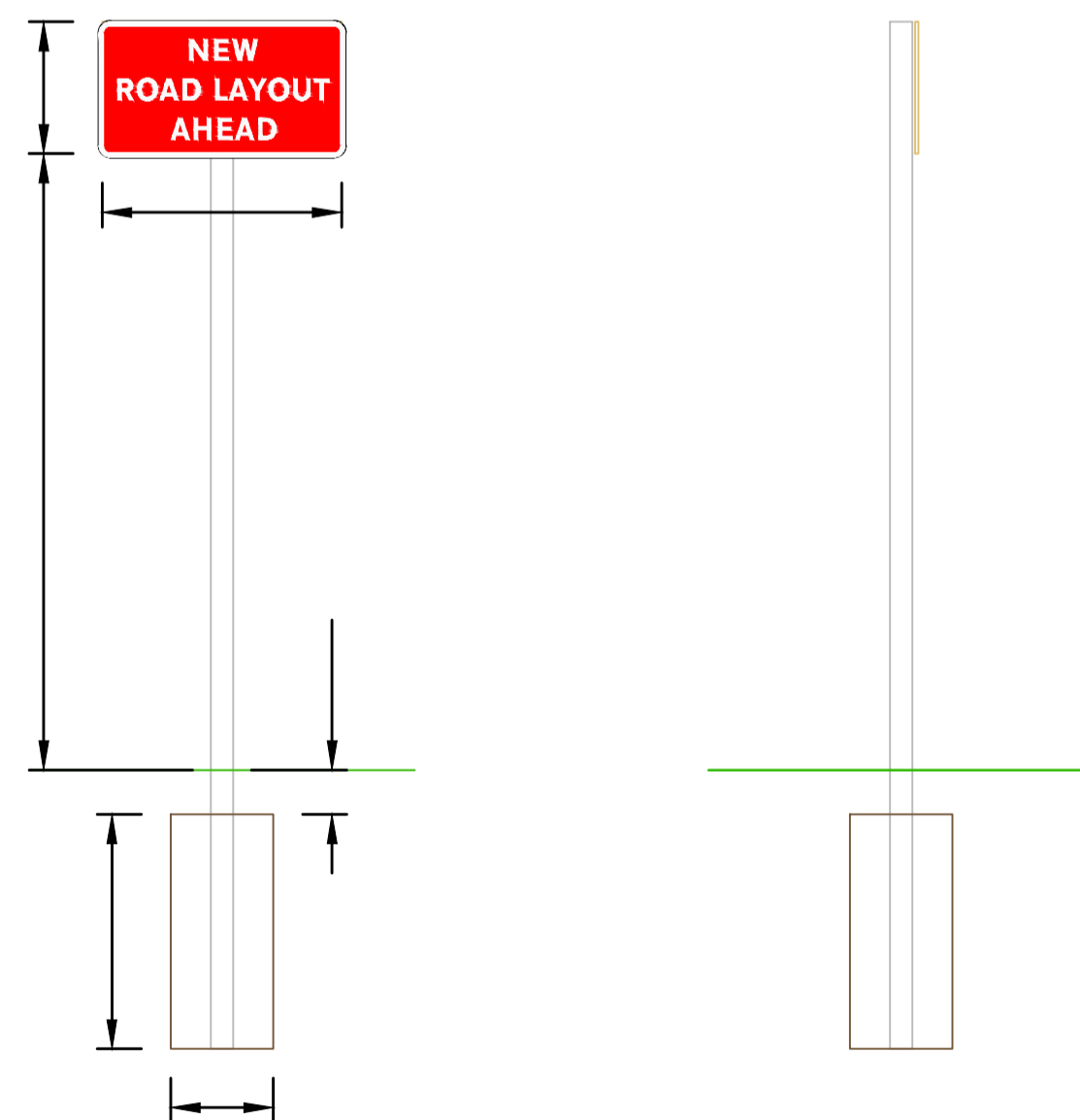
Structural Calculation Details

Signs:
 Reference: TS04
 Width: 545mm, Height: 415mm
 Shape: Rectangle
 Orientation: Facing front
 Mounted on posts: 1
 Mounting height: 2100mm, at post

Posts:
 Quantity: 1
 Type: 76.1 x 3.2 CHS (S355)
 Spacing: N/A
 Post 1 Length: 3465mm

Base:
 Type: Planted
 Diameter: 300mm
 Depth: 800mm
 Volume: 0.06cum
 Earth cover: 150mm

Signing Project	Signing Project	Width (mm)	Height (mm)	Area (sq.m)	Background	Border	Legend
Sign Reference	TS13	815	450	0.37	Red	White	White
x Height							
Material	Class RA2						



TS13 - 7014

Structural Calculation Details

Signs:
 Reference: TS13
 Width: 815mm, Height: 450mm
 Shape: Rectangle
 Orientation: Facing front
 Mounted on posts: 1
 Mounting height: 2100mm, at post

Posts:
 Quantity: 1
 Type: 76.1 x 3.2 CHS (S355)
 Spacing: N/A
 Post 1 Length: 3500mm

Base:
 Type: Planted
 Diameter: 350mm
 Depth: 800mm
 Volume: 0.08cum
 Earth cover: 150mm

SIGN SCHEDULE

Sign Reference	TSRGD NO.	x Height	Width (mm)	Height (mm)	Area (m ²)	Mounting height	Post quantity	Post type	Post 1 length	Base type	Base depth	Base volume	Earth cover	Illumination
TS02	670/1	100	600	600	0.28	2100mm, at post	1	76.1 x 3.2 CHS (S355)	3770	Planted	800	0.06	150	Yes
TS04	974.2	40.0	545	415	0.23	2100mm, at post	1	76.1 x 3.2 CHS (S355)	3465	Planted	800	0.06	150	No
TS05	974.2	40.0	590	465	0.27	2100mm, at post	1	76.1 x 3.2 CHS (S355)	3515	Planted	800	0.06	150	No
TS06	660.6.1	15.0	280	455	0.13	2100mm, at post	1	76.1 x 3.2 CHS (S355)	3505	Planted	800	0.06	150	No
TS07	660.7.1	15.0	280	505	0.14	2100mm, at post	1	76.1 x 3.2 CHS (S355)	3555	Planted	800	0.06	150	No
TS08	660.7.2	15.0	280	505	0.14	2100mm, at post	1	76.1 x 3.2 CHS (S355)	3555	Planted	800	0.06	150	No
TS09	660.7.3	15.0	280	505	0.14	2100mm, at post	1	76.1 x 3.2 CHS (S355)	3555	Planted	800	0.06	150	No
TS10	661.2A	15.0	235	165	0.04	2100mm, at post	1	76.1 x 3.2 CHS (S355)	3215	Planted	800	0.06	150	No
TS11	660.1.V1	15.0	270	195	0.05	2100mm, at post	1	76.1 x 3.2 CHS (S355)	3245	Planted	800	0.06	150	No
TS12	660.1.V2	15.0	270	235	0.06	2100mm, at post	1	76.1 x 3.2 CHS (S355)	3285	Planted	800	0.06	150	No
TS13	7014	50.0	815	450	0.37	2100mm, at post	1	76.1 x 3.2 CHS (S355)	3500	Planted	800	0.08	150	No

REV	DATE	BY	DESCRIPTION	CHK	APP
P03	10/10/24	AB	UPDATED FOLLOWING CLIENT REQUIREMENTS	JS	PWW
P02	31/07/24	NR	ISSUED FOR CLIENT APPROVAL	JS	PWW
P01	23/07/24	NR	ISSUE FOR COST ESTIMATE	JS	PWW
DR	15/07/24	NR	DRAFT ISSUE	JS	PWW

DRAWING STATUS:

Amber Court, William Armstrong Drive, Newcastle upon Tyne, NE4 7YQ, UK
 T+ 44 (0) 191 226 2000, F+ 44 (0) 191 226 2104
 wsp.com

SITE/PROJECT:
 LINTHORPE ROAD RESTORATION

TITLE:
 TRAFFIC SIGN DETAILS
 (SHEET 1 OF 3)

SCALE @ A1:	CHECKED:	APPROVED:
NTS	JS	PWW

PROJECT NO:	DESIGNED:	DRAWN:	DATE:
2024UK293315	NR	NR	JULY 2024

DRAWING NO:	REV:
288933-WSP-XX-XX-DR-C-1211	P03

File name: C:\USERS\NATEBOWEN\WSP\03602024\288933\TVC\LINTHORPE ROAD RESTORATION - DOCUMENTS\03602024\288933\WSP\XX-XX-DR-C-1211\TRAFFIC SIGNS DETAILS.DWG, printed on 10 October 2024 13:22:39 by NateBowen

Inputs are:
Funding
Resources

What will be delivered

What will be the direct result of
the activities (note outputs
relevant to investment priority)

Results or changes produced
by the intervention

Inputs (Funding/Resources)

TVCA funding
£2,387,585

Detailed Design

Advertisement of
Traffic Regulation
Orders

Activities (Monitor)

Reinstatement of 47
parking spaces

Removal of 1km
existing cycle way – in
both directions

Reinstatement of
previous TROs

Outputs (Quantify & Monitor)

47 pay & display parking
spaces installed

Removal of street
furniture within the
carriageway

Realignment of highway,
providing a more linear
layout

Reinstatement of bus
stops to previous location

Outcomes (Monitor & Evaluate)

Eradicate all accidents associated
with pedestrian conflict with
cycle infrastructure

45,805 extra Pay and Display
users

Reduction in vacant units

Greater satisfaction with highway
users, including bus operators

Increased Pay and Display income
of £84,913.94 p.a.

High Value VfM

Direct Construction Costs

Base Construction Cost (Q2 2021 Prices)	£891,445
Allowance for traffic signals	£150,000

Indirect Construction Costs

Main Contractors preliminaries and traffic management	£325,000
Overheads and Profits	£136,644
Fees (design, construction PM, CDM)	£263,041
Benefits Realisation Report	£30,000

Total excl. Risk and Inflation	£1,796,130
---------------------------------------	-------------------

Risk	£285,518
------	----------

Inflation	£305,937
-----------	----------

Total	£591,455
--------------	-----------------

Total cost	£2,387,585
-------------------	-------------------



Appendix 1

TVCA Equalities Impact Assessment

Equality Impact Assessment (EqIA): Linthorpe Road Proposed Cycle Lane Removal

PART 1. OVERVIEW	
1.1 Decision Title: Name of policy/Project /Activity being assessed	Linthorpe Road Proposed Cycle Lane Removal
1.2 Date of Assessment	August 2024
1.3 Version number	1.0
1.4 Equality Analysis completed by: (Names & Job Titles)	Maisie Chadwick/ Emily Jones – WSP Communications & Engagement
1.5 Lead Director & Authoriser Senior Responsible Officer (if different)	TBC
1.6 Is the Activity: (Existing / Proposed / New)	Proposed
PART 2. PURPOSE AND SCOPE	
2.1 Summary of aims and objectives of the Decision (policy/funding/activity/event)	<p>The Tees Valley Combined Authority (TVCA) is proposing the removal of the segregated cycle lane on Linthorpe Road. The cycle lane was introduced in 2022 but since then has received significant political opposition and negative feedback, particularly from businesses in the area. Safety concerns have also been raised.</p> <p>Both the Mayor of Middlesbrough, Chris Cooke and Tees Valley Mayor, Ben Houchen have pledged to remove the cycle lane during their electoral campaigns.</p>

	<p>The proposal is to remove the cycle lanes on Linthorpe Road and revert the road back to its original layout, including reinstating the on-street parking and reopening side streets (with the exception of Victoria Road and King Edward Square which would remain closed). The current 20mph speed limit will be retained, and blue badge car parking will be retained on Waterloo Road, subject to Middlesbrough’s Executive Approval.</p> <p>The removal of the cycle lane and reinstatement of the previous road layout will be funded by the TVCA.</p>
<p>2.2 What involvement and consultation has been done in relation to this Decision? <i>(e.g. with relevant groups and stakeholders)</i></p>	<p>Internal discussions between TVCA and Middlesbrough Borough Council (MBC), including with their Accessibility Officer representing the views of different groups and users needs across the authority. MBC’s Accessibility Officer has reviewed and provided recommendations, which will be considered as part of the final design.</p> <p>TVCA and MBC have also held discussions with political stakeholders, including Ben Houchen and Chris Cooke. The proposal is subject to approval by Middlesbrough’s Executive Team with regards to which Traffic Regulation Orders (TROs) they would advertise.</p> <p>Furthermore, the plans will be presented to Active Travel England’s design review panel for consideration and review.</p> <p>A public consultation on the proposal was held from 5 August 2024 to 2 September 2024. Following this, the detailed design and statutory consultation i.e. Traffic Regulation Orders (TROs) which are due to be concluded by the end of 2024, subject to MBC’s Executive Committee.</p> <p>An accessibility workshop was held on Tuesday 27 August 2024 with the RNIB (Royal National Institute of Blind People) in attendance. The workshop aimed to understand the impact of the proposal on those who are visually or partially sighted. It was noted at the workshop that the RNIB were in favour of the scheme’s removal, however it is noted that further engagement will be required from other disability and seldom heard groups.</p>
<p>PART 3. ANALYSIS OF IMPACT ON PEOPLE</p>	
<p>3.1 Who is affected by Decision (policy/funding activity/event)?</p>	<p>The cycle lane removal will impact several groups: Residents will be affected as the proposals spans from Borough Road to Ayresome Street. There will be better access</p>

	<p>to and from Linthorpe Road for motorists due to the reopening of some side roads.</p> <p>Cyclists currently using the cycle lanes will be affected and will either need to cycle on the main carriageway with other vehicles or choose an alternative route, although it should be noted that the 20mph may remain, improving safety on the road for cyclists. Existing cycle parking will also be removed on Linthorpe Road and on side closed roads such as King Edward’s Square.</p> <p>Businesses on Linthorpe Road will be affected, as there will be increased parking and vehicle access. There will be more loading bay availability. This could lead to a boost in the local economy and an increase in footfall along the route.</p> <p>The removal of the bus islands will remove potential conflicts between cyclists, pedestrians and bus users. However, the existing infrastructure has been designed to LTN1/20 standard.</p> <p>Pedestrians on Linthorpe Road will benefit from some areas of widened footways and narrowed crossing distances. The ambiance of the road for pedestrians may be negatively impacted due to increased numbers of on street parking.</p> <p>Staff and students at Teesside University traveling by car on Linthorpe Road may be affected by longer travel times, especially when accessing Clarendon car park.</p> <p>Staff and users of the One Life Medical Centre by Ayresome Gardens may be impacted. The impacts will depend on their current mode of travel, but with the removal of the bus islands, journeys on the road will be improved.</p> <p>All road users will be affected during the construction phase, although this will be mitigated through appropriate work times and construction methods wherever possible.</p>
--	---

PART 4. RESPONSE TO ANALYSIS, ACTION PLAN AND MONITORING

<p>4.1 What are the arrangements for monitoring and reviewing the actual impact of the Decision (policy/funding activity/event)?</p>	<p>TVCA will conduct regular progress reviews throughout the project’s design, implementation, monitoring and evaluation period. These reviews will assess whether the project is meeting its key objectives, including providing better access to businesses, boosting trade, and</p>
---	--

	<p>supporting the local economy. Reviews will consider the impacts on all interests including residents, businesses and road users by mode. SMART objectives will be developed and will be monitored post-delivery in line with the key objectives.</p> <p>Monitoring is anticipated to include:</p> <ul style="list-style-type: none">Traffic counts which will be undertaken pre- and post-delivery in order to measure the impact in terms of modes used. This will include pedestrian, traffic and cycle counts.Parking surveys including parking accumulation and turn over/duration surveys.Accident levels pre- and post-delivery – including pedestrian/cycle accidents not only vehicle accidents.Information on business turn over and vacancy rates will be considered alongside feedback from the businesses themselves. This is to be considered in the context of wider economic trends.
--	---

PART 5: DATA								
Baseline Table		England %	Tees Valley %	Darlington %	Hartlepool %	Middlesbrough %	Redcar & Cleveland %	Stockton-on-Tees %
Census 2021 data								
Age	0-15	18.6	19.2	18.3	19.1	20.8	17.7	19.6
	16-64	63.0	61.1	61.3	61.1	62.4	59.0	61.5
	65+	18.4	19.7	20.4	19.7	16.8	23.3	18.9
Disability in household	No long-term disability	68.0	63.4	66.2	60.3	62.8	61.7	64.9
	Long-term disability	32.0	36.6	33.8	39.7	37.2	38.3	35.1
Gender (sex)	Male	49.0	48.9	48.9	48.4	49.3	48.4	49.1
	Female	51.0	51.1	51.1	51.6	50.7	51.6	50.9
Marriage / Civil partnership	Married or in civil partnership	44.7	43.3	44.2	42.3	39.4	44.0	45.6
	Single / other	55.3	56.7	55.8	57.7	60.6	56.0	54.4
Race / ethnicity	Asian / Asian British / Asian Welsh	9.6	4.4	2.8	1.7	10.5	0.8	4.6
	Black / Black British / Black Welsh / Caribbean or African	4.2	1.1	0.7	0.5	2.7	0.2	1.1
	Mixed / Multiple ethnic groups	3.0	1.3	1.4	0.7	2.1	0.9	1.4
	White	81.0	92.1	94.4	96.5	82.4	97.7	92.0
	Other ethnic group	2.2	1.1	0.9	0.6	2.4	0.4	0.8
Religion / belief	No religion	36.7	39.0	39.7	40.1	36.4	40.2	39.1
	Christian	46.3	50.7	52.1	52.5	46.0	53.0	51.1
	Buddhist	0.5	0.3	0.3	0.2	0.3	0.2	0.3
	Hindu	1.8	0.5	0.4	0.2	1.0	0.1	0.4
	Jewish	0.5	0.0	0.0	0.0	0.0	0.0	0.0
	Muslim	6.7	3.8	1.7	1.3	10.2	0.7	3.4
	Sikh	0.9	0.3	0.4	0.2	0.4	0.1	0.4

	Other religion	0.6	0.3	0.4	0.3	0.3	0.4	0.3
	Not answered	6.0	5.2	4.9	5.1	5.3	5.3	5.0
Sexual Orientation	Straight or heterosexual	89.4	91.0	91.4	91.1	89.5	91.3	91.6
	Gay or lesbian	1.5	1.5	1.5	1.5	1.6	1.3	1.4
	Bisexual	1.3	1.0	1.1	1.0	1.2	0.9	0.9
	All other orientations	0.3	0.3	0.3	0.2	0.4	0.2	0.2
	Not answered	7.5	6.2	5.7	6.1	7.3	6.3	5.9
ONS and IMD data								
Socio-economic status	IMD average rank score	N/A	N/A	18284.63	22581.98	23729.10	20348.40	17541.72
	IMD Proportion of LSOAs in most deprived decile rank (out of 317 local authority areas)	N/A	2 nd most deprived LEP area out of 38	47 th most deprived	10 th most deprived	Most deprived district in England	29 th most deprived	39 th most deprived
	16+ Unemployment rate % (Jul'22-Jun'23)	3.8	3.7	2.5	4.9	5.6	2.4	3.5
	Median F/T weekly wage £ (2022)	645.8	580.4	558.2	593.7	555.0	568.4	609.4

Source: ONS, Census 2021, IMD 2019

PART 6: IMPACT ASSESSMENT BY COHORT			
Cohort: Protected Group/ Other	Impact effect (Positive/ Negative/ Neutral)	Please explain the identified impact(s) Provide examples, relate to any evidence/data used	Measures that will be taken to mitigate negative impacts, and/or maximise opportunities
Age	Neutral	<p>People of all ages travelling by car/taxi will be able to park on Linthorpe Road or be dropped off/ picked up by taxis more easily. They will benefit from better access to the businesses / properties along the route.</p> <p>Older people travelling by car and using businesses or services on Linthorpe Road may benefit from the increase in available parking, improved access from side roads and increased taxi drop off and rank availability. This group may also benefit from the simplification of the road layout including the removal of floating bus stops.</p> <p>Younger people are more likely to cycle and therefore could be negatively impacted by the proposed removal of the cycle lane.</p>	<p>Actively target older population groups during the consultation period. Provide alternative means for older people to engage with the consultation process.</p> <p>Maintain clear communication throughout consultation and moving into next design stage – not just relying on online platforms.</p> <p>Accessible survey and content made available upon telephone or email request. Option to also request printed surveys and return via post.</p>
Disability	Positive and Negative	<p>The proposal will allow for closer on street parking (and drop off/pick up facilities) to many of the local businesses, making them more accessible for people with mobility impairments. (Though noting that these are not currently proposed to be blue badge parking spaces, but general parking).</p> <p>The floating bus stops have been raised as an issue by people with sensory impairments. The proposed removal of these could lead to improved safety and a perceived benefit by these user groups.</p>	<p>TVCA held a workshop with the RNIB to understand the accessibility requirements of people with visual impairments. The Access Officer at Middlesbrough Council has reviewed the proposals and provided comments.</p> <p>The feedback has been taken on board and the groups engaged will be kept up to date with information on the design and site works going forward in order that any further concerns can be considered.</p> <p>Engage with further disability user groups to ensure views of others with differing needs within the community are considered and heard.</p>

		<p>Removal of the orcas and fixings that formed the segregation of the cycle lane may benefit those with sensory (particularly visual) impairments where people found these difficult to cross and a trip hazard.</p> <p>Physically impaired people walking along Linthorpe Road may benefit from the pedestrian crossings and appropriate tactile features at crossing points resulting in shorter crossing distances.</p> <p>The proposals may negatively impact cyclists including those who with physical, sensory, intellectual impairments, hidden disabilities and other conditions where users may prefer a segregated cycle route for safety rather than mixing with general traffic.</p> <p>The increase in parking availability and focus on vehicular traffic resulting from the cycle lane removal may lead to increased traffic levels. This could make the road and side streets less permeable for pedestrians, particularly impacting those with mobility difficulties.</p>	
Gender (Sex)	Slight negative	Women are potentially more likely to prefer segregated cycle lanes and are more likely to cycle where these are in place, compared to men. With the proposed removal of the cycle lane, women may feel less safe when cycling and may be less likely to cycle at all without dedicated cycle lanes in place on this busy route.	Engage with community groups including women's and cycling groups as the scheme progresses to provide information and ensure any concerns are considered and mitigated where necessary.
Gender reassignment	Neutral	No specific impact identified. The proposals will not discriminate in any way based on gender reassignment.	No specific mitigations
Marriage or civil partnership	Neutral	No specific impact identified. The proposals will not discriminate in any way based on marriage or civil partnership.	No specific mitigations
Pregnancy and maternity	Neutral	No specific impact identified. The proposal allows for closer parking to local businesses, making them more	No specific mitigations

		<p>accessible for pregnant women using vehicles on Linthorpe Road. This could benefit pregnant women in later pregnancy when mobility can become more difficult for some.</p> <p>Pregnant woman who use the dedicated cycle lane during earlier pregnancy will be negative affected if the cycle lane is removed.</p>	
Race / Ethnicity	Neutral	No specific impact identified. The proposals will not discriminate in any way based on race or ethnicity.	Engage with community groups as the scheme progresses to provide information and ensure any concerns are considered and mitigated where necessary.
Religion / belief	Neutral	<p>No specific impact identified. The proposals will not discriminate in any way based on religion or belief.</p> <p>There are no places of worship directly on this section of Linthorpe Road.</p> <p>Middlesbrough Community Church is located on Clifton Street, off Linthorpe Road. The scheme will not impact access to the church.</p>	Engage with local places of worship as the scheme progresses to provide information and ensure any concerns are considered and mitigated where necessary.
Sexual orientation	Neutral	No specific impact identified. The project will not discriminate in any way based on sexual orientation.	Engage with relevant community groups as the scheme progresses to provide information and ensure any concerns are considered and mitigated where necessary.
Deprived Communities	Positive and Negative	<p>People in deprived communities are less likely to own or have access to a car than on average. The proposal to remove the cycle lanes may mean these communities are negatively impacted.</p> <p>Those who do own or use a car or taxi to access businesses on Linthorpe Road may benefit from easier side road access, additional parking and drop off spaces.</p> <p>Additionally, those walking or those using a mobility device on Linthorpe Road will benefit from more space due to a wider footway.</p>	<p>Engage with local community groups as the scheme progresses to provide information and ensure any concerns are considered and mitigated where necessary.</p> <p>Engagement via key community representatives and local groups will be required to reach into deprived communities around Linthorpe Road as these communities will be less likely to engage online.</p>
Other (please state)			

PART 7: EVALUATION & AUTHORISATION	
Question	Summary & Recommendation
Is it possible the proposed Decision (policy/funding activity/event) or activity could discriminate or unfairly disadvantage people?	<p>There is the potential for the proposals to impact some protected groups, but measures to mitigate any potential impacts are being taken.</p> <p>This includes a workshop with the RNIB and establishing an Accessibility Working Group to gather views from accessibility and community groups in the area.</p> <p>The proposals will meet design guidance, as well as appropriate standards for disadvantaged people. Through future schemes, we will also connect Linthorpe and Acklam with the National Cycling Route at Emmerson Ave/Keith Road. This route allows for direct connections into Middlesbrough Town Centre and beyond.</p> <p>Ongoing engagement with the local community including those with protected characteristics through the detailed design and implementation will be required in order to ensure that any issues raised can be responded to and actioned. This engagement must include outreach via key community representatives and groups to ensure hard to reach communities are aware of the proposed changes and programme for implementation.</p>

For completion by the Lead Director (Authoriser)
Please identify the Option selected from below:

Option 1. <input checked="" type="checkbox"/>	Proceed with no major changes, with any small changes explained in the action plan at the end of the template
Option 2. <input type="checkbox"/>	Continue despite negative implications - because these are proportionate and cannot be avoided or mitigated, with the overall outcome important and overwhelmingly positive
Option 3. <input type="checkbox"/>	Adjust the proposal – there are some negative outcomes but you have identified actions to mitigate these - explain these in the action plan; or
Option 4. <input type="checkbox"/>	Do not proceed – you have identified major problems with serious impacts that cannot be avoided.

Will this EqIA be published? (EqIAs should always be published alongside relevant piece of work e.g. Cabinet papers, strategy and policy documents, calls and events)	<input type="checkbox"/> Yes <input type="checkbox"/> No If no, provide rationale:
Date completed	25/10/2024
Review date (if applicable)	
Lead Director (signature)	<i>Tam Bynst.</i>

EqIA ACTION PLAN: Linthorpe Road Cycleway Removal August 2024

Task No	Task Description	Task Owner	Completion Date	Outcome
1	Engage with Middlesbrough Borough Councils Accessibility Officer on the proposals.	BD	August 24	Added to stakeholder matrix, will consult on future Middlesbrough projects
2	Hold workshop with RNIB to understand impact on blind and partially sighted community.	BD	August 24	Tied feedback into consultation. Setting up a Guided Walk for TPOG to increase awareness of those visually impaired.
3	Complete further community outreach via community groups and representatives to	TVCA	In Progress	

	ensure all protected groups and those with differing access needs have opportunity to raise concerns or support prior to scheme being implemented.			
4	Ensure local communities including protected groups are kept up to date with progress through design and construction.	TVCA	In Progress	
5	Engage with ATE to mitigate potential impacts for protected groups and cyclists more generally.	TVCA	In Progress	



Linthorpe Road Cycle Lane Removal Consultation Summary

Introduction

Both the Middlesbrough Mayor Chris Cooke and the Tees Valley Mayor Ben Houchen have pledged to remove the Linthorpe Road cycle lane. On August 1 2024, the Tees Valley Combined Authority (TVCA) launched a consultation, running until September 2, to seek public views on the removal. The consultation proposed removing the cycle lanes between Borough Road and Ayresome Street, returning the road to its original layout, including reinstating car parking and bus stop locations, whilst maintaining the closure of Victoria Road.

Objectives and Approach

The objective of the consultation on the Linthorpe Road Cycle Lane removal is to gather public feedback on the proposed plan to return the road to its original layout. The plan aims to reinstate the previous road layout, which would restore parking spaces and improve access for businesses. The consultation seeks to understand public opinion on the impact of these changes on road users, residents, businesses, and the local economy.

TVCA has promoted and shared the consultation in a variety of ways to encourage local communities to respond and feedback:

- Consultation information and survey available online via the Tees Valley Combined Authority website
- Social media content shared by TVCA
- Political Briefing Notes
- Bus Operator Briefing Notes
- Stakeholder briefings held (where requested)

Feedback

Online Survey:

A consultation page was hosted on the TVCA website with information about the proposal. The page also featured a survey for public feedback. As part of our wider transport consultation occurring at the same time, we also received a number of responses regarding the Linthorpe Road cycle lane, all of which have been included in this report, resulting in a total of 390 responses.

1. When asked “**Are you responding as....?**” the public responded with the following:

- A resident of Middlesbrough: **59.2%**
- A Tees Valley resident living outside of Middlesbrough: **28.2%**
- A resident from outside the Tees Valley: **3.1%**
- A business owner or representative: **6.9%**
- A representative of a local community group or residents' association: **0.3%**
- On behalf of an educational establishment, such as a school or college: **0.5%**



On behalf of a charity, voluntary or community sector organisation (VCS): **1.3%**
Other (please specify): **2.3%**

2. When asked “**How can we improve Linthorpe Road?**” the public responded with the following:

Return to its layout as it was before the cycle lanes and pedestrian changes were made in September 2022: **55.21%**
Return to its layout but keeping the closures of all side streets, except for Victoria Road: **6.29%**
Return to its layout with some side streets remaining closed: **9.20%**
Keep it as it is now: **17.92%**
Other (please specify): **11.38%**

The survey responses show 70.7% support the removal of the cycle lanes on Linthorpe Road, with some differences of view regarding side street closures, as shown in the above.

(390 responses, 0 skipped this question).

3. When asked “**Car parking spaces were removed as part of the cycleway scheme. Would you like to see parking and/or loading space on Linthorpe Road reinstated?**” the public responded with the following:

Yes: **72.82%**
No: **27.18%**

(390 responses, 0 skipped this question).

4. When asked “**Please let us know what you think of the plan to remove the cycle lanes and reinstate the road**” the public were provided with an open text box for comments. When reviewing these comments, we categorised each as either supporting or opposing the proposal:

Remove Linthorpe Road Cycle Lane: **74.5%**
Keep Linthorpe Road Cycle Lane: **23.8%**
Not Applicable: **1.6%**

Reoccurring themes which were mentioned in responses include:

Business owners state that the cycle lanes reduce car parking, affecting trade, loading and delivery.
Comments that the cycle lanes have worsened traffic congestion. The reduction in road width and changes to bus stop designs have led to longer delays and difficulties for drivers, particularly during peak hours.
Comments that the cycle lane’s layout is said to obstruct emergency vehicles, making it harder for them to access Linthorpe Road.
Comments the cycle lanes are not well-used compared to the disruption they cause.
Comments about the cycle lane’s role in improving cyclist safety and encouraging active travel.
Comments that cycle lanes promote health and environmental goals, benefiting Middlesbrough’s residents in the long run.
Comments that other countries show that well-designed cycle lanes can enhance local economies and public health.
Some comments include fixing current issues rather than removing the cycle lanes.

(369 responses, 21 skipped this question).

5. When asked **“Are there any existing safety concerns you would like to see addressed?”** the public responded with the following:

The responses highlight concerns regarding safety and functionality of the current road layout and pedestrian infrastructure, key issues include:

Many responses mention that the scheme previously used orcas to maintain segregation from road users and pedestrians. Responses highlight the orcas as hazards for pedestrians, especially the elderly and those with mobility issues.

Concerns about narrow road lanes, confusing layouts, and the impact on emergency vehicles were also frequently raised.

While some responses appreciate the intention behind the cycle lanes, others argue that the design is flawed, leading to dangerous situations for both cyclists and pedestrians. Issues such as illegal parking in cycle lanes and cyclists using footpaths.

There is widespread concern about pedestrians inadvertently entering cycle lanes and the risk posed by cyclists riding on pavements despite having designated lanes.

Responses suggest various solutions, such as more crossings, better signage, speed restrictions, and improved enforcement of parking regulations. Some comments suggest a return to the previous road layout, while others propose modifications to enhance safety for all users.

(313 responses, 77 skipped this question).

6. When asked **“Is there anything else you would like to tell us?”** the public responded with the following:

There are recurring themes regarding improving the Middlesbrough and Linthorpe area, concerns about safety and making the area better.

Several responses felt that removing the scheme would improve business and or that businesses have been negatively impacted by the scheme.

A number of comments included looking at how pedestrians use Linthorpe road, including pavement space being cluttered and not enough space or consideration of pedestrians in the designs.

Responses also noted to improve layouts and consideration of one-way streets in the area, alongside traffic calming, signage and 20 mph and loading zones during key times.

There were comments both for and against increasing and removing parking; more respondents requested easier access and additional parking, but some noted that there is sufficient parking in the area; while others requested the scheme to be improved for public transport and taxis.

Other responses included concerns about enforcement of parking issues, safe bike storage, safety for cycle delivery, elderly mobility issues, and access for emergency vehicles.



Several responses raised concerns pertaining to the cost of building and the subsequent removal costs of the scheme.

Consultation, evidence and business case processes were raised as concerns.

More promotion of active travel was raised as well as concerns that removing a scheme benefiting more active travel would adversely affect the health and well-being of residents.

(270 responses, 120 skipped this question).

Bus Operators:

TVCA shared a briefing note outlining the proposal to remove the cycle lanes on Linthorpe Road with both Stagecoach and Arriva, the operators of bus services along this route. Stagecoach strongly advocated for the removal of the cycle lane, citing significant delays to their services entering Middlesbrough due to the current road layout.

The operators support reinstating key bus stops, including at The Crown/King Edward Square, Granville Road, and outside One Life Medical Centre, with recommendations to consolidate stops into double-length ones for improved traffic flow and accessibility. They also urge a return to a wider highway gauge to address safety concerns.

Conclusion

The online survey results show a strong public preference (70.7%) for removing the Linthorpe Road cycle lanes, with some variation in opinions on side street closures. A significant majority (72.82%) also support reinstating parking and loading spaces along the route. Most respondents advocate for removing the cycle lanes, citing negative impacts on traffic, emergency vehicle access, and businesses. Additionally, concerns about pedestrian safety and the flawed design of the cycle lanes were highlighted, with suggestions for better signage and road layouts. Bus operators echo these concerns, pointing to service delays and safety issues due to the current road layout.

Further information

For additional information on the proposal or the consultation activities please contact:
consultations@teesvalley-ca.gov.uk

Document & Drawing Issue Sheet

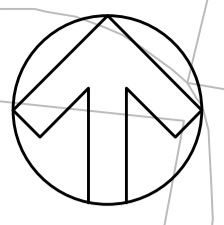


Project No: 2024UK288933
 Project Name: TVCA Linthorpe Road Restoration
 Date of Issue: 10-Oct-2024
 Purpose of Issue: Client Approval
 Method of Issue: Email
 Number of items Issued: 28
 Format of Issue: PDF
 Service: DI

Issue No: 5
 Issuing Office:
 #N/A

Issued to:		No of Recipients: 1
Company	Name	Position/Function
Tees Valley Combined Authority	Craig MacLennan	

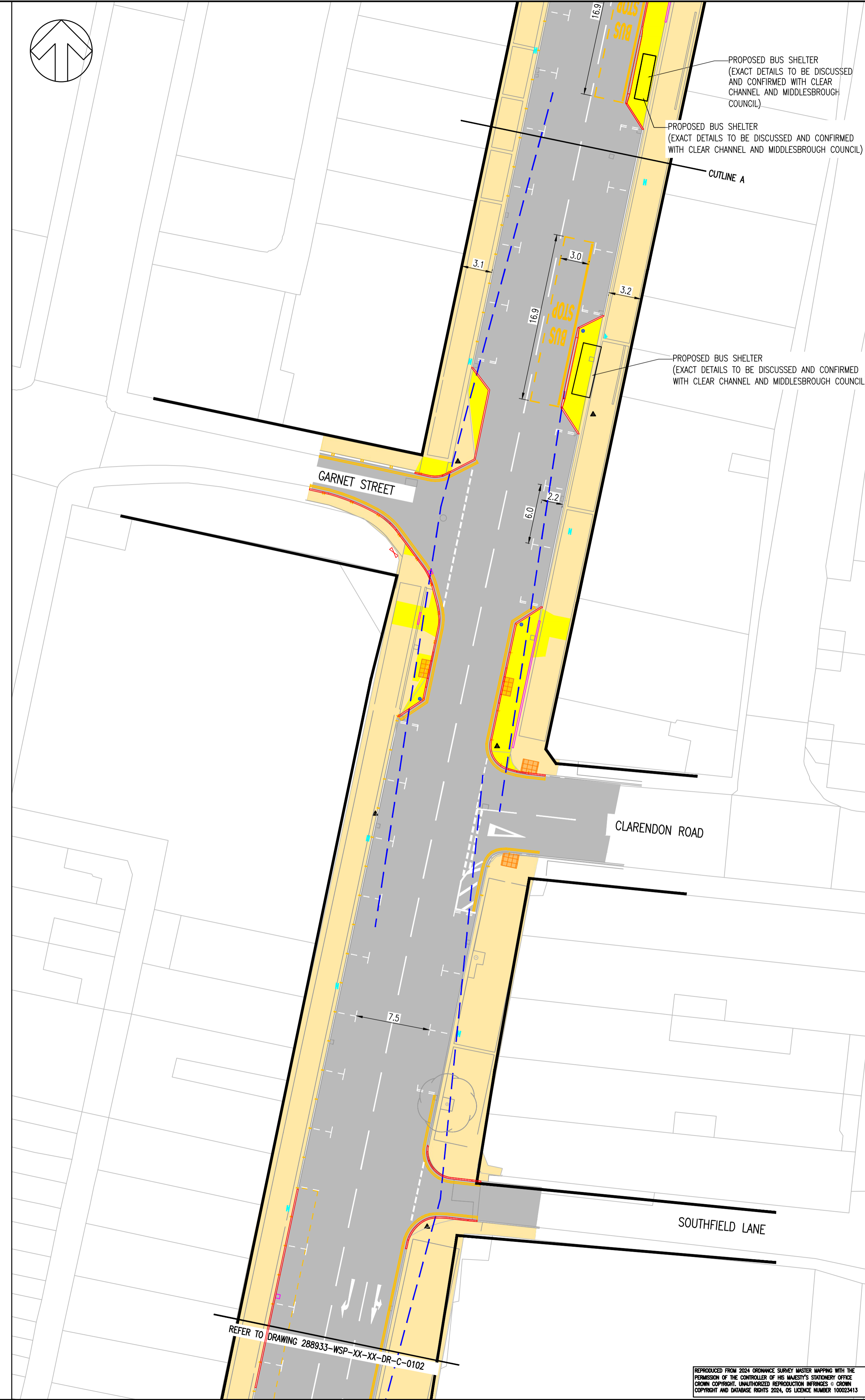
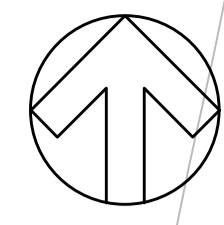
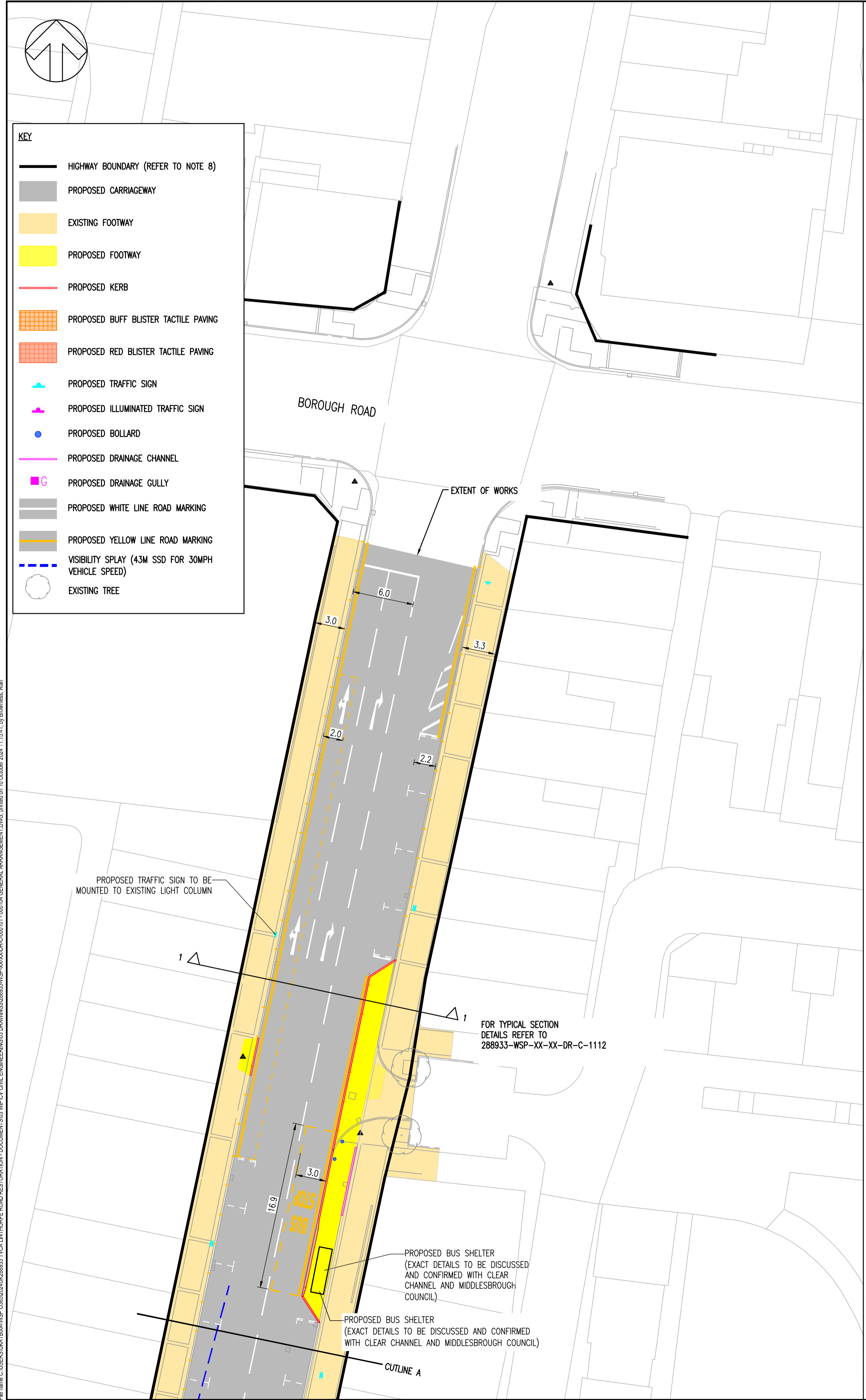
Dwg/Doc Number	Title	Scale	Size	Revision
288933-WSP-XX-XX-DR-C-0101	General Arrangement (Sheet 1 of 4)	1:250	A1	P04
288933-WSP-XX-XX-DR-C-0102	General Arrangement (Sheet 2 of 4)	1:250	A1	P04
288933-WSP-XX-XX-DR-C-0103	General Arrangement (Sheet 3 of 4)	1:250	A1	P04
288933-WSP-XX-XX-DR-C-0104	General Arrangement (Sheet 4 of 4)	1:250	A1	P04
288933-WSP-XX-XX-DR-C-0151	Setting Out (Sheet 1 of 4)	1:250	A1	P02
288933-WSP-XX-XX-DR-C-0152	Setting Out (Sheet 2 of 4)	1:250	A1	P02
288933-WSP-XX-XX-DR-C-0153	Setting Out (Sheet 3 of 4)	1:250	A1	P02
288933-WSP-XX-XX-DR-C-0154	Setting Out (Sheet 4 of 4)	1:250	A1	P02
288933-WSP-XX-XX-DR-C-0155	Setting Out	1:250	A1	P02
288933-WSP-XX-XX-DR-C-0201	Site Clearance (Sheet 1 of 4)	1:250	A1	P04
288933-WSP-XX-XX-DR-C-0202	Site Clearance (Sheet 2 of 4)	1:250	A1	P04
288933-WSP-XX-XX-DR-C-0203	Site Clearance (Sheet 3 of 4)	1:250	A1	P04
288933-WSP-XX-XX-DR-C-0204	Site Clearance (Sheet 4 of 4)	1:250	A1	P04
288933-WSP-XX-XX-DR-C-0501	Drainage Layout (Sheet 1 of 4)	1:250	A1	P04
288933-WSP-XX-XX-DR-C-0502	Drainage Layout (Sheet 1 of 4)	1:250	A1	P04
288933-WSP-XX-XX-DR-C-0503	Drainage Layout (Sheet 1 of 4)	1:250	A1	P04
288933-WSP-XX-XX-DR-C-0504	Drainage Layout (Sheet 1 of 4)	1:250	A1	P04
288933-WSP-XX-XX-DR-C-1101	Kerbs, Footways & Paved Areas (Sheet 1 of 4)	1:250	A1	P04
288933-WSP-XX-XX-DR-C-1102	Kerbs, Footways & Paved Areas (Sheet 2 of 4)	1:250	A1	P04
288933-WSP-XX-XX-DR-C-1103	Kerbs, Footways & Paved Areas (Sheet 3 of 4)	1:250	A1	P04
288933-WSP-XX-XX-DR-C-1104	Kerbs, Footways & Paved Areas (Sheet 4 of 4)	1:250	A1	P04
288933-WSP-XX-XX-DR-C-1201	Traffic Signs & Road Markings (Sheet 1 of 4)	1:250	A1	P04
288933-WSP-XX-XX-DR-C-1202	Traffic Signs & Road Markings (Sheet 2 of 4)	1:250	A1	P04
288933-WSP-XX-XX-DR-C-1203	Traffic Signs & Road Markings (Sheet 3 of 4)	1:250	A1	P04
288933-WSP-XX-XX-DR-C-1204	Traffic Signs & Road Markings (Sheet 4 of 4)	1:250	A1	P04
288933-WSP-XX-XX-DR-C-1211	Traffic Sign Details (Sheet 1 of 3)	NTS	A1	P03
288933-WSP-XX-XX-DR-C-1212	Traffic Sign Details (Sheet 2 of 3)	NTS	A1	P03
288933-WSP-XX-XX-DR-C-1213	Traffic Sign Details (Sheet 3 of 3)	NTS	A1	P03



KEY

- HIGHWAY BOUNDARY (REFER TO NOTE 8)
- PROPOSED CARRIAGEWAY
- EXISTING FOOTWAY
- PROPOSED FOOTWAY
- PROPOSED KERB
- PROPOSED BUFF BLISTER TACTILE PAVING
- PROPOSED RED BLISTER TACTILE PAVING
- PROPOSED TRAFFIC SIGN
- PROPOSED ILLUMINATED TRAFFIC SIGN
- PROPOSED BOLLARD
- PROPOSED DRAINAGE CHANNEL
- PROPOSED DRAINAGE GULLY
- PROPOSED WHITE LINE ROAD MARKING
- PROPOSED YELLOW LINE ROAD MARKING
- VISIBILITY SPLAY (43M SSD FOR 30MPH VEHICLE SPEED)
- EXISTING TREE

File name: C:\USERS\WATB001\NSP_03602024\288933_TVC\LINTHORPE ROAD RESTORATION - DOCUMENTS\01 MP\CV\01 CIVIL ENGINEERING\03 DRAWINGS\288933-WSP-XX-XX-DR-C-00011 - 001014_GENERAL ARRANGEMENT.DWG, printed on 10 October 2024 11:54:41, by Grawness, Alan



DO NOT SCALE

- NOTES**
1. THIS DRAWING SHOULD BE READ IN CONJUNCTION WITH ALL RELATED DOCUMENTATION, DRAWINGS AND STANDARD DETAILS.
 2. THE ORIGINAL OF THIS DRAWING WAS PRODUCED IN COLOUR - A MONOCHROME COPY CANNOT BE RELIED ON.
 3. ALL DIMENSIONS ARE IN METRES UNLESS NOTED OTHERWISE.
 4. INFORMATION REGARDING THE LOCATION AND DEPTH OF EXISTING SERVICES CANNOT BE GUARANTEED BY THE STATUTORY UNDERTAKER AND SHOULD BE INVESTIGATED. THE CONTRACTOR SHALL ESTABLISH THE PRECISE LINE AND DEPTH OF ALL EXISTING UTILITIES PRIOR TO COMMENCING ANY EXCAVATION WORK.
 5. THE DESIGN IS BASED ON TOPOGRAPHICAL SURVEY REFERENCE ASS 2963 BY ADH SITE SURVEYS LIMITED RECEIVED FEBRUARY 2021
 6. REFERENCE SHOULD BE MADE TO INDIVIDUAL DISCIPLINE DRAWINGS AND THE DESIGN RISK MANAGEMENT SCHEDULE FOR FURTHER HEALTH AND SAFETY INFORMATION - NOT ALL RISKS ARE IDENTIFIED IN THIS DRAWING.
 7. CONFLICTING INFORMATION SHOWN ON THE ENGINEER'S DRAWINGS OR DISCREPANCIES BETWEEN THE INFORMATION GIVEN BY THE ENGINEER AND THAT PROVIDED BY OTHERS MUST BE REFERRED TO THE ENGINEER BEFORE THE WORKS COMMENCE.
 8. THIS DRAWING IS BASED IN A WHOLE OR IN PART ON INFORMATION AND/OR DATA APPROVED BY A THIRD PARTY INCLUDING MIDDLESBROUGH COUNCIL AND AS SUCH IN CONTINGENT UPON THE ACCURACY AND COMPLETENESS OF THAT INFORMATION AND/OR DATA. WSP UK LIMITED SHALL NOT BE LIABLE IN RELATION TO ANY INACCURACY PRESENTED WITHIN THE DRAWING AND/OR IN RELATION TO ANY DISCREPANCY OR AMBIGUITY WITH ANY OTHER PROJECT DOCUMENTS SHOULD ANY DATA AND/OR INFORMATION UPON WHICH RELIANCE HAS BEEN BASED BE INCORRECT OR OUT OF DATE OR HAVE BEEN CONCEALED, WITHHELD OR BE INCOMPLETE, MISREPRESENTED OR OTHERWISE NOT FULLY DISCLOSED TO WSP UK LIMITED.

SAFETY HEALTH & ENVIRONMENTAL INFORMATION BOX

HEALTH AND SAFETY SYMBOLS LEGEND

	INDICATES A RESIDUAL RISK REQUIRING A COMPULSORY ACTION		INDICATES A RESIDUAL RISK FOR INFORMATION
	INDICATES A RESIDUAL RISK REQUIRING A PROHIBITIVE ACTION		INDICATES A RESIDUAL RISK AS A WARNING
	INDICATES AN ENVIRONMENTAL HAZARD		

This table is provided to assist the Principal Contractor to fulfill their obligations under the Construction Design & Management Regulations, 2015. It does not include residual risks that a competent Contractor will be aware of nor does it absolve the Principal Contractor of their legal responsibilities.
The reference number corresponds to the Design Risk Management Schedule.

UNTIL TECHNICAL APPROVAL HAS BEEN OBTAINED FROM THE RELEVANT LOCAL AUTHORITIES OR STATUTORY BODIES, IT SHOULD BE UNDERSTOOD THAT ALL DRAWINGS ARE ISSUED AS PRELIMINARY AND NOT FOR CONSTRUCTION. SHOULD THE CONTRACTOR AND / OR EMPLOYER COMMENCE WORK PRIOR TO APPROVAL BEING GIVEN, IT IS ENTIRELY AT THEIR OWN RISK

REV	DATE	BY	DESCRIPTION	CHK	APP
P04	10/10/24	AB	UPDATED FOLLOWING CLIENT REQUIREMENTS	JS	PWW
P03	03/09/24	NR	UPDATED FOLLOWING CLIENT REVIEW	JS	PWW
P02	31/07/24	NR	ISSUED FOR CLIENT APPROVAL	JS	PWW
P01	23/07/24	NR	ISSUE FOR COST ESTIMATE	JS	PWW
DR	10/07/24	NR	DRAFT ISSUE	JS	PWW

DRAWING STATUS:

wsp

Amber Court, William Armstrong Drive, Newcastle upon Tyne, NE4 7YQ, UK
T+ 44 (0) 191 226 2000, F+ 44 (0) 191 226 2104
wsp.com

CLIENT:

TEES VALLEY

SITE/PROJECT:

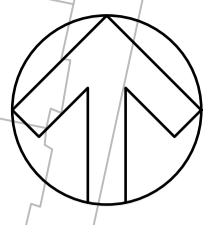
LINTHORPE ROAD RESTORATION

TITLE:

GENERAL ARRANGEMENT
(SHEET 1 OF 4)

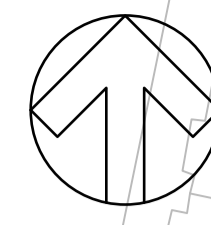
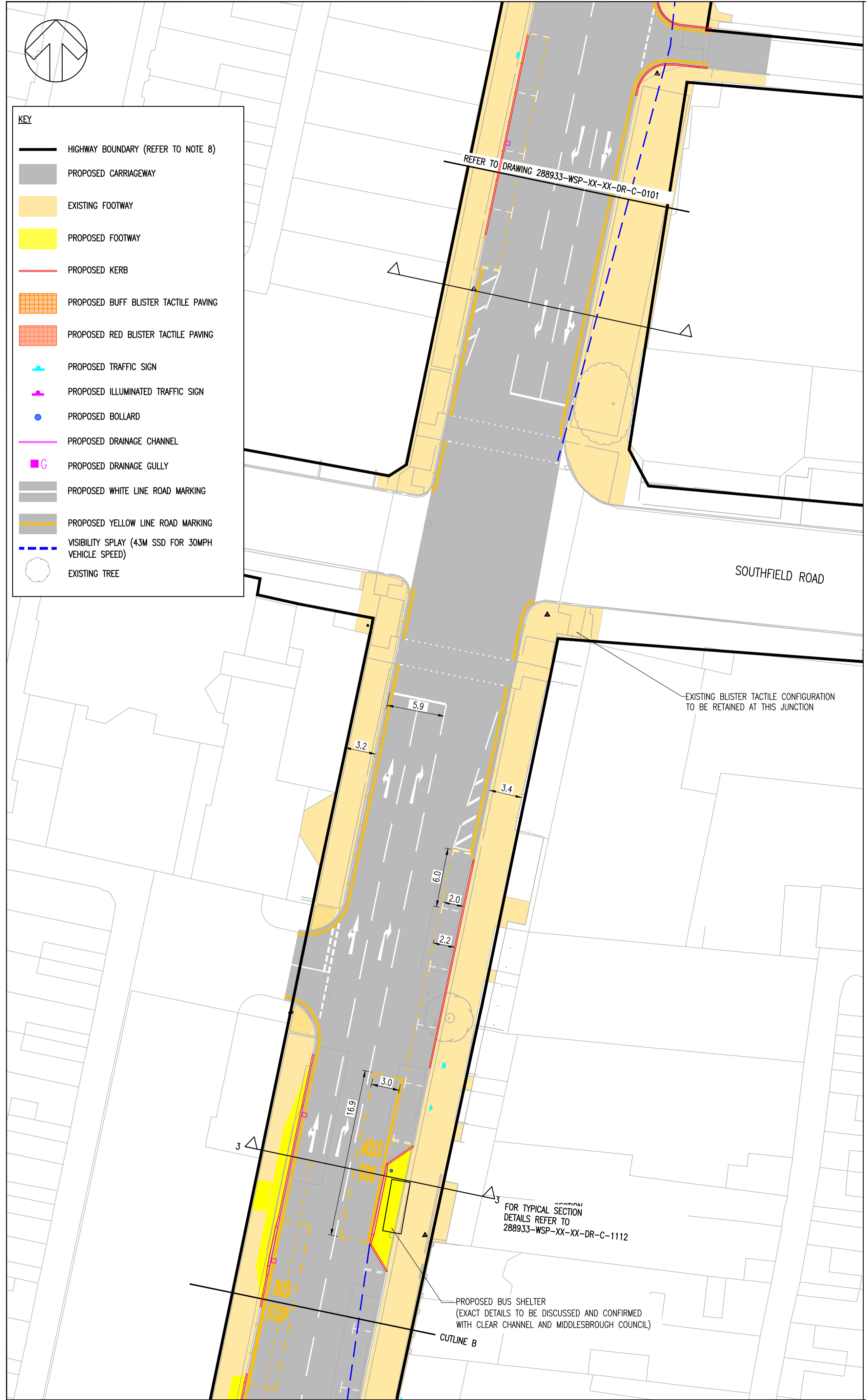
SCALE @ A1:	1:250	CHECKED:	JS	APPROVED:	PWW
PROJECT NO:	2024UK293315	DESIGNED:	NR	DRAWN:	NR
DRAWING NO:	288933-WSP-XX-XX-DR-C-000101	DATE:	JULY 2024	REV:	P04

REPRODUCED FROM 2024 ORDNANCE SURVEY MASTER MAPPING WITH THE PERMISSION OF THE CONTROLLER OF HIS MAJESTY'S STATIONERY OFFICE. CROWN COPYRIGHT. UNAUTHORIZED REPRODUCTION INFRINGES © CROWN COPYRIGHT AND DATABASE RIGHTS 2024, OS LICENSE NUMBER 10002413



KEY

- HIGHWAY BOUNDARY (REFER TO NOTE 8)
- PROPOSED CARRIAGEWAY
- EXISTING FOOTWAY
- PROPOSED FOOTWAY
- PROPOSED KERB
- PROPOSED BUFF BLISTER TACTILE PAVING
- PROPOSED RED BLISTER TACTILE PAVING
- PROPOSED TRAFFIC SIGN
- PROPOSED ILLUMINATED TRAFFIC SIGN
- PROPOSED BOLLARD
- PROPOSED DRAINAGE CHANNEL
- PROPOSED DRAINAGE GULLY
- PROPOSED WHITE LINE ROAD MARKING
- PROPOSED YELLOW LINE ROAD MARKING
- VISIBILITY SPLAY (43M SSD FOR 30MPH VEHICLE SPEED)
- EXISTING TREE



DO NOT SCALE

NOTES

1. THIS DRAWING SHOULD BE READ IN CONJUNCTION WITH ALL RELATED DOCUMENTATION, DRAWINGS AND STANDARD DETAILS.
2. THE ORIGINAL OF THIS DRAWING WAS PRODUCED IN COLOUR - A MONOCHROME COPY CANNOT BE RELIED ON.
3. ALL DIMENSIONS ARE IN METRES UNLESS NOTED OTHERWISE.
4. INFORMATION REGARDING THE LOCATION AND DEPTH OF EXISTING SERVICES CANNOT BE GUARANTEED BY THE STATUTORY UNDERTAKER AND SHOULD BE INVESTIGATED. THE CONTRACTOR SHALL ESTABLISH THE PRECISE LINE AND DEPTH OF ALL EXISTING UTILITIES PRIOR TO COMMENCING ANY EXCAVATION WORK.
5. THE DESIGN IS BASED ON TOPOGRAPHICAL SURVEY REFERENCE ASS 2963 BY ADH SITE SURVEYS LIMITED RECEIVED FEBRUARY 2021
6. REFERENCE SHOULD BE MADE TO INDIVIDUAL DISCIPLINE DRAWINGS AND THE DESIGN RISK MANAGEMENT SCHEDULE FOR FURTHER HEALTH AND SAFETY INFORMATION - NOT ALL RISKS ARE IDENTIFIED IN THIS DRAWING.
7. CONFLICTING INFORMATION SHOWN ON THE ENGINEER'S DRAWINGS OR DISCREPANCIES BETWEEN THE INFORMATION GIVEN BY THE ENGINEER AND THAT PROVIDED BY OTHERS MUST BE REFERRED TO THE ENGINEER BEFORE THE WORKS COMMENCE.
8. THIS DRAWING IS BASED IN A WHOLE OR IN PART ON INFORMATION AND/OR DATA APPROVED BY A THIRD PARTY INCLUDING MIDDLESBROUGH COUNCIL AND AS SUCH IN CONTINGENT UPON THE ACCURACY AND COMPLETENESS OF THAT INFORMATION AND/OR DATA WSP UK LIMITED SHALL NOT BE LIABLE IN RELATION TO ANY INACCURACY PRESENTED WITHIN THE DRAWING AND/OR IN RELATION TO ANY DISCREPANCY OR AMBIGUITY WITH ANY OTHER PROJECT DOCUMENTS SHOULD ANY DATA AND OR INFORMATION UPON WHICH RELIANCE HAS BEEN BASED BE INCORRECT OR OUT OF DATE OR HAVE BEEN CONCEALED, WITHHELD OR BE INCOMPLETE, MISREPRESENTED OR OTHERWISE NOT FULLY DISCLOSED TO WSP UK LIMITED.

SAFETY HEALTH & ENVIRONMENTAL INFORMATION BOX

HEALTH AND SAFETY SYMBOLS LEGEND

	INDICATES A RESIDUAL RISK REQUIRING A COMPULSORY ACTION		INDICATES A RESIDUAL RISK FOR INFORMATION
	INDICATES A RESIDUAL RISK REQUIRING A PROHIBITIVE ACTION		INDICATES A RESIDUAL RISK AS A WARNING
	INDICATES AN ENVIRONMENTAL HAZARD		

This table is provided to assist the Principal Contractor to fulfill their obligations under the Construction Design & Management Regulations, 2015. It does not include residual risks that a competent Contractor will be aware of nor does it absolve the Principal Contractor of their legal responsibilities.
The reference number corresponds to the Design Risk Management Schedule.

UNTIL TECHNICAL APPROVAL HAS BEEN OBTAINED FROM THE RELEVANT LOCAL AUTHORITIES OR STATUTORY BODIES, IT SHOULD BE UNDERSTOOD THAT ALL DRAWINGS ARE ISSUED AS PRELIMINARY AND NOT FOR CONSTRUCTION. SHOULD THE CONTRACTOR AND / OR EMPLOYER COMMENCE WORK PRIOR TO APPROVAL BEING GIVEN, IT IS ENTIRELY AT THEIR OWN RISK

REV	DATE	BY	DESCRIPTION	CHK	APP
P04	10/10/24	AB	UPDATED FOLLOWING CLIENT REQUIREMENTS	JS	PWW
P03	03/09/24	NR	UPDATED FOLLOWING CLIENT REVIEW	JS	PWW
P02	31/07/24	NR	ISSUED FOR CLIENT APPROVAL	JS	PWW
P01	23/07/24	NR	ISSUE FOR COST ESTIMATE	JS	PWW
DR	10/07/24	NR	DRAFT ISSUE	JS	PWW

DRAWING STATUS:

wsp

Amber Court, William Armstrong Drive, Newcastle upon Tyne, NE4 7YQ, UK
T+44 (0) 191 226 2000, F+44 (0) 191 226 2104
wsp.com

CLIENT:

TEES VALLEY

SITE/PROJECT:

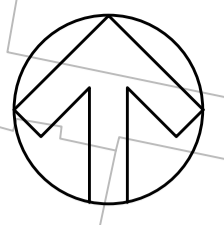
LINTHORPE ROAD RESTORATION

TITLE:

GENERAL ARRANGEMENT
(SHEET 2 OF 4)

SCALE @ A1:	1:250	CHECKED:	JS	APPROVED:	PWW
PROJECT NO:	2024UK293315	DESIGNED:	NR	DRAWN:	NR
DRAWING NO:	288933-WSP-XX-XX-DR-C-000102	DATE:	JULY 2024	REV:	P04

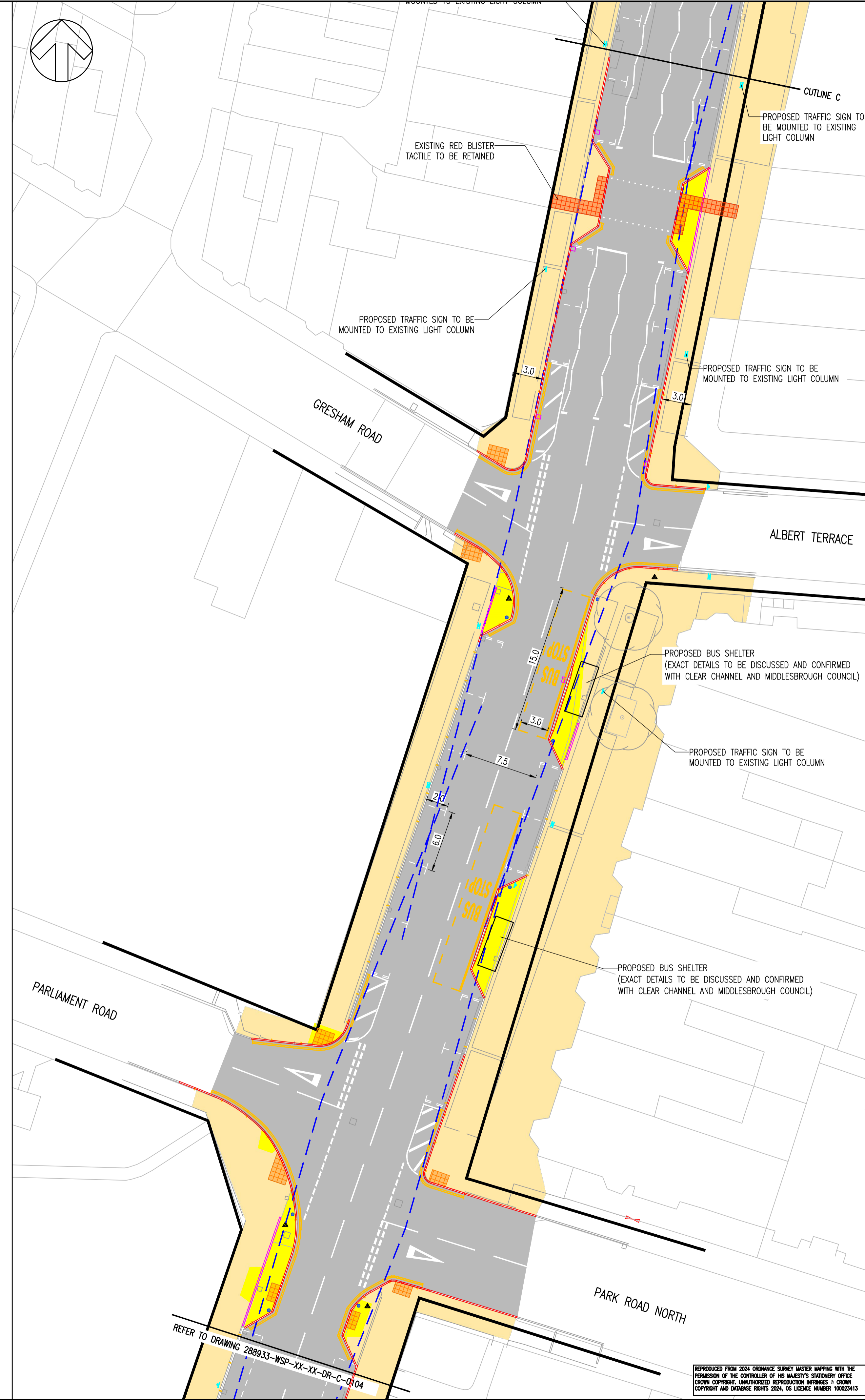
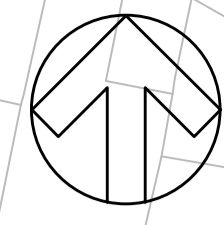
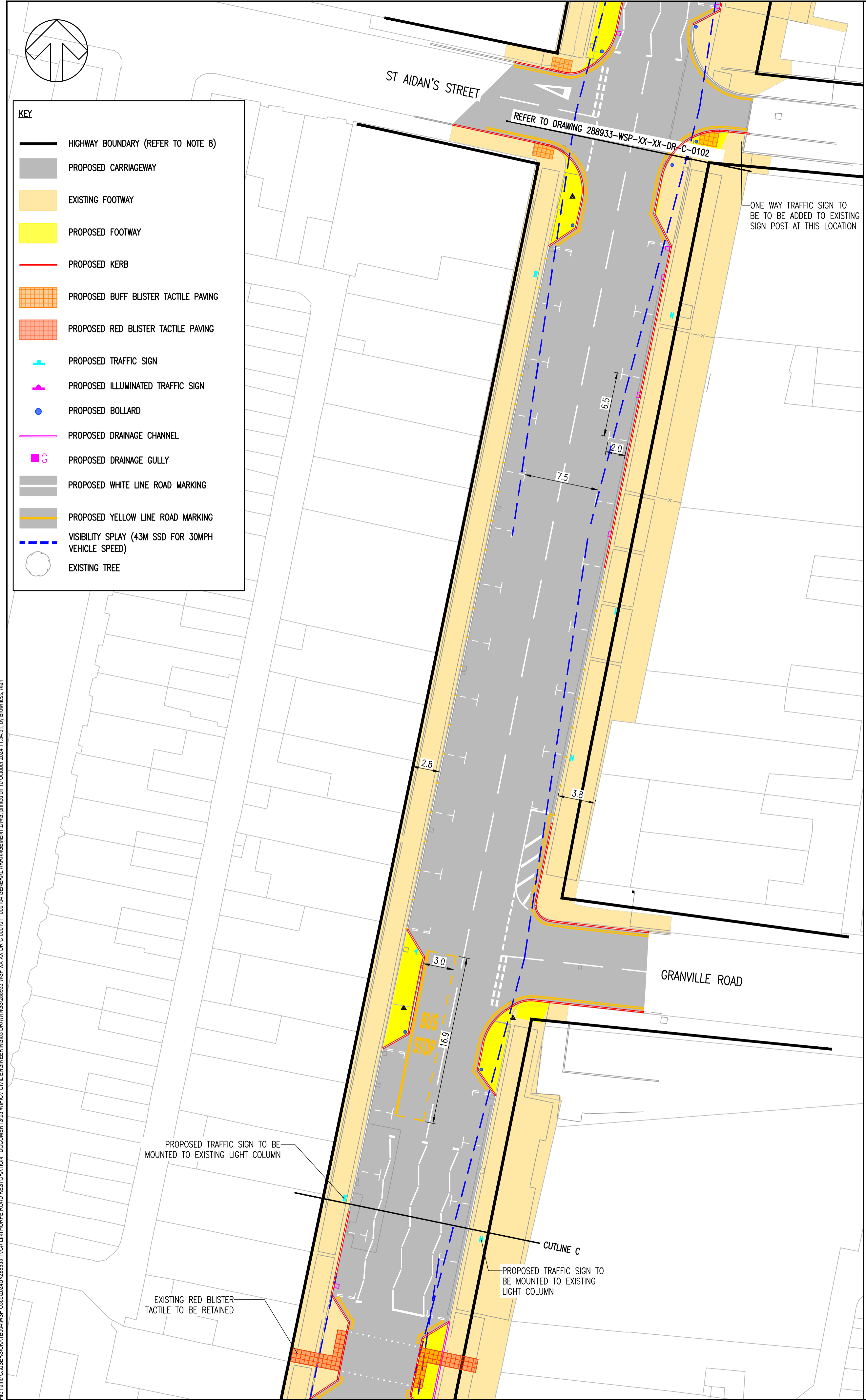
REPRODUCED FROM 2024 ORDNANCE SURVEY MASTER MAPPING WITH THE PERMISSION OF THE CONTROLLER OF HIS MAJESTY'S STATIONERY OFFICE. CROWN COPYRIGHT. UNAUTHORIZED REPRODUCTION INFRINGES © CROWN COPYRIGHT AND DATABASE RIGHTS 2024. OS LICENSE NUMBER 10003413



KEY

- HIGHWAY BOUNDARY (REFER TO NOTE 8)
- PROPOSED CARRIAGEWAY
- EXISTING FOOTWAY
- PROPOSED FOOTWAY
- PROPOSED KERB
- PROPOSED BUFF BLISTER TACTILE PAVING
- PROPOSED RED BLISTER TACTILE PAVING
- PROPOSED TRAFFIC SIGN
- PROPOSED ILLUMINATED TRAFFIC SIGN
- PROPOSED BOLLARD
- PROPOSED DRAINAGE CHANNEL
- PROPOSED DRAINAGE GULLY
- PROPOSED WHITE LINE ROAD MARKING
- PROPOSED YELLOW LINE ROAD MARKING
- VISIBILITY SPLAY (43M SSD FOR 30MPH VEHICLE SPEED)
- EXISTING TREE

File name: C:\USERS\WATBOW\NSP_03602024\288933\TVC\LINTHORPE ROAD RESTORATION - DOCUMENTS\01 MP\01 CIVIL ENGINEERING\03 DRAWINGS\288933-WSP-XX-XX-DR-C-00104 - GENERAL ARRANGEMENT.DWG, printed on 10 October 2024 11:34:31, by: Rowan.Hess, Alan



DO NOT SCALE

- NOTES**
- THIS DRAWING SHOULD BE READ IN CONJUNCTION WITH ALL RELATED DOCUMENTATION, DRAWINGS AND STANDARD DETAILS.
 - THE ORIGINAL OF THIS DRAWING WAS PRODUCED IN COLOUR - A MONOCHROME COPY CANNOT BE RELIED ON.
 - ALL DIMENSIONS ARE IN METRES UNLESS NOTED OTHERWISE.
 - INFORMATION REGARDING THE LOCATION AND DEPTH OF EXISTING SERVICES CANNOT BE GUARANTEED BY THE STATUTORY UNDERTAKER AND SHOULD BE INVESTIGATED. THE CONTRACTOR SHALL ESTABLISH THE PRECISE LINE AND DEPTH OF ALL EXISTING UTILITIES PRIOR TO COMMENCING ANY EXCAVATION WORK.
 - THE DESIGN IS BASED ON TOPOGRAPHICAL SURVEY REFERENCE ASS 2963 BY ADH SITE SURVEYS LIMITED RECEIVED FEBRUARY 2021
 - REFERENCE SHOULD BE MADE TO INDIVIDUAL DISCIPLINE DRAWINGS AND THE DESIGN RISK MANAGEMENT SCHEDULE FOR FURTHER HEALTH AND SAFETY INFORMATION - NOT ALL RISKS ARE IDENTIFIED IN THIS DRAWING.
 - CONFLICTING INFORMATION SHOWN ON THE ENGINEER'S DRAWINGS OR DISCREPANCIES BETWEEN THE INFORMATION GIVEN BY THE ENGINEER AND THAT PROVIDED BY OTHERS MUST BE REFERRED TO THE ENGINEER BEFORE THE WORKS COMMENCE.
 - THIS DRAWING IS BASED IN A WHOLE OR IN PART ON INFORMATION AND/OR DATA APPROVED BY A THIRD PARTY INCLUDING MIDDLESBROUGH COUNCIL AND AS SUCH IN CONTINGENT UPON THE ACCURACY AND COMPLETENESS OF THAT INFORMATION AND/OR DATA. WSP UK LIMITED SHALL NOT BE LIABLE IN RELATION TO ANY INACCURACY PRESENTED WITHIN THE DRAWING AND/OR IN RELATION TO ANY DISCREPANCY OR AMBIGUITY WITH ANY OTHER PROJECT DOCUMENTS SHOULD ANY DATA AND OR INFORMATION UPON WHICH RELIANCE HAS BEEN BASED BE INCORRECT OR OUT OF DATE OR HAVE BEEN CONCEALED, WITHHELD OR BE INCOMPLETE, MISREPRESENTED OR OTHERWISE NOT FULLY DISCLOSED TO WSP UK LIMITED.

SAFETY HEALTH & ENVIRONMENTAL INFORMATION BOX

HEALTH AND SAFETY SYMBOLS LEGEND

	INDICATES A RESIDUAL RISK REQUIRING A COMPULSORY ACTION		INDICATES A RESIDUAL RISK FOR INFORMATION
	INDICATES A RESIDUAL RISK REQUIRING A PROHIBITIVE ACTION		INDICATES A RESIDUAL RISK AS A WARNING
	INDICATES AN ENVIRONMENTAL HAZARD		

This table is provided to assist the Principal Contractor to fulfill their obligations under the Construction Design & Management Regulations, 2015. It does not include residual risks that a competent Contractor will be aware of nor does it absolve the Principal Contractor of their legal responsibilities.

The reference number corresponds to the Design Risk Management Schedule.

UNTIL TECHNICAL APPROVAL HAS BEEN OBTAINED FROM THE RELEVANT LOCAL AUTHORITIES OR STATUTORY BODIES, IT SHOULD BE UNDERSTOOD THAT ALL DRAWINGS ARE ISSUED AS PRELIMINARY AND NOT FOR CONSTRUCTION. SHOULD THE CONTRACTOR AND / OR EMPLOYER COMMENCE WORK PRIOR TO APPROVAL BEING GIVEN, IT IS ENTIRELY AT THEIR OWN RISK

REV	DATE	BY	DESCRIPTION	CHK	APP
P04	10/10/24	AB	UPDATED FOLLOWING CLIENT REQUIREMENTS	JS	PWW
P03	03/09/24	NR	UPDATED FOLLOWING CLIENT REVIEW	JS	PWW
P02	31/07/24	NR	ISSUE FOR CLIENT APPROVAL	JS	PWW
P01	23/07/24	NR	ISSUE FOR COST ESTIMATE	JS	PWW
DR	10/07/24	NR	DRAFT ISSUE	JS	PWW

DRAWING STATUS:

Amber Court, William Armstrong Drive, Newcastle upon Tyne, NE4 7YQ, UK
T+44 (0) 191 226 2000, F+44 (0) 191 226 2104
wsp.com

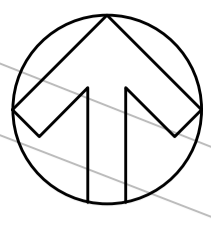
CLIENT:

SITE/PROJECT:
LINTHORPE ROAD RESTORATION

TITLE:
GENERAL ARRANGEMENT (SHEET 3 OF 4)

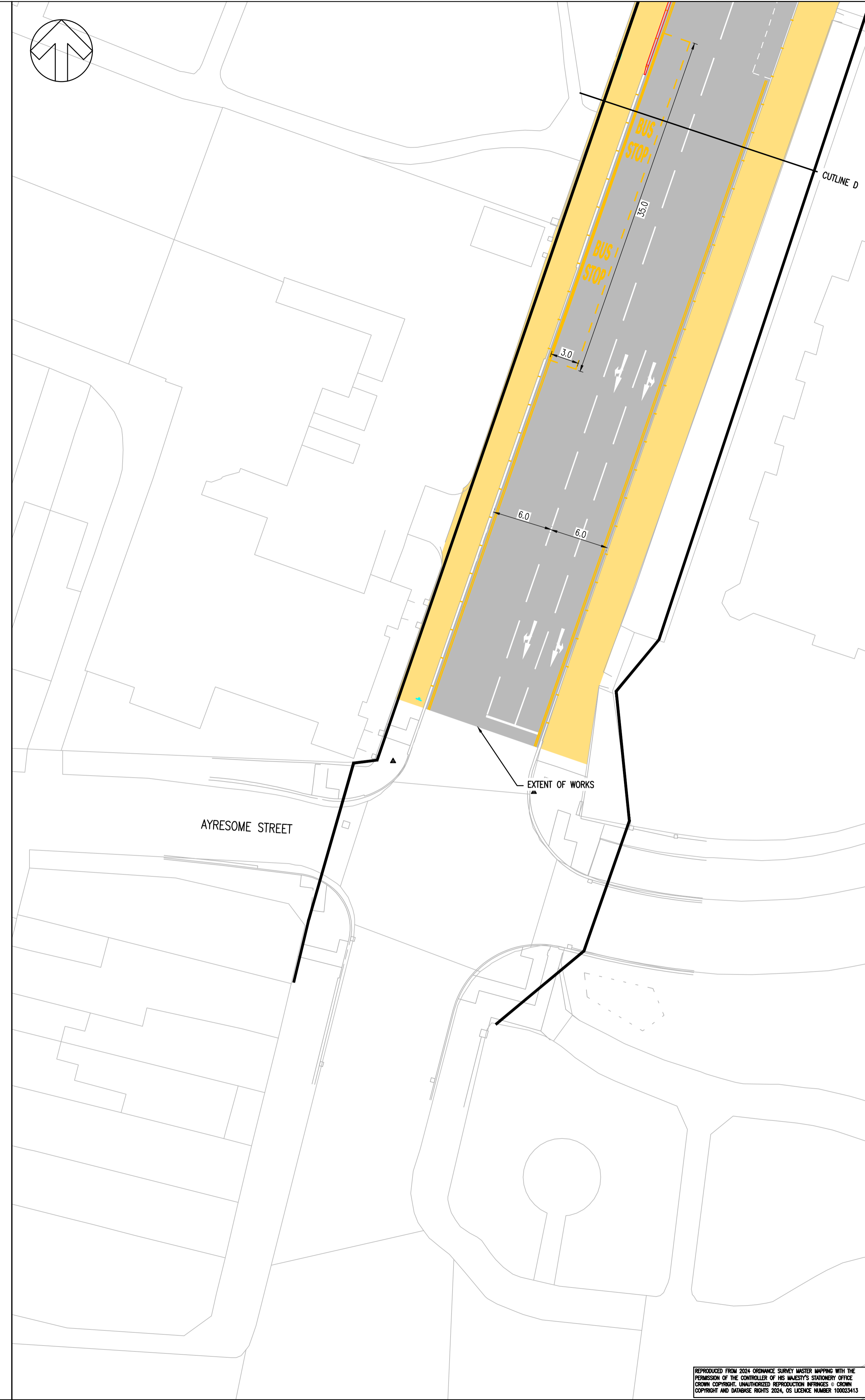
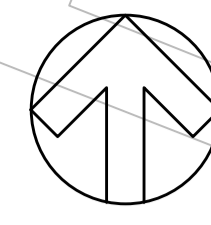
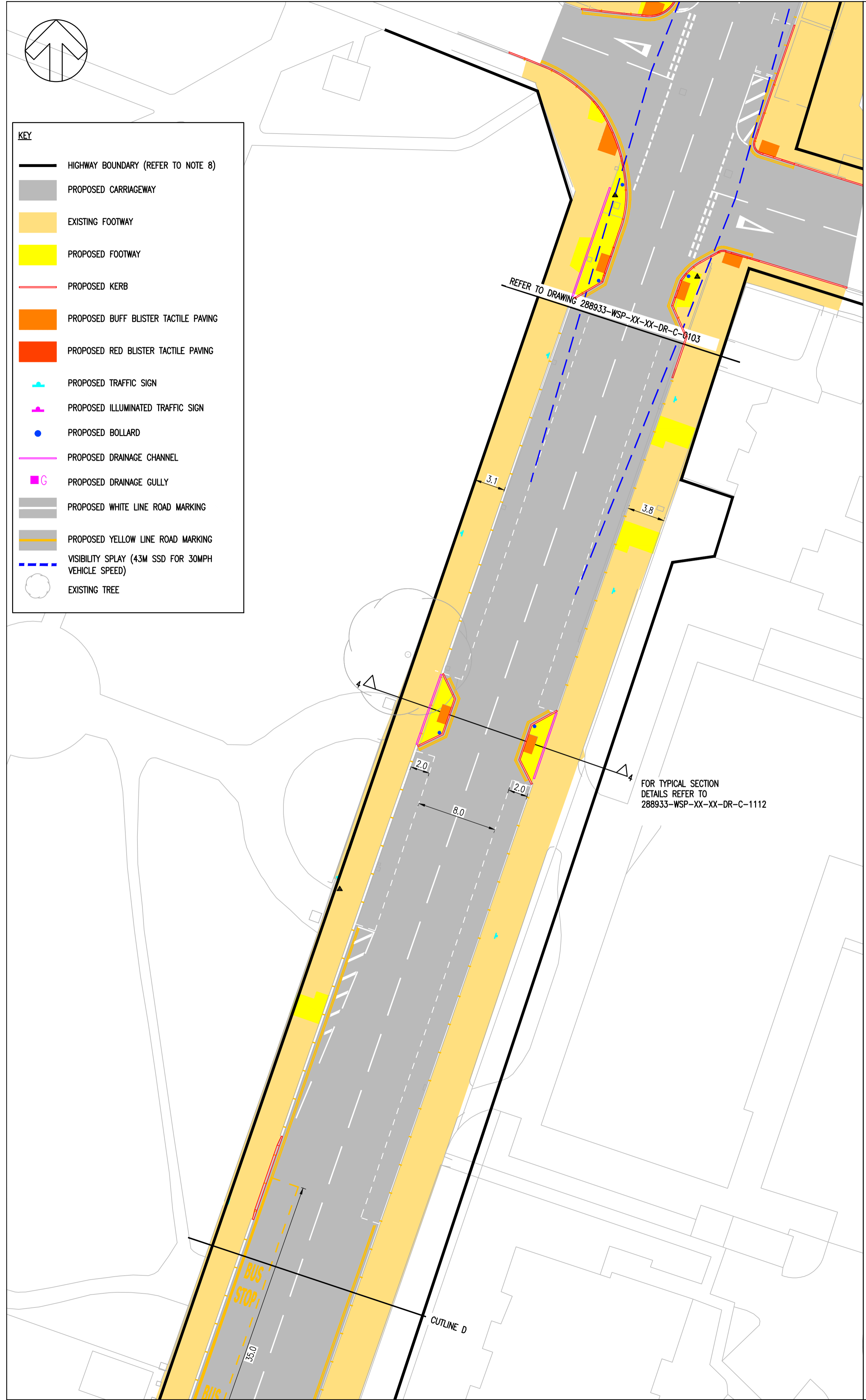
SCALE @ A1:	1:250	CHECKED:	JS	APPROVED:	PWW
PROJECT NO:	2024UK293315	DESIGNED:	NR	DATE:	JULY 2024
DRAWING NO:	288933-WSP-XX-XX-DR-C-000103	REV:			P04

REPRODUCED FROM 2024 ORDNANCE SURVEY MASTER MAPPING WITH THE PERMISSION OF THE CONTROLLER OF HIS MAJESTY'S STATIONERY OFFICE (CROWN COPYRIGHT. UNAUTHORISED REPRODUCTION INFRINGES © CROWN COPYRIGHT AND DATABASE RIGHTS 2024, OS LICENCE NUMBER 100023413)



KEY

- HIGHWAY BOUNDARY (REFER TO NOTE 8)
- PROPOSED CARRIAGEWAY
- EXISTING FOOTWAY
- PROPOSED FOOTWAY
- PROPOSED KERB
- PROPOSED BUFF BLISTER TACTILE PAVING
- PROPOSED RED BLISTER TACTILE PAVING
- PROPOSED TRAFFIC SIGN
- PROPOSED ILLUMINATED TRAFFIC SIGN
- PROPOSED BOLLARD
- PROPOSED DRAINAGE CHANNEL
- PROPOSED DRAINAGE GULLY
- PROPOSED WHITE LINE ROAD MARKING
- PROPOSED YELLOW LINE ROAD MARKING
- VISIBILITY SPLAY (43M SSD FOR 30MPH VEHICLE SPEED)
- EXISTING TREE



DO NOT SCALE

- NOTES**
1. THIS DRAWING SHOULD BE READ IN CONJUNCTION WITH ALL RELATED DOCUMENTATION, DRAWINGS AND STANDARD DETAILS.
 2. THE ORIGINAL OF THIS DRAWING WAS PRODUCED IN COLOUR – A MONOCHROME COPY CANNOT BE RELIED ON.
 3. ALL DIMENSIONS ARE IN METRES UNLESS NOTED OTHERWISE.
 4. INFORMATION REGARDING THE LOCATION AND DEPTH OF EXISTING SERVICES CANNOT BE GUARANTEED BY THE STATUTORY UNDERTAKER AND SHOULD BE INVESTIGATED. THE CONTRACTOR SHALL ESTABLISH THE PRECISE LINE AND DEPTH OF ALL EXISTING UTILITIES PRIOR TO COMMENCING ANY EXCAVATION WORK.
 5. THE DESIGN IS BASED ON TOPOGRAPHICAL SURVEY REFERENCE ASS 2963 BY ADH SITE SURVEYS LIMITED RECEIVED FEBRUARY 2021.
 6. REFERENCE SHOULD BE MADE TO INDIVIDUAL DISCIPLINE DRAWINGS AND THE DESIGN RISK MANAGEMENT SCHEDULE FOR FURTHER HEALTH AND SAFETY INFORMATION – NOT ALL RISKS ARE IDENTIFIED IN THIS DRAWING.
 7. CONFLICTING INFORMATION SHOWN ON THE ENGINEER'S DRAWINGS OR DISCREPANCIES BETWEEN THE INFORMATION GIVEN BY THE ENGINEER AND THAT PROVIDED BY OTHERS MUST BE REFERRED TO THE ENGINEER BEFORE THE WORKS COMMENCE.
 8. THIS DRAWING IS BASED IN A WHOLE OR IN PART ON INFORMATION AND/OR DATA APPROVED BY A THIRD PARTY INCLUDING MIDDLESBROUGH COUNCIL AND AS SUCH IN CONTINGENT UPON THE ACCURACY AND COMPLETENESS OF THAT INFORMATION AND/OR DATA. WSP UK LIMITED SHALL NOT BE LIABLE IN RELATION TO ANY INACCURACY PRESENTED WITHIN THE DRAWING AND/OR IN RELATION TO ANY DISCREPANCY OR AMBIGUITY WITH ANY OTHER PROJECT DOCUMENTS SHOULD ANY DATA AND OR INFORMATION UPON WHICH RELIANCE HAS BEEN BASED BE INCORRECT OR OUT OF DATE OR HAVE BEEN CONCEALED, WITHHELD OR BE INCOMPLETE, MISREPRESENTED OR OTHERWISE NOT FULLY DISCLOSED TO WSP UK LIMITED.

SAFETY HEALTH & ENVIRONMENTAL INFORMATION BOX

HEALTH AND SAFETY SYMBOLS LEGEND

	INDICATES A RESIDUAL RISK REQUIRING A COMPULSORY ACTION		INDICATES A RESIDUAL RISK FOR INFORMATION
	INDICATES A RESIDUAL RISK REQUIRING A PROHIBITIVE ACTION		INDICATES A RESIDUAL RISK AS A WARNING
	INDICATES AN ENVIRONMENTAL HAZARD		

This table is provided to assist the Principal Contractor to fulfil their obligations under the Construction Design & Management Regulations, 2015. It does not include residual risks that a competent Contractor will be aware of nor does it absolve the Principal Contractor of their legal responsibilities.
The reference number corresponds to the Design Risk Management Schedule.

UNTIL TECHNICAL APPROVAL HAS BEEN OBTAINED FROM THE RELEVANT LOCAL AUTHORITIES OR STATUTORY BODIES, IT SHOULD BE UNDERSTOOD THAT ALL DRAWINGS ARE ISSUED AS PRELIMINARY AND NOT FOR CONSTRUCTION. SHOULD THE CONTRACTOR AND / OR EMPLOYER COMMENCE WORK PRIOR TO APPROVAL BEING GIVEN, IT IS ENTIRELY AT THEIR OWN RISK

PO4	10/10/24	AB	UPDATED FOLLOWING CLIENT REQUIREMENTS	JS	PWW
PO3	03/09/24	NR	UPDATED FOLLOWING CLIENT REVIEW	JS	PWW
PO2	31/07/24	NR	ISSUED FOR CLIENT APPROVAL	JS	PWW
PO1	23/07/24	NR	ISSUE FOR COST ESTIMATE	JS	PWW
DR	10/07/24	NR	DRAFT ISSUE	JS	PWW
REV	DATE	BY	DESCRIPTION	CHK	APP

DRAWING STATUS:

wsp

Amber Court, William Armstrong Drive, Newcastle upon Tyne, NE4 7YQ, UK
T+ 44 (0) 191 226 2000, F+ 44 (0) 191 226 2104
wsp.com

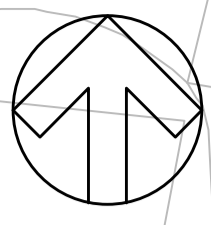
TEES VALLEY

SITE/PROJECT:
LINTHORPE ROAD RESTORATION

TITLE:
GENERAL ARRANGEMENT (SHEET 4 OF 4)

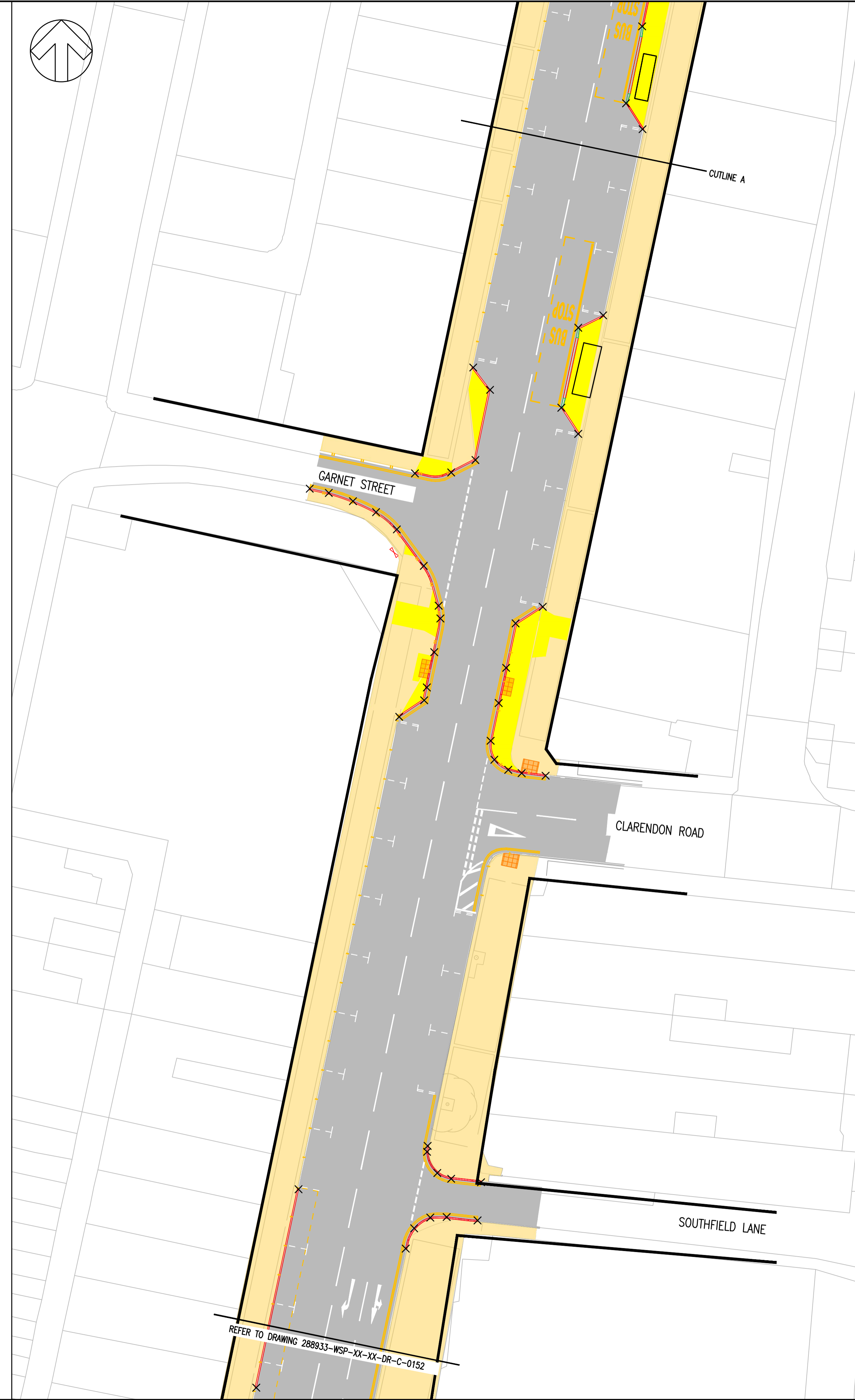
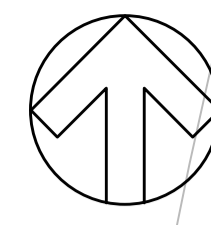
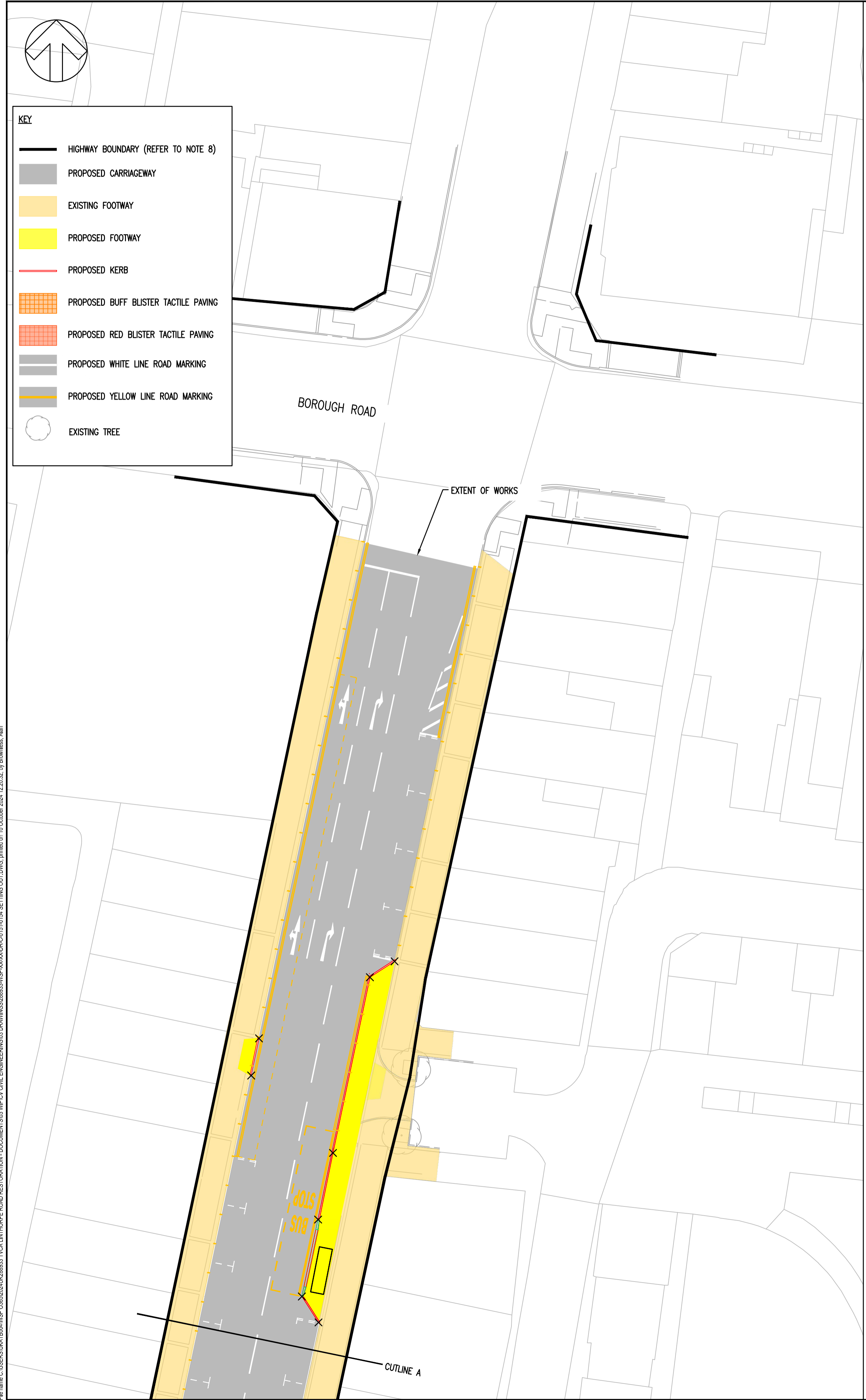
SCALE @ A1:	1:250	CHECKED:	JS	APPROVED:	PWW
PROJECT NO:	2024UK293315	DESIGNED:	NR	DRAWN:	NR
DRAWING NO:	288933-WSP-XX-XX-DR-C-000104	DATE:	JULY 2024	REV:	P04

REPRODUCED FROM 2024 ORDNANCE SURVEY MASTER MAPPING WITH THE PERMISSION OF THE CONTROLLER OF HIS MAJESTY'S STATIONERY OFFICE. CROWN COPYRIGHT. UNAUTHORIZED REPRODUCTION INFRINGES © CROWN COPYRIGHT AND DESIGN RIGHTS 2024. OS LICENSE NUMBER 10003413



KEY

- HIGHWAY BOUNDARY (REFER TO NOTE 8)
- PROPOSED CARRIAGEWAY
- EXISTING FOOTWAY
- PROPOSED FOOTWAY
- PROPOSED KERB
- PROPOSED BUFF BLISTER TACTILE PAVING
- PROPOSED RED BLISTER TACTILE PAVING
- PROPOSED WHITE LINE ROAD MARKING
- PROPOSED YELLOW LINE ROAD MARKING
- EXISTING TREE



DO NOT SCALE

- NOTES**
1. THIS DRAWING SHOULD BE READ IN CONJUNCTION WITH ALL RELATED DOCUMENTATION, DRAWINGS AND STANDARD DETAILS.
 2. THE ORIGINAL OF THIS DRAWING WAS PRODUCED IN COLOUR – A MONOCHROME COPY CANNOT BE RELIED ON.
 3. ALL DIMENSIONS ARE IN METRES UNLESS NOTED OTHERWISE.
 4. INFORMATION REGARDING THE LOCATION AND DEPTH OF EXISTING SERVICES CANNOT BE GUARANTEED BY THE STATUTORY UNDERTAKER AND SHOULD BE INVESTIGATED. THE CONTRACTOR SHALL ESTABLISH THE PRECISE LINE AND DEPTH OF ALL EXISTING UTILITIES PRIOR TO COMMENCING ANY EXCAVATION WORK.
 5. THE DESIGN IS BASED ON TOPOGRAPHICAL SURVEY REFERENCE ASS 2963 BY ADH SITE SURVEYS LIMITED RECEIVED FEBRUARY 2021
 6. REFERENCE SHOULD BE MADE TO INDIVIDUAL DISCIPLINE DRAWINGS AND THE DESIGN RISK MANAGEMENT SCHEDULE FOR FURTHER HEALTH AND SAFETY INFORMATION – NOT ALL RISKS ARE IDENTIFIED IN THIS DRAWING.
 7. CONFLICTING INFORMATION SHOWN ON THE ENGINEER'S DRAWINGS OR DISCREPANCIES BETWEEN THE INFORMATION GIVEN BY THE ENGINEER AND THAT PROVIDED BY OTHERS MUST BE REFERRED TO THE ENGINEER BEFORE THE WORKS COMMENCE.
 8. THIS DRAWING IS BASED IN A WHOLE OR IN PART ON INFORMATION AND/OR DATA APPROVED BY A THIRD PARTY INCLUDING MIDDLESBROUGH COUNCIL AND AS SUCH IN CONTINGENT UPON THE ACCURACY AND COMPLETENESS OF THAT INFORMATION AND/OR DATA. WSP UK LIMITED SHALL NOT BE LIABLE IN RELATION TO ANY INACCURACY PRESENTED WITHIN THE DRAWING AND/OR IN RELATION TO ANY DISCREPANCY OR AMBIGUITY WITH ANY OTHER PROJECT DOCUMENTS SHOULD ANY DATA AND/OR INFORMATION UPON WHICH RELIANCE HAS BEEN BASED BE INCORRECT OR OUT OF DATE OR HAVE BEEN CONCEALED, WITHHELD OR BE INCOMPLETE, MISREPRESENTED OR OTHERWISE NOT FULLY DISCLOSED TO WSP UK LIMITED.

HEALTH AND SAFETY SYMBOLS LEGEND

	INDICATES A RESIDUAL RISK REQUIRING A COMPULSORY ACTION		INDICATES A RESIDUAL RISK FOR INFORMATION
	INDICATES A RESIDUAL RISK REQUIRING A PROHIBITIVE ACTION		INDICATES A RESIDUAL RISK AS A WARNING
	INDICATES AN ENVIRONMENTAL HAZARD		

This table is provided to assist the Principal Contractor to fulfil their obligations under the Construction Design & Management Regulations, 2015. It does not include residual risks that a competent Contractor will be aware of nor does it absolve the Principal Contractor of their legal responsibilities.

The reference number corresponds to the Design Risk Management Schedule.

UNTIL TECHNICAL APPROVAL HAS BEEN OBTAINED FROM THE RELEVANT LOCAL AUTHORITIES OR STATUTORY BODIES, IT SHOULD BE UNDERSTOOD THAT ALL DRAWINGS ARE ISSUED AS PRELIMINARY AND NOT FOR CONSTRUCTION. SHOULD THE CONTRACTOR AND / OR EMPLOYER COMMENCE WORK PRIOR TO APPROVAL BEING GIVEN, IT IS ENTIRELY AT THEIR OWN RISK

PO2	10/10/24	AB	UPDATED FOLLOWING CLIENT REQUIREMENTS	JS	PWW
P01	12/09/24	NR	ISSUED FOR CLIENT APPROVAL	JS	PWW
REV	DATE	BY	DESCRIPTION	CHK	APP

DRAWING STATUS:

wsp

Amber Court, William Armstrong Drive, Newcastle upon Tyne, NE4 7YQ, UK
T+ 44 (0) 191 226 2000, F+ 44 (0) 191 226 2104
wsp.com

CLIENT:

TEES VALLEY

SITE/PROJECT:

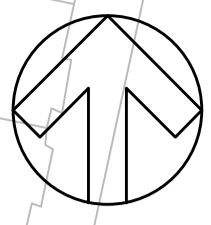
LINTHORPE ROAD RESTORATION

TITLE:

SETTING OUT
(SHEET 1 OF 4)

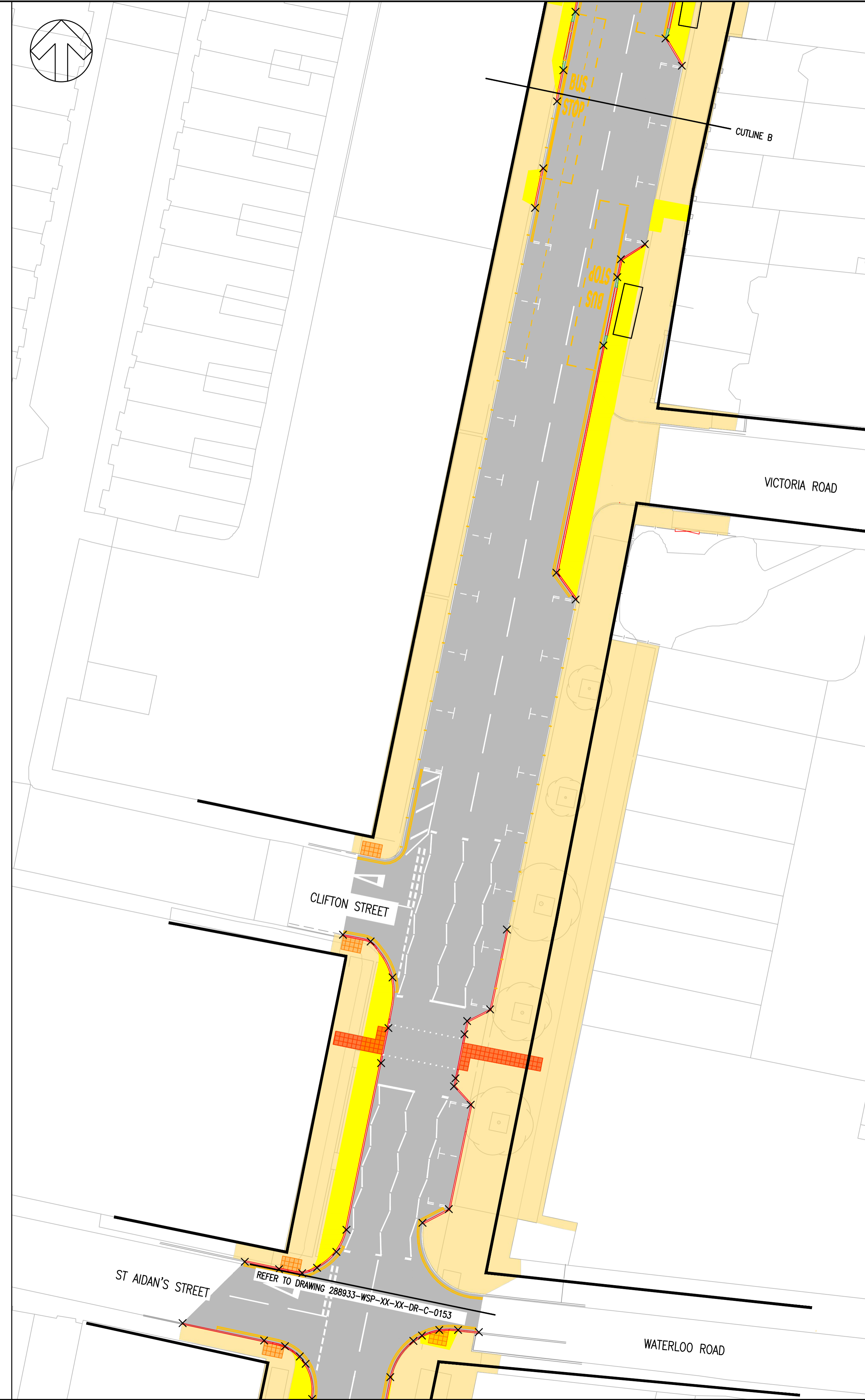
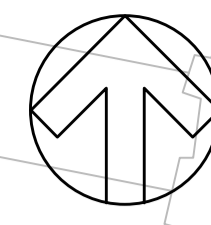
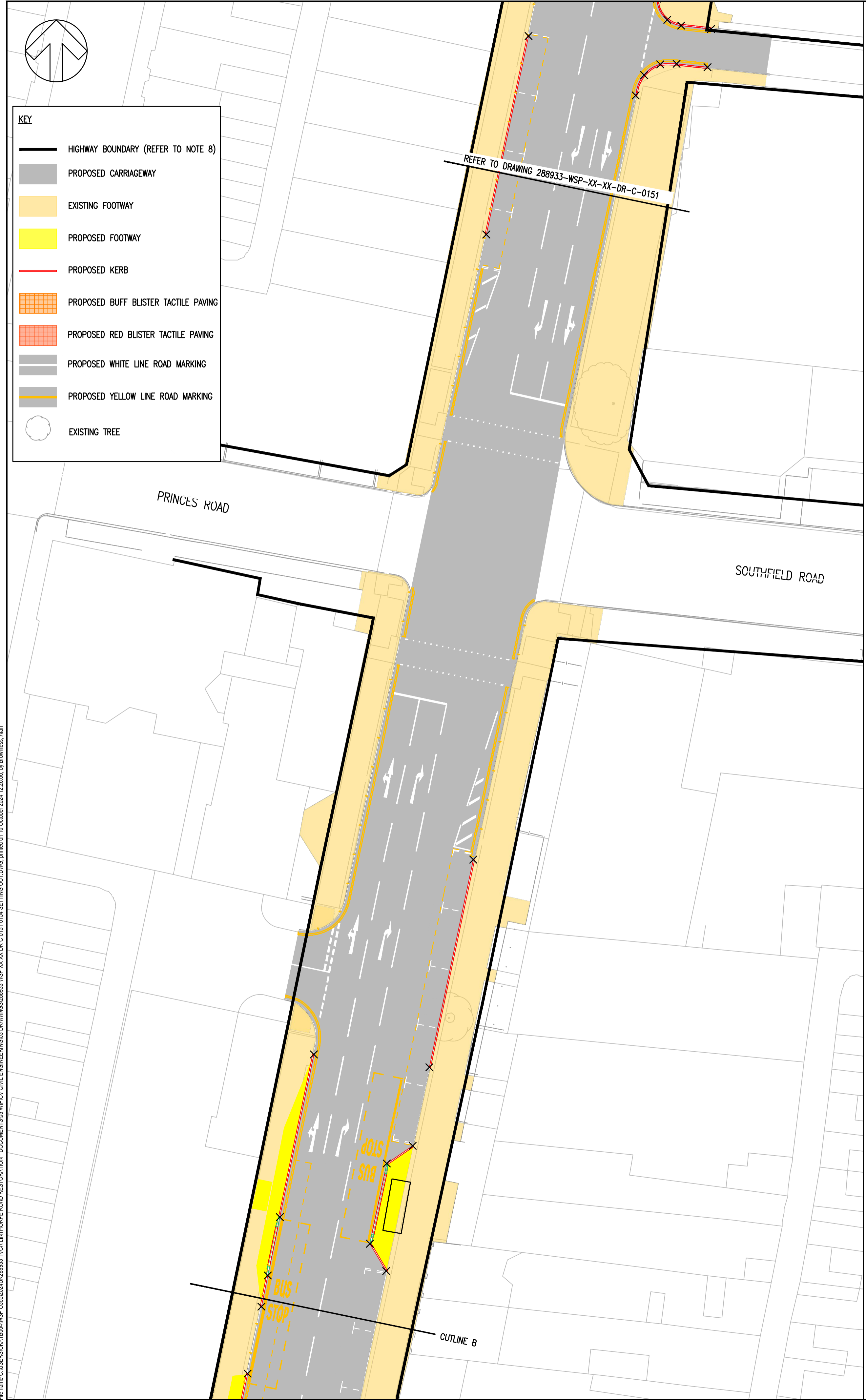
SCALE @ A1:	1:250	CHECKED:	JS	APPROVED:	PWW
PROJECT NO:	2024UK293315	DESIGNED:	NR	DRAWN:	NR
DATE:	JULY 2024	DATE:	JULY 2024	DATE:	JULY 2024
DRAWING NO:	288933-WSP-XX-XX-DR-C-0151				REV:
					P02

File name: C:\USERS\WATB\Documents\288933-WSP-XX-XX-DR-C-0151\SETTING OUT.DWG, printed on 10 October 2024 12:20:32, by: Ben.Hess, Alan



KEY

- HIGHWAY BOUNDARY (REFER TO NOTE 8)
- PROPOSED CARRIAGEWAY
- EXISTING FOOTWAY
- PROPOSED FOOTWAY
- PROPOSED KERB
- PROPOSED BUFF BLISTER TACTILE PAVING
- PROPOSED RED BLISTER TACTILE PAVING
- PROPOSED WHITE LINE ROAD MARKING
- PROPOSED YELLOW LINE ROAD MARKING
- EXISTING TREE



DO NOT SCALE

- NOTES**
1. THIS DRAWING SHOULD BE READ IN CONJUNCTION WITH ALL RELATED DOCUMENTATION, DRAWINGS AND STANDARD DETAILS.
 2. THE ORIGINAL OF THIS DRAWING WAS PRODUCED IN COLOUR - A MONOCHROME COPY CANNOT BE RELIED ON.
 3. ALL DIMENSIONS ARE IN METRES UNLESS NOTED OTHERWISE.
 4. INFORMATION REGARDING THE LOCATION AND DEPTH OF EXISTING SERVICES CANNOT BE GUARANTEED BY THE STATUTORY UNDERTAKER AND SHOULD BE INVESTIGATED. THE CONTRACTOR SHALL ESTABLISH THE PRECISE LINE AND DEPTH OF ALL EXISTING UTILITIES PRIOR TO COMMENCING ANY EXCAVATION WORK.
 5. THE DESIGN IS BASED ON TOPOGRAPHICAL SURVEY REFERENCE ASS 2963 BY ADH SITE SURVEYS LIMITED RECEIVED FEBRUARY 2021
 6. REFERENCE SHOULD BE MADE TO INDIVIDUAL DISCIPLINE DRAWINGS AND THE DESIGN RISK MANAGEMENT SCHEDULE FOR FURTHER HEALTH AND SAFETY INFORMATION - NOT ALL RISKS ARE IDENTIFIED IN THIS DRAWING.
 7. CONFLICTING INFORMATION SHOWN ON THE ENGINEER'S DRAWINGS OR DISCREPANCIES BETWEEN THE INFORMATION GIVEN BY THE ENGINEER AND THAT PROVIDED BY OTHERS MUST BE REFERRED TO THE ENGINEER BEFORE THE WORKS COMMENCE.
 8. THIS DRAWING IS BASED IN A WHOLE OR IN PART ON INFORMATION AND/OR DATA APPROVED BY A THIRD PARTY INCLUDING MIDLESBROUGH COUNCIL AND AS SUCH IN CONTINGENT UPON THE ACCURACY AND COMPLETENESS OF THAT INFORMATION AND/OR DATA. WSP UK LIMITED SHALL NOT BE LIABLE IN RELATION TO ANY INACCURACY PRESENTED WITHIN THE DRAWING AND/OR IN RELATION TO ANY DISCREPANCY OR AMBIGUITY WITH ANY OTHER PROJECT DOCUMENTS SHOULD ANY DATA AND OR INFORMATION UPON WHICH RELIANCE HAS BEEN BASED BE INCORRECT OR OUT OF DATE OR HAVE BEEN CONCEALED, WITHHELD OR BE INCOMPLETE, MISREPRESENTED OR OTHERWISE NOT FULLY DISCLOSED TO WSP UK LIMITED.

SAFETY HEALTH & ENVIRONMENTAL INFORMATION BOX

HEALTH AND SAFETY SYMBOLS LEGEND

	INDICATES A RESIDUAL RISK REQUIRING A COMPULSORY ACTION		INDICATES A RESIDUAL RISK FOR INFORMATION
	INDICATES A RESIDUAL RISK REQUIRING A PROHIBITIVE ACTION		INDICATES A RESIDUAL RISK AS A WARNING
	INDICATES AN ENVIRONMENTAL HAZARD		

This table is provided to assist the Principal Contractor to fulfil their obligations under the Construction Design & Management Regulations, 2015. It does not include residual risks that a competent Contractor will be aware of nor does it absolve the Principal Contractor of their legal responsibilities.

The reference number corresponds to the Design Risk Management Schedule.

UNTIL TECHNICAL APPROVAL HAS BEEN OBTAINED FROM THE RELEVANT LOCAL AUTHORITIES OR STATUTORY BODIES, IT SHOULD BE UNDERSTOOD THAT ALL DRAWINGS ARE ISSUED AS PRELIMINARY AND NOT FOR CONSTRUCTION. SHOULD THE CONTRACTOR AND / OR EMPLOYER COMMENCE WORK PRIOR TO APPROVAL BEING GIVEN, IT IS ENTIRELY AT THEIR OWN RISK

PO2	10/10/24	AB	UPDATED FOLLOWING CLIENT REQUIREMENTS	JS	PWW
P01	12/09/24	NR	ISSUED FOR CLIENT APPROVAL	JS	PWW
REV	DATE	BY	DESCRIPTION	CHK	APP

DRAWING STATUS:

wsp

Amber Court, William Armstrong Drive, Newcastle upon Tyne, NE4 7YQ, UK
T+44 (0) 191 226 2000, F+44 (0) 191 226 2104
wsp.com

CLIENT:

TEES VALLEY

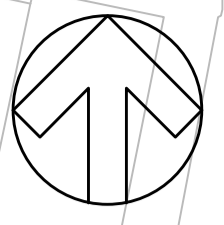
SITE/PROJECT:

LINTHORPE ROAD RESTORATION

TITLE:

SETTING OUT
(SHEET 2 OF 4)

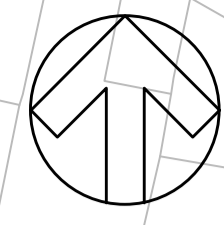
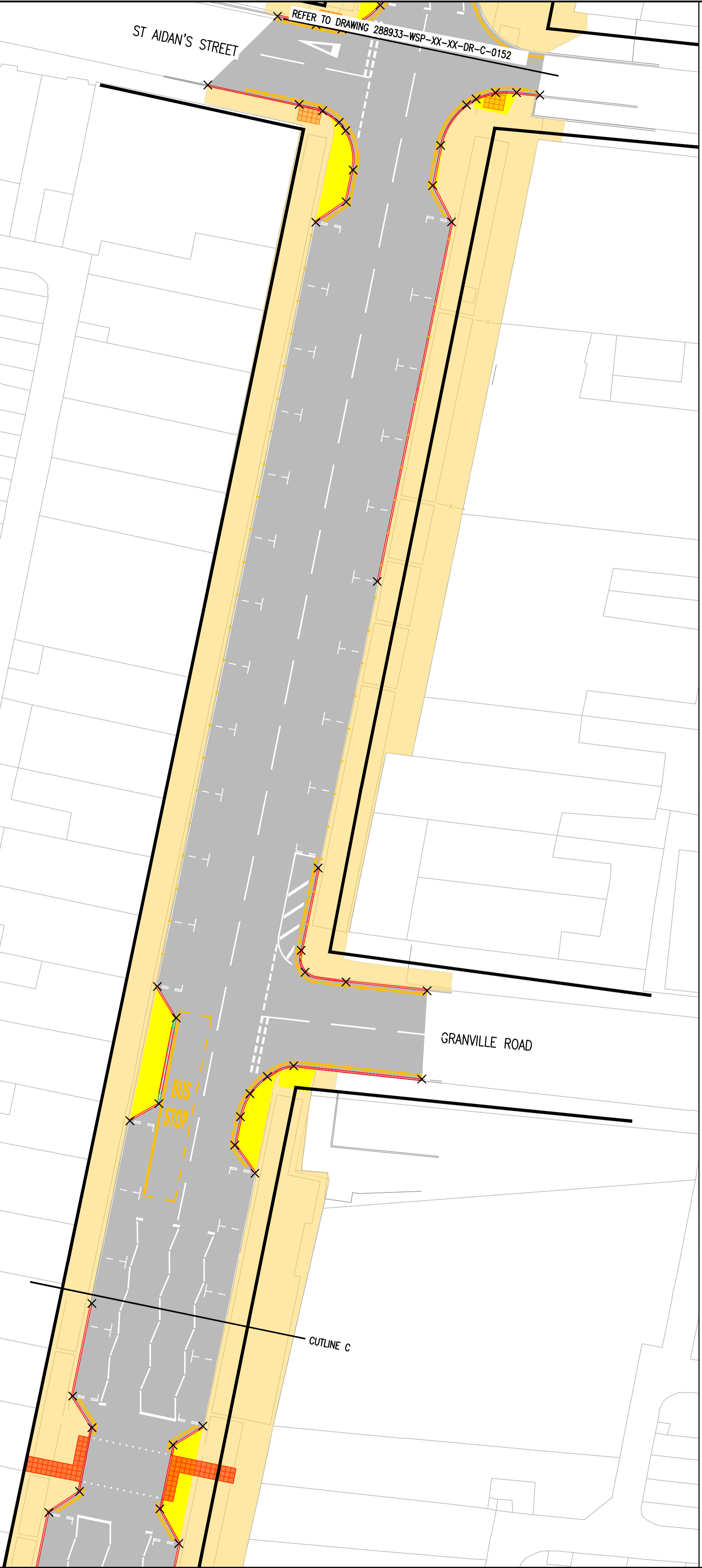
SCALE @ A1:	1:250	CHECKED:	JS	APPROVED:	PWW
PROJECT NO:	2024UK293315	DESIGNED:	NR	DRAWN:	NR
DRAWING NO:	288933-WSP-XX-XX-DR-C-0152	DATE:	JULY 2024	REV:	P02



ST AIDAN'S STREET
REFER TO DRAWING 288933-WSP-XX-XX-DR-C-0152

KEY

- HIGHWAY BOUNDARY (REFER TO NOTE 8)
- PROPOSED CARRIAGEWAY
- EXISTING FOOTWAY
- PROPOSED FOOTWAY
- PROPOSED KERB
- PROPOSED BUFF BLISTER TACTILE PAVING
- PROPOSED RED BLISTER TACTILE PAVING
- PROPOSED WHITE LINE ROAD MARKING
- PROPOSED YELLOW LINE ROAD MARKING
- EXISTING TREE



GRESHAM ROAD

ALBERT TERRACE



DO NOT SCALE

- NOTES**
1. THIS DRAWING SHOULD BE READ IN CONJUNCTION WITH ALL RELATED DOCUMENTATION, DRAWINGS AND STANDARD DETAILS.
 2. THE ORIGINAL OF THIS DRAWING WAS PRODUCED IN COLOUR - A MONOCHROME COPY CANNOT BE RELIED ON.
 3. ALL DIMENSIONS ARE IN METRES UNLESS NOTED OTHERWISE.
 4. INFORMATION REGARDING THE LOCATION AND DEPTH OF EXISTING SERVICES CANNOT BE GUARANTEED BY THE STATUTORY UNDERTAKER AND SHOULD BE INVESTIGATED. THE CONTRACTOR SHALL ESTABLISH THE PRECISE LINE AND DEPTH OF ALL EXISTING UTILITIES PRIOR TO COMMENCING ANY EXCAVATION WORK.
 5. THE DESIGN IS BASED ON TOPOGRAPHICAL SURVEY REFERENCE ASS 2963 BY ADH SITE SURVEYS LIMITED RECEIVED FEBRUARY 2021
 6. REFERENCE SHOULD BE MADE TO INDIVIDUAL DISCIPLINE DRAWINGS AND THE DESIGN RISK MANAGEMENT SCHEDULE FOR FURTHER HEALTH AND SAFETY INFORMATION - NOT ALL RISKS ARE IDENTIFIED IN THIS DRAWING.
 7. CONFLICTING INFORMATION SHOWN ON THE ENGINEER'S DRAWINGS OR DISCREPANCIES BETWEEN THE INFORMATION GIVEN BY THE ENGINEER AND THAT PROVIDED BY OTHERS MUST BE REFERRED TO THE ENGINEER BEFORE THE WORKS COMMENCE.
 8. THIS DRAWING IS BASED IN A WHOLE OR IN PART ON INFORMATION AND/OR DATA APPROVED BY A THIRD PARTY INCLUDING MIDDLESBROUGH COUNCIL AND AS SUCH IN CONTINGENT UPON THE ACCURACY AND COMPLETENESS OF THAT INFORMATION AND/OR DATA. WSP UK LIMITED SHALL NOT BE LIABLE IN RELATION TO ANY INACCURACY PRESENTED WITHIN THE DRAWING AND/OR IN RELATION TO ANY DISCREPANCY OR AMBIGUITY WITH ANY OTHER PROJECT DOCUMENTS SHOULD ANY DATA AND/OR INFORMATION UPON WHICH RELIANCE HAS BEEN BASED BE INCORRECT OR OUT OF DATE OR HAVE BEEN CONCEALED, WITHHELD OR BE INCOMPLETE, MISREPRESENTED OR OTHERWISE NOT FULLY DISCLOSED TO WSP UK LIMITED.

SAFETY HEALTH & ENVIRONMENTAL INFORMATION BOX

HEALTH AND SAFETY SYMBOLS LEGEND

	INDICATES A RESIDUAL RISK REQUIRING A COMPULSORY ACTION		INDICATES A RESIDUAL RISK FOR INFORMATION
	INDICATES A RESIDUAL RISK REQUIRING A PROHIBITIVE ACTION		INDICATES A RESIDUAL RISK AS A WARNING
	INDICATES AN ENVIRONMENTAL HAZARD		

This table is provided to assist the Principal Contractor to fulfil their obligations under the Construction Design & Management Regulations, 2015. It does not include residual risks that a competent Contractor will be aware of nor does it absolve the Principal Contractor of their legal responsibilities.

The reference number corresponds to the Design Risk Management Schedule.

UNTIL TECHNICAL APPROVAL HAS BEEN OBTAINED FROM THE RELEVANT LOCAL AUTHORITIES OR STATUTORY BODIES, IT SHOULD BE UNDERSTOOD THAT ALL DRAWINGS ARE ISSUED AS PRELIMINARY AND NOT FOR CONSTRUCTION. SHOULD THE CONTRACTOR AND / OR EMPLOYER COMMENCE WORK PRIOR TO APPROVAL BEING GIVEN, IT IS ENTIRELY AT THEIR OWN RISK

PO2	10/10/24	AB	UPDATED FOLLOWING CLIENT REQUIREMENTS	JS	PWW
P01	12/09/24	NR	ISSUED FOR CLIENT APPROVAL	JS	PWW
REV	DATE	BY	DESCRIPTION	CHK	APP

DRAWING STATUS:

wsp

Amber Court, William Armstrong Drive, Newcastle upon Tyne, NE4 7YQ, UK
T+44 (0) 191 226 2000, F+44 (0) 191 226 2104
wsp.com

CLIENT:

TEES VALLEY

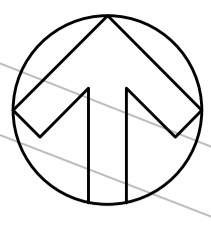
SITE/PROJECT:
LINTHORPE ROAD RESTORATION

TITLE:
SETTING OUT
(SHEET 3 OF 4)

SCALE @ A1:	1:250	CHECKED:	JS	APPROVED:	PWW
PROJECT NO:	2024UK293315	DESIGNED:	NR	DRAWN:	NR
				DATE:	JULY 2024

DRAWING NO:	288933-WSP-XX-XX-DR-C-0153	REV:	P02
-------------	----------------------------	------	-----

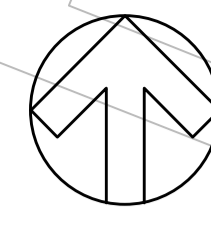
File name: C:\USERS\WATBOW\WSP_03602024\288933\TVC\LINTHORPE ROAD RESTORATION - DOCUMENTS\MP\PC\DWG\ENGINEERING\DWG\288933-WSP-XX-XX-DR-C-0153-SETTING OUT.DWG, printed on 10 October 2024 12:24:01, by: BenWiddess, Alan



KEY

- HIGHWAY BOUNDARY (REFER TO NOTE 8)
- PROPOSED CARRIAGEWAY
- EXISTING FOOTWAY
- PROPOSED FOOTWAY
- PROPOSED KERB
- PROPOSED BUFF BLISTER TACTILE PAVING
- PROPOSED RED BLISTER TACTILE PAVING
- PROPOSED WHITE LINE ROAD MARKING
- PROPOSED YELLOW LINE ROAD MARKING
- EXISTING TREE

REFER TO DRAWING 288933-WSP-XX-XX-DR-C-0153



AYRESOME STREET

EXTENT OF WORKS

DO NOT SCALE

NOTES

1. THIS DRAWING SHOULD BE READ IN CONJUNCTION WITH ALL RELATED DOCUMENTATION, DRAWINGS AND STANDARD DETAILS.
2. THE ORIGINAL OF THIS DRAWING WAS PRODUCED IN COLOUR - A MONOCHROME COPY CANNOT BE RELIED ON.
3. ALL DIMENSIONS ARE IN METRES UNLESS NOTED OTHERWISE.
4. INFORMATION REGARDING THE LOCATION AND DEPTH OF EXISTING SERVICES CANNOT BE GUARANTEED BY THE STATUTORY UNDERTAKER AND SHOULD BE INVESTIGATED. THE CONTRACTOR SHALL ESTABLISH THE PRECISE LINE AND DEPTH OF ALL EXISTING UTILITIES PRIOR TO COMMENCING ANY EXCAVATION WORK.
5. THE DESIGN IS BASED ON TOPOGRAPHICAL SURVEY REFERENCE ASS 2963 BY ADH SITE SURVEYS LIMITED RECEIVED FEBRUARY 2021
6. REFERENCE SHOULD BE MADE TO INDIVIDUAL DISCIPLINE DRAWINGS AND THE DESIGN RISK MANAGEMENT SCHEDULE FOR FURTHER HEALTH AND SAFETY INFORMATION - NOT ALL RISKS ARE IDENTIFIED IN THIS DRAWING.
7. CONFLICTING INFORMATION SHOWN ON THE ENGINEER'S DRAWINGS OR DISCREPANCIES BETWEEN THE INFORMATION GIVEN BY THE ENGINEER AND THAT PROVIDED BY OTHERS MUST BE REFERRED TO THE ENGINEER BEFORE THE WORKS COMMENCE.
8. THIS DRAWING IS BASED IN A WHOLE OR IN PART ON INFORMATION AND/OR DATA APPROVED BY A THIRD PARTY INCLUDING MIDDLESBROUGH COUNCIL AND AS SUCH IN CONTINGENT UPON THE ACCURACY AND COMPLETENESS OF THAT INFORMATION AND/OR DATA. WSP UK LIMITED SHALL NOT BE LIABLE IN RELATION TO ANY INACCURACY PRESENTED WITHIN THE DRAWING AND/OR IN RELATION TO ANY DISCREPANCY OR AMBIGUITY WITH ANY OTHER PROJECT DOCUMENTS SHOULD ANY DATA AND OR INFORMATION UPON WHICH RELIANCE HAS BEEN BASED BE INCORRECT OR OUT OF DATE OR HAVE BEEN CONCEALED, WITHHELD OR BE INCOMPLETE, MISREPRESENTED OR OTHERWISE NOT FULLY DISCLOSED TO WSP UK LIMITED.

SAFETY HEALTH & ENVIRONMENTAL INFORMATION BOX

HEALTH AND SAFETY SYMBOLS LEGEND

	INDICATES A RESIDUAL RISK REQUIRING A COMPULSORY ACTION		INDICATES A RESIDUAL RISK FOR INFORMATION
	INDICATES A RESIDUAL RISK REQUIRING A PROHIBITIVE ACTION		INDICATES A RESIDUAL RISK AS A WARNING
	INDICATES AN ENVIRONMENTAL HAZARD		

This table is provided to assist the Principal Contractor to fulfil their obligations under the Construction Design & Management Regulations, 2015. It does not include residual risks that a competent Contractor will be aware of nor does it absolve the Principal Contractor of their legal responsibilities.

The reference number corresponds to the Design Risk Management Schedule.

UNTIL TECHNICAL APPROVAL HAS BEEN OBTAINED FROM THE RELEVANT LOCAL AUTHORITIES OR STATUTORY BODIES, IT SHOULD BE UNDERSTOOD THAT ALL DRAWINGS ARE ISSUED AS PRELIMINARY AND NOT FOR CONSTRUCTION. SHOULD THE CONTRACTOR AND / OR EMPLOYER COMMENCE WORK PRIOR TO APPROVAL BEING GIVEN, IT IS ENTIRELY AT THEIR OWN RISK

PO2	10/10/24	AB	UPDATED FOLLOWING CLIENT REQUIREMENTS	JS	PWW
P01	12/09/24	NR	ISSUED FOR CLIENT APPROVAL	JS	PWW
REV	DATE	BY	DESCRIPTION	CHK	APP

DRAWING STATUS:

wsp

Amber Court, William Armstrong Drive, Newcastle upon Tyne, NE4 7YQ, UK
T+ 44 (0) 191 226 2000, F+ 44 (0) 191 226 2104
wsp.com

CLIENT:

TEES VALLEY

SITE/PROJECT:

LINTHORPE ROAD RESTORATION

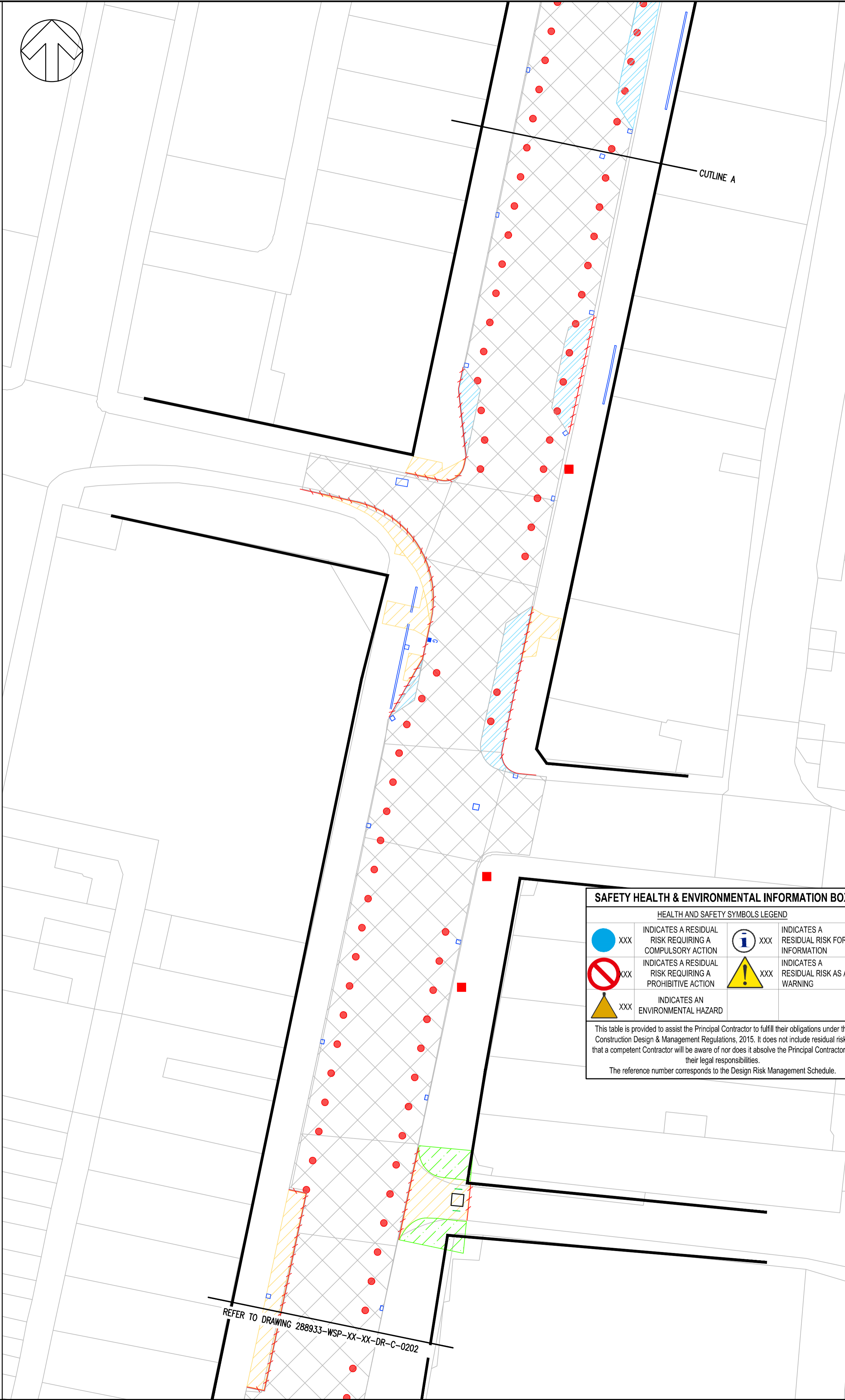
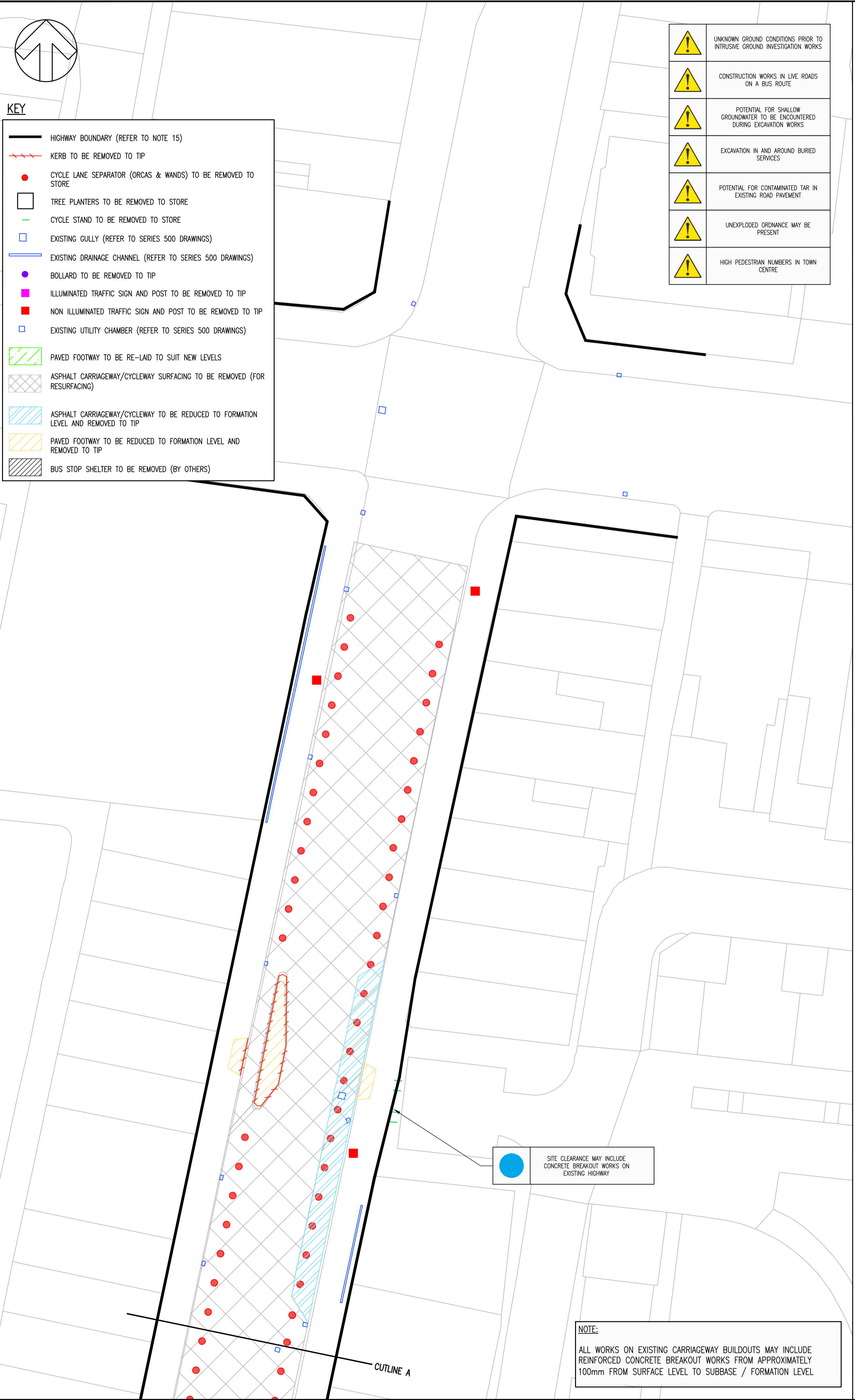
TITLE:

SETTING OUT
(SHEET 4 OF 4)

SCALE @ A1:	1:250	CHECKED:	JS	APPROVED:	PWW
PROJECT NO:	2024UK293315	DESIGNED:	NR	DRAWN:	NR
DRAWING NO:	288933-WSP-XX-XX-DR-C-0154	DATE:	JULY 2024	REV:	P02

File name: C:\USERS\WATBOW\WSP_03602024\288933_TVC\LINTHORPE ROAD RESTORATION - DOCUMENTS\01 MP\CV\01 CIVIL ENGINEERING\03 DRAWINGS\288933-WSP-XX-XX-DR-C-0154.SETTING OUT.DWG, printed on 10 October 2024 12:38:04, by Brouilless, Alan

File name: C:\Users\JMATB\OneDrive\Work\2024\24\288933\TVA\LINTHORPE ROAD RESTORATION - DOCUMENTS\01\MP\01\CIVIL\ENGINEERING\03 DRAWINGS\288933-WSP-XX-XX-DR-C-0201-0204 SITE CLEARANCE.DWG, printed on 10 October 2024, 10:19:04, by Bowditch, Alan



- NOTES**
- THIS DRAWING SHOULD BE READ IN CONJUNCTION WITH ALL RELEVANT DOCUMENTATION DRAWINGS AND STANDARD DETAILS BUT WITH PARTICULAR REFERENCE TO THE FOLLOWING
-288933-WSP-XX-XX-DR-C-0101-0104
-288933-WSP-XX-XX-DR-C-1101-1104
-288933-WSP-XX-XX-DR-C-0501-0504
 - ALL DIMENSIONS ARE IN METRES UNLESS STATED OTHERWISE.
 - THE ORIGINAL OF THIS DRAWING WAS PRODUCED IN COLOUR - A MONOCHROME COPY CANNOT BE RELIED ON.
 - INFORMATION REGARDING THE LOCATION AND DEPTH OF EXISTING SERVICES CANNOT BE GUARANTEED BY THE STATUTORY UNDERTAKER AND SHOULD BE INVESTIGATED. THE CONTRACTOR SHALL ESTABLISH THE PRECISE LINE AND DEPTH OF ALL EXISTING UTILITIES PRIOR TO COMMENCING ANY EXCAVATION WORK.
 - THE DESIGN IS BASED ON TOPOGRAPHICAL SURVEY REFERENCE ASS 2963 BY ADH SITE SURVEYS LIMITED RECEIVED FEBRUARY 2021.
 - CONFLICTING INFORMATION SHOWN ON THE ENGINEER'S DRAWINGS OR DISCREPANCIES BETWEEN THE INFORMATION GIVEN BY THE ENGINEER AND THAT PROVIDED BY OTHERS MUST BE REFERRED TO THE ENGINEER BEFORE THE WORKS COMMENCE.
 - ALL ITEMS IN THE KEY ARE EXISTING UNLESS NOTED OTHERWISE.
 - ALL ITEMS TO BE "REMOVED TO TIP" ARE TO BE MOVED TO THE CONTRACTORS LICENSED TIP OFF SITE.
 - ALL ITEMS TO BE "REMOVED TO STORE" ARE TO BE STORED TO BE BY THE CONTRACTOR EITHER ON OR OFF SITE UNTIL NEEDED.
 - EXISTING DRAINAGE APPARATUS TO BE REMOVED AS SHOWN AND BACKFILLED AS PER SPECIFICATION.
 - ALL ITEMS NOT HIGHLIGHTED ARE TO BE RETAINED UNLESS NOTED OTHERWISE.
 - FEED TO ALL ELECTRICAL ITEMS IS TO BE DISCONNECTED PRIOR TO REMOVAL.
 - FOR LOCATION OF PAVING AND KERB ITEMS TO BE REUSED, REFER TO LANDSCAPE ARCHITECT INFORMATION AND 1100 SERIES DRAWING AND SPECIFICATIONS.
 - NO EXISTING TRAFFIC SIGN OR LIGHTING COLUMN SHALL BE SWITCHED OFF, DISMANTLED OR REMOVED WITHOUT THE PRIOR APPROVAL OF MIDDLESBROUGH COUNCIL.
 - THIS DRAWING IS BASED IN A WHOLE OR IN PART ON INFORMATION AND/OR DATA APPROVED BY A THIRD PARTY INCLUDING MIDDLESBROUGH COUNCIL AND AS SUCH IN CONTINGENT UPON THE ACCURACY AND COMPLETENESS OF THAT INFORMATION AND/OR DATA. WSP UK LIMITED SHALL NOT BE LIABLE IN RELATION TO ANY INACCURACY PRESENTED WITHIN THE DRAWING AND/OR IN RELATION TO ANY DISCREPANCY OR AMBIGUITY WITH ANY OTHER PROJECT DOCUMENTS SHOULD ANY DATA AND OR INFORMATION UPON WHICH RELIANCE HAS BEEN BASED BE INCORRECT OR OUT OF DATE OR HAVE BEEN CONCEALED, WITHHELD OR BE INCOMPLETE, MISREPRESENTED OR OTHERWISE NOT FULLY DISCLOSED TO WSP UK LIMITED.

UNTIL TECHNICAL APPROVAL HAS BEEN OBTAINED FROM THE RELEVANT LOCAL AUTHORITIES OR STATUTORY BODIES, IT SHOULD BE UNDERSTOOD THAT ALL DRAWINGS ARE ISSUED AS PRELIMINARY AND NOT FOR CONSTRUCTION. SHOULD THE CONTRACTOR AND / OR EMPLOYER COMMENCE WORK PRIOR TO APPROVAL BEING GIVEN, IT IS ENTIRELY AT THEIR OWN RISK

REV	DATE	BY	DESCRIPTION	CHK	APP
PO4	10/10/24	AB	UPDATED FOLLOWING CLIENT REQUIREMENTS	JS	PWW
PO3	03/09/24	NR	UPDATED FOLLOWING CLIENT REVIEW	JS	PWW
PO2	31/07/24	NR	ISSUED FOR CLIENT APPROVAL	JS	PWW
PO1	23/07/24	NR	ISSUE FOR COSTING	JS	PWW
DR	15/07/24	NR	DRAFT ISSUE	JS	PWW

DRAWING STATUS:

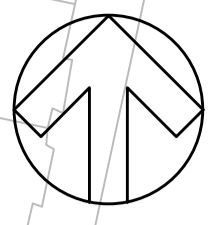
Amber Court, William Armstrong Drive, Newcastle upon Tyne, NE4 7YQ, UK
T+ 44 (0) 191 226 2000, F+ 44 (0) 191 226 2104
wsp.com

CLIENT:

SITE PROJECT:
LINTHORPE ROAD RESTORATION

TITLE:
SITE CLEARANCE (SHEET 1 OF 4)

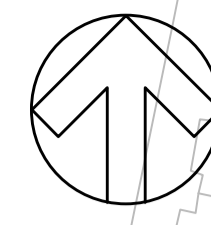
SCALE @ A1:	CHECKED:	APPROVED:	
1:250	JS	PWW	
PROJECT NO:	DESIGNED:	DRAWN:	DATE:
2024UK293315	NR	NR	JULY 2024
DRAWING NO:	REV:		
288933-WSP-XX-XX-DR-C-0201	P04		



- KEY**
- HIGHWAY BOUNDARY (REFER TO NOTE 15)
 - KERB TO BE REMOVED TO TIP
 - CYCLE LANE SEPARATOR (ORCAS & WANDS) TO BE REMOVED TO STORE
 - TREE PLANTERS TO BE REMOVED TO STORE
 - CYCLE STAND TO BE REMOVED TO STORE
 - EXISTING GULLY (REFER TO SERIES 500 DRAWINGS)
 - EXISTING DRAINAGE CHANNEL (REFER TO SERIES 500 DRAWINGS)
 - BOLLARD TO BE REMOVED TO TIP
 - ILLUMINATED TRAFFIC SIGN AND POST TO BE REMOVED TO TIP
 - NON ILLUMINATED TRAFFIC SIGN AND POST TO BE REMOVED TO TIP
 - EXISTING UTILITY CHAMBER (REFER TO SERIES 500 DRAWINGS)
 - ▨ PAVED FOOTWAY TO BE RE-LAID TO SUIT NEW LEVELS
 - ▨ ASPHALT CARRIAGEWAY/CYCLEWAY SURFACING TO BE REMOVED (FOR RESURFACING)
 - ▨ ASPHALT CARRIAGEWAY/CYCLEWAY TO BE REDUCED TO FORMATION LEVEL AND REMOVED TO TIP
 - ▨ PAVED FOOTWAY TO BE REDUCED TO FORMATION LEVEL AND REMOVED TO TIP
 - ▨ BUS STOP SHELTER TO BE REMOVED (BY OTHERS)

File name: C:\Users\WATBOW\OneDrive\Documents\WSP\CIVIL\ENGINEERING\DRAWINGS\288933\WSP-XX-XX-DR-C-0204 SITE CLEARANCE.DWG, printed on 10 October 2024 10:25:48, by Bowdass, Alan

REFER TO DRAWING 288933-WSP-XX-XX-DR-C-0201



SITE CLEARANCE MAY INCLUDE CONCRETE BREAKOUT WORKS ON EXISTING HIGHWAY

SITE CLEARANCE MAY INCLUDE CONCRETE BREAKOUT WORKS ON EXISTING HIGHWAY

CONTRACTOR TO LIAISE WITH CLEAR CHANNEL FOR BUS SHELTER REMOVAL

CONTRACTOR TO LIAISE WITH CLEAR CHANNEL FOR BUS SHELTER REMOVAL

- ⚠ UNKNOWN GROUND CONDITIONS PRIOR TO INTRUSIVE GROUND INVESTIGATION WORKS
- ⚠ CONSTRUCTION WORKS IN LIVE ROADS ON A BUS ROUTE
- ⚠ POTENTIAL FOR SHALLOW GROUNDWATER TO BE ENCOUNTERED DURING EXCAVATION WORKS
- ⚠ EXCAVATION IN AND AROUND BURIED SERVICES
- ⚠ POTENTIAL FOR CONTAMINATED TAR IN EXISTING ROAD PAVEMENT
- ⚠ UNEXPLODED ORDNANCE MAY BE PRESENT
- ⚠ HIGH PEDESTRIAN NUMBERS IN TOWN CENTRE

NOTE:
ALL WORKS ON EXISTING CARRIAGEWAY BUILDOUTS MAY INCLUDE REINFORCED CONCRETE BREAKOUT WORKS FROM APPROXIMATELY 100mm FROM SURFACE LEVEL TO SUBBASE / FORMATION LEVEL

REFER TO DRAWING 288933-WSP-XX-XX-DR-C-0203

SAFETY HEALTH & ENVIRONMENTAL INFORMATION BOX

HEALTH AND SAFETY SYMBOLS LEGEND

	INDICATES A RESIDUAL RISK REQUIRING A COMPULSORY ACTION		INDICATES A RESIDUAL RISK FOR INFORMATION
	INDICATES A RESIDUAL RISK REQUIRING A PROHIBITIVE ACTION		INDICATES A RESIDUAL RISK AS A WARNING
	INDICATES AN ENVIRONMENTAL HAZARD		

This table is provided to assist the Principal Contractor to fulfill their obligations under the Construction Design & Management Regulations, 2015. It does not include residual risks that a competent Contractor will be aware of nor does it absolve the Principal Contractor of their legal responsibilities.
The reference number corresponds to the Design Risk Management Schedule.

NOTES DO NOT SCALE

1. THIS DRAWING SHOULD BE READ IN CONJUNCTION WITH ALL RELEVANT DOCUMENTATION DRAWINGS AND STANDARD DETAILS BUT WITH PARTICULAR REFERENCE TO THE FOLLOWING
-288933-WSP-XX-XX-DR-C-0101-0104
-288933-WSP-XX-XX-DR-C-1101-1104
-288933-WSP-XX-XX-DR-C-0501-0504
2. ALL DIMENSIONS ARE IN METRES UNLESS STATED OTHERWISE.
3. THE ORIGINAL OF THIS DRAWING WAS PRODUCED IN COLOUR - A MONOCHROME COPY CANNOT BE RELIED ON.
4. INFORMATION REGARDING THE LOCATION AND DEPTH OF EXISTING SERVICES CANNOT BE GUARANTEED BY THE STATUTORY UNDERTAKER AND SHOULD BE INVESTIGATED. THE CONTRACTOR SHALL ESTABLISH THE PRECISE LINE AND DEPTH OF ALL EXISTING UTILITIES PRIOR TO COMMENCING ANY EXCAVATION WORK.
5. THE DESIGN IS BASED ON TOPOGRAPHICAL SURVEY REFERENCE ASS 2963 BY ADH SITE SURVEYS LIMITED RECEIVED FEBRUARY 2021.
6. CONFLICTING INFORMATION SHOWN ON THE ENGINEER'S DRAWINGS OR DISCREPANCIES BETWEEN THE INFORMATION GIVEN BY THE ENGINEER AND THAT PROVIDED BY OTHERS MUST BE REFERRED TO THE ENGINEER BEFORE THE WORKS COMMENCE.
7. ALL ITEMS IN THE KEY ARE EXISTING UNLESS NOTED OTHERWISE.
8. ALL ITEMS TO BE "REMOVED TO TIP" ARE TO BE MOVED TO THE CONTRACTORS LICENSED TIP OFF SITE.
9. ALL ITEMS TO BE "REMOVED TO STORE" ARE TO BE STORED TO BE BY THE CONTRACTOR EITHER ON OR OFF SITE UNTIL NEEDED.
10. EXISTING DRAINAGE APPARATUS TO BE REMOVED AS SHOWN AND BACKFILLED AS PER SERIES SPECIFICATION.
11. ALL ITEMS NOT HIGHLIGHTED ARE TO BE RETAINED UNLESS NOTED OTHERWISE.
12. FEED TO ALL ELECTRICAL ITEMS IS TO BE DISCONNECTED PRIOR TO REMOVAL.
13. FOR LOCATION OF PAVING AND KERB ITEMS TO BE REUSED, REFER TO LANDSCAPE ARCHITECT INFORMATION AND 1100 SERIES DRAWING AND SPECIFICATIONS.
14. NO EXISTING TRAFFIC SIGN OR LIGHTING COLUMN SHALL BE SWITCHED OFF, DISMANTLED OR REMOVED WITHOUT THE PRIOR APPROVAL OF MIDDLESBROUGH COUNCIL.
15. THIS DRAWING IS BASED IN A WHOLE OR IN PART ON INFORMATION AND/OR DATA APPROVED BY A THIRD PARTY INCLUDING MIDDLESBROUGH COUNCIL AND AS SUCH IN CONTINGENT UPON THE ACCURACY AND COMPLETENESS OF THAT INFORMATION AND/OR DATA. WSP UK LIMITED SHALL NOT BE LIABLE IN RELATION TO ANY INACCURACY PRESENTED WITHIN THE DRAWING AND/OR IN RELATION TO ANY DISCREPANCY OR AMBIGUITY WITH ANY OTHER PROJECT DOCUMENTS SHOULD ANY DATA AND OR INFORMATION UPON WHICH RELIANCE HAS BEEN BASED BE INCORRECT OR OUT OF DATE OR HAVE BEEN CONCEALED, WITHHELD OR BE INCOMPLETE, MISREPRESENTED OR OTHERWISE NOT FULLY DISCLOSED TO WSP UK LIMITED.

UNLESS TECHNICAL APPROVAL HAS BEEN OBTAINED FROM THE RELEVANT LOCAL AUTHORITIES OR STATUTORY BODIES, IT SHOULD BE UNDERSTOOD THAT ALL DRAWINGS ARE ISSUED AS PRELIMINARY AND NOT FOR CONSTRUCTION. SHOULD THE CONTRACTOR AND / OR EMPLOYER COMMENCE WORK PRIOR TO APPROVAL BEING GIVEN, IT IS ENTIRELY AT THEIR OWN RISK

PO4	10/10/24	AB	UPDATED FOLLOWING CLIENT REQUIREMENTS	JS	PWW
PO3	03/09/24	NR	UPDATED FOLLOWING CLIENT REVIEW	JS	PWW
PO2	31/07/24	NR	ISSUED FOR CLIENT APPROVAL	JS	PWW
PO1	23/07/24	NR	ISSUE FOR COSTING	JS	PWW
DR	15/07/24	NR	DRAFT ISSUE	JS	PWW
REV	DATE	BY	DESCRIPTION	CHK	APP

DRAWING STATUS:

wsp

Amber Court, William Armstrong Drive, Newcastle upon Tyne, NE4 7YQ, UK
T+ 44 (0) 191 226 2000, F+ 44 (0) 191 226 2104
wsp.com

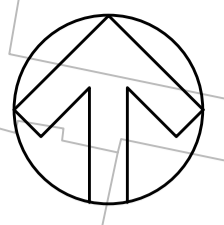
CLIENT:

TEES VALLEY

SITE PROJECT:
LINTHORPE ROAD RESTORATION

TITLE:
SITE CLEARANCE (SHEET 2 OF 4)

SCALE @ A1:	1:250	CHECKED:	JS	APPROVED:	PWW
PROJECT NO:	2024UK293315	DESIGNED:	NR	DATE:	JULY 2024
DRAWING NO:	288933-WSP-XX-XX-DR-C-0202	REV:			P04



- KEY**
- HIGHWAY BOUNDARY (REFER TO NOTE 15)
 - - - KERB TO BE REMOVED TO TIP
 - CYCLE LANE SEPARATOR (ORCAS & WANDS) TO BE REMOVED TO STORE
 - TREE PLANTERS TO BE REMOVED TO STORE
 - - - CYCLE STAND TO BE REMOVED TO STORE
 - EXISTING GULLY (REFER TO SERIES 500 DRAWINGS)
 - EXISTING DRAINAGE CHANNEL (REFER TO SERIES 500 DRAWINGS)
 - BOLLARD TO BE REMOVED TO TIP
 - ILLUMINATED TRAFFIC SIGN AND POST TO BE REMOVED TO TIP
 - NON ILLUMINATED TRAFFIC SIGN AND POST TO BE REMOVED TO TIP
 - EXISTING UTILITY CHAMBER (REFER TO SERIES 500 DRAWINGS)
 - ▨ PAVED FOOTWAY TO BE RE-LAID TO SUIT NEW LEVELS
 - ▨ ASPHALT CARRIAGEWAY/CYCLEWAY SURFACING TO BE REMOVED (FOR RESURFACING)
 - ▨ ASPHALT CARRIAGEWAY/CYCLEWAY TO BE REDUCED TO FORMATION LEVEL AND REMOVED TO TIP
 - ▨ PAVED FOOTWAY TO BE REDUCED TO FORMATION LEVEL AND REMOVED TO TIP
 - ▨ BUS STOP SHELTER TO BE REMOVED (BY OTHERS)

SITE CLEARANCE MAY INCLUDE CONCRETE BREAKOUT WORKS ON EXISTING HIGHWAY

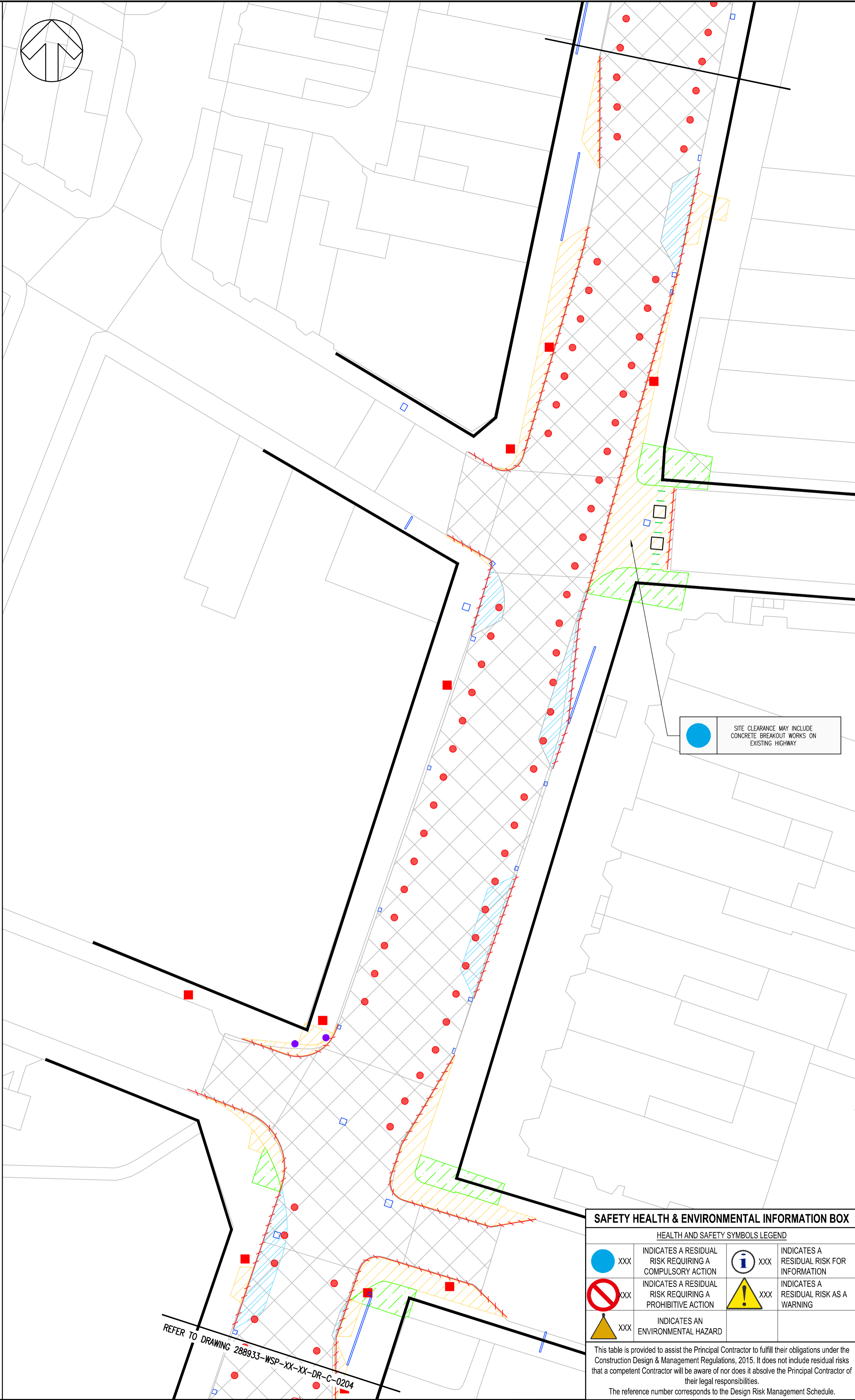
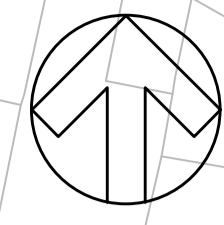
REFER TO DRAWING 288933-WSP-XX-XX-DR-C-0202

File name: C:\Users\JWATB\OneDrive\Documents\WSP\CIVIL\ENGINEERING\DWG\288933-WSP-XX-XX-DR-C-0204 SITE CLEARANCE.DWG, printed on 10 October 2024 10:31:18, by: Broudas, Alan

SITE CLEARANCE MAY INCLUDE CONCRETE BREAKOUT WORKS ON EXISTING HIGHWAY

- ⚠ UNKNOWN GROUND CONDITIONS PRIOR TO INTRUSIVE GROUND INVESTIGATION WORKS
- ⚠ CONSTRUCTION WORKS IN LIVE ROADS ON A BUS ROUTE
- ⚠ POTENTIAL FOR SHALLOW GROUNDWATER TO BE ENCOUNTERED DURING EXCAVATION WORKS
- ⚠ EXCAVATION IN AND AROUND BURIED SERVICES
- ⚠ POTENTIAL FOR CONTAMINATED TAR IN EXISTING ROAD PAVEMENT
- ⚠ UNEXPLODED ORDNANCE MAY BE PRESENT
- ⚠ HIGH PEDESTRIAN NUMBERS IN TOWN CENTRE

NOTE:
ALL WORKS ON EXISTING CARRIAGEWAY BUILDOUTS MAY INCLUDE REINFORCED CONCRETE BREAKOUT WORKS FROM APPROXIMATELY 100mm FROM SURFACE LEVEL TO SUBBASE / FORMATION LEVEL



SITE CLEARANCE MAY INCLUDE CONCRETE BREAKOUT WORKS ON EXISTING HIGHWAY

SAFETY HEALTH & ENVIRONMENTAL INFORMATION BOX

HEALTH AND SAFETY SYMBOLS LEGEND

● XXX	INDICATES A RESIDUAL RISK REQUIRING A COMPULSORY ACTION	ⓘ XXX	INDICATES A RESIDUAL RISK FOR INFORMATION
⊘ XXX	INDICATES A RESIDUAL RISK REQUIRING A PROHIBITIVE ACTION	⚠ XXX	INDICATES A RESIDUAL RISK AS A WARNING
⚠ XXX	INDICATES AN ENVIRONMENTAL HAZARD		

This table is provided to assist the Principal Contractor to fulfill their obligations under the Construction Design & Management Regulations, 2015. It does not include residual risks that a competent Contractor will be aware of nor does it absolve the Principal Contractor of their legal responsibilities.

The reference number corresponds to the Design Risk Management Schedule.

NOTES DO NOT SCALE

1. THIS DRAWING SHOULD BE READ IN CONJUNCTION WITH ALL RELEVANT DOCUMENTATION DRAWINGS AND STANDARD DETAILS BUT WITH PARTICULAR REFERENCE TO THE FOLLOWING
-288933-WSP-XX-XX-DR-C-0101-0104
-288933-WSP-XX-XX-DR-C-1101-1104
-288933-WSP-XX-XX-DR-C-0501-0504
2. ALL DIMENSIONS ARE IN METRES UNLESS STATED OTHERWISE.
3. THE ORIGINAL OF THIS DRAWING WAS PRODUCED IN COLOUR - A MONOCHROME COPY CANNOT BE RELIED ON.
4. INFORMATION REGARDING THE LOCATION AND DEPTH OF EXISTING SERVICES CANNOT BE GUARANTEED BY THE STATUTORY UNDERTAKER AND SHOULD BE INVESTIGATED. THE CONTRACTOR SHALL ESTABLISH THE PRECISE LINE AND DEPTH OF ALL EXISTING UTILITIES PRIOR TO COMMENCING ANY EXCAVATION WORK.
5. THE DESIGN IS BASED ON TOPOGRAPHICAL SURVEY REFERENCE ASS 2963 BY ADH SITE SURVEYS LIMITED RECEIVED FEBRUARY 2021.
6. CONFLICTING INFORMATION SHOWN ON THE ENGINEER'S DRAWINGS OR DISCREPANCIES BETWEEN THE INFORMATION GIVEN BY THE ENGINEER AND THAT PROVIDED BY OTHERS MUST BE REFERRED TO THE ENGINEER BEFORE THE WORKS COMMENCE.
7. ALL ITEMS IN THE KEY ARE EXISTING UNLESS NOTED OTHERWISE.
8. ALL ITEMS TO BE "REMOVED TO TIP" ARE TO BE MOVED TO THE CONTRACTORS LICENSED TIP OFF SITE.
9. ALL ITEMS TO BE "REMOVED TO STORE" ARE TO BE STORED TO BE BY THE CONTRACTOR EITHER ON OR OFF SITE UNTIL NEEDED.
10. EXISTING DRAINAGE APPARATUS TO BE REMOVED AS SHOWN AND BACKFILLED AS PER SPECIFICATION.
11. ALL ITEMS NOT HIGHLIGHTED ARE TO BE RETAINED UNLESS NOTED OTHERWISE.
12. FEED TO ALL ELECTRICAL ITEMS IS TO BE DISCONNECTED PRIOR TO REMOVAL.
13. FOR LOCATION OF PAVING AND KERB ITEMS TO BE REUSED, REFER TO LANDSCAPE ARCHITECT INFORMATION AND 1100 SERIES DRAWING AND SPECIFICATIONS.
14. NO EXISTING TRAFFIC SIGN OR LIGHTING COLUMN SHALL BE SWITCHED OFF, DISMANTLED OR REMOVED WITHOUT THE PRIOR APPROVAL OF MIDDLESBROUGH COUNCIL.
15. THIS DRAWING IS BASED IN A WHOLE OR IN PART ON INFORMATION AND/OR DATA APPROVED BY A THIRD PARTY INCLUDING MIDDLESBROUGH COUNCIL AND AS SUCH IN CONTINGENT UPON THE ACCURACY AND COMPLETENESS OF THAT INFORMATION AND/OR DATA. WSP UK LIMITED SHALL NOT BE LIABLE IN RELATION TO ANY INACCURACY PRESENTED WITHIN THE DRAWING AND/OR IN RELATION TO ANY DISCREPANCY OR AMBIGUITY WITH ANY OTHER PROJECT DOCUMENTS SHOULD ANY DATA AND OR INFORMATION UPON WHICH RELIANCE HAS BEEN BASED BE INCORRECT OR OUT OF DATE OR HAVE BEEN CONCEALED, WITHHELD OR BE INCOMPLETE, MISREPRESENTED OR OTHERWISE NOT FULLY DISCLOSED TO WSP UK LIMITED.

UNIL TECHNICAL APPROVAL HAS BEEN OBTAINED FROM THE RELEVANT LOCAL AUTHORITIES OR STATUTORY BODIES, IT SHOULD BE UNDERSTOOD THAT ALL DRAWINGS ARE ISSUED AS PRELIMINARY AND NOT FOR CONSTRUCTION. SHOULD THE CONTRACTOR AND / OR EMPLOYER COMMENCE WORK PRIOR TO APPROVAL BEING GIVEN, IT IS ENTIRELY AT THEIR OWN RISK

REV	DATE	BY	DESCRIPTION	CHK	APP
P04	10/10/24	AB	UPDATED FOLLOWING CLIENT REQUIREMENTS	JS	PWW
P03	03/09/24	NR	UPDATED FOLLOWING CLIENT REVIEW	JS	PWW
P02	31/07/24	NR	ISSUED FOR CLIENT APPROVAL	JS	PWW
P01	23/07/24	NR	ISSUE FOR COSTING	JS	PWW
DR	15/07/24	NR	DRAFT ISSUE	JS	PWW

DRAWING STATUS:

wsp

Amber Court, William Armstrong Drive, Newcastle upon Tyne, NE4 7YQ, UK
T+ 44 (0) 191 226 2000, F+ 44 (0) 191 226 2104
wsp.com

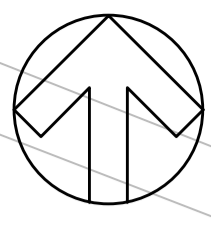
CLIENT:

TEES VALLEY

SITE PROJECT:
LINTHORPE ROAD RESTORATION

TITLE:
SITE CLEARANCE
(SHEET 3 OF 4)

SCALE @ A1:	1:250	CHECKED:	JS	APPROVED:	PWW
PROJECT NO:	2024UK293315	DESIGNED:	NR	DATE:	JULY 2024
DRAWING NO:	288933-WSP-XX-XX-DR-C-0203	REV:	P04		



- KEY**
- HIGHWAY BOUNDARY (REFER TO NOTE 15)
 - - - KERB TO BE REMOVED TO TIP
 - CYCLE LANE SEPARATOR (ORCAS & WANDS) TO BE REMOVED TO STORE
 - TREE PLANTERS TO BE REMOVED TO STORE
 - - - CYCLE STAND TO BE REMOVED TO STORE
 - EXISTING GULLY (REFER TO SERIES 500 DRAWINGS)
 - EXISTING DRAINAGE CHANNEL (REFER TO SERIES 500 DRAWINGS)
 - BOLLARD TO BE REMOVED TO TIP
 - ILLUMINATED TRAFFIC SIGN AND POST TO BE REMOVED TO TIP
 - NON ILLUMINATED TRAFFIC SIGN AND POST TO BE REMOVED TO TIP
 - EXISTING UTILITY CHAMBER (REFER TO SERIES 500 DRAWINGS)
 - ▨ PAVED FOOTWAY TO BE RE-LAID TO SUIT NEW LEVELS
 - ▨ ASPHALT CARRIAGEWAY/CYCLEWAY SURFACING TO BE REMOVED (FOR RESURFACING)
 - ▨ ASPHALT CARRIAGEWAY/CYCLEWAY TO BE REDUCED TO FORMATION LEVEL AND REMOVED TO TIP
 - ▨ PAVED FOOTWAY TO BE REDUCED TO FORMATION LEVEL AND REMOVED TO TIP
 - ▨ BUS STOP SHELTER TO BE REMOVED (BY OTHERS)

REFER TO DRAWING 288933-WSP-XX-XX-DR-C-0203

CONTRACTOR TO LIAISE WITH CLEAR CHANNEL FOR BUS SHELTER REMOVAL

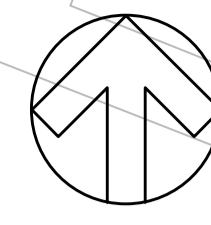
CONTRACTOR TO LIAISE WITH CLEAR CHANNEL FOR BUS SHELTER REMOVAL

SITE CLEARANCE MAY INCLUDE CONCRETE BREAKOUT WORKS ON EXISTING HIGHWAY

CONTRACTOR TO LIAISE WITH CLEAR CHANNEL FOR BUS SHELTER REMOVAL

- UNKNOWN GROUND CONDITIONS PRIOR TO INTRUSIVE GROUND INVESTIGATION WORKS
- CONSTRUCTION WORKS IN LIVE ROADS ON A BUS ROUTE
- POTENTIAL FOR SHALLOW GROUNDWATER TO BE ENCOUNTERED DURING EXCAVATION WORKS
- EXCAVATION IN AND AROUND BURIED SERVICES
- POTENTIAL FOR CONTAMINATED TAR IN EXISTING ROAD PAVEMENT
- UNEXPLODED ORDNANCE MAY BE PRESENT
- HIGH PEDESTRIAN NUMBERS IN TOWN CENTRE

NOTE:
ALL WORKS ON EXISTING CARRIAGEWAY BUILDOUTS MAY INCLUDE REINFORCED CONCRETE BREAKOUT WORKS FROM APPROXIMATELY 100mm FROM SURFACE LEVEL TO SUBBASE / FORMATION LEVEL



NOTES DO NOT SCALE

1. THIS DRAWING SHOULD BE READ IN CONJUNCTION WITH ALL RELEVANT DOCUMENTATION DRAWINGS AND STANDARD DETAILS BUT WITH PARTICULAR REFERENCE TO THE FOLLOWING
-288933-WSP-XX-XX-DR-C-0101-0104
-288933-WSP-XX-XX-DR-C-1101-1104
-288933-WSP-XX-XX-DR-C-0501-0504
2. ALL DIMENSIONS ARE IN METRES UNLESS STATED OTHERWISE.
3. THE ORIGINAL OF THIS DRAWING WAS PRODUCED IN COLOUR - A MONOCHROME COPY CANNOT BE RELIED ON.
4. INFORMATION REGARDING THE LOCATION AND DEPTH OF EXISTING SERVICES CANNOT BE GUARANTEED BY THE STATUTORY UNDERTAKER AND SHOULD BE INVESTIGATED. THE CONTRACTOR SHALL ESTABLISH THE PRECISE LINE AND DEPTH OF ALL EXISTING UTILITIES PRIOR TO COMMENCING ANY EXCAVATION WORK.
5. THE DESIGN IS BASED ON TOPOGRAPHICAL SURVEY REFERENCE ASS 2963 BY ADH SITE SURVEYS LIMITED RECEIVED FEBRUARY 2021.
6. CONFLICTING INFORMATION SHOWN ON THE ENGINEER'S DRAWINGS OR DISCREPANCIES BETWEEN THE INFORMATION GIVEN BY THE ENGINEER AND THAT PROVIDED BY OTHERS MUST BE REFERRED TO THE ENGINEER BEFORE THE WORKS COMMENCE.
7. ALL ITEMS IN THE KEY ARE EXISTING UNLESS NOTED OTHERWISE.
8. ALL ITEMS TO BE "REMOVED TO TIP" ARE TO BE MOVED TO THE CONTRACTORS LICENSED TIP OFF SITE.
9. ALL ITEMS TO BE "REMOVED TO STORE" ARE TO BE STORED TO BE BY THE CONTRACTOR EITHER ON OR OFF SITE UNTIL NEEDED.
10. EXISTING DRAINAGE APPARATUS TO BE REMOVED AS SHOWN AND BACKFILLED AS PER SPECIFICATION.
11. ALL ITEMS NOT HIGHLIGHTED ARE TO BE RETAINED UNLESS NOTED OTHERWISE.
12. FEED TO ALL ELECTRICAL ITEMS IS TO BE DISCONNECTED PRIOR TO REMOVAL.
13. FOR LOCATION OF PAVING AND KERB ITEMS TO BE REUSED, REFER TO LANDSCAPE ARCHITECT INFORMATION AND 1100 SERIES DRAWING AND SPECIFICATIONS.
14. NO EXISTING TRAFFIC SIGN OR LIGHTING COLUMN SHALL BE SWITCHED OFF, DISMANTLED OR REMOVED WITHOUT THE PRIOR APPROVAL OF MIDDLESBROUGH COUNCIL.
15. THIS DRAWING IS BASED IN A WHOLE OR IN PART ON INFORMATION AND/OR DATA APPROVED BY A THIRD PARTY INCLUDING MIDDLESBROUGH COUNCIL AND AS SUCH IN CONTINGENT UPON THE ACCURACY AND COMPLETENESS OF THAT INFORMATION AND/OR DATA. WSP UK LIMITED SHALL NOT BE LIABLE IN RELATION TO ANY INACCURACY PRESENTED WITHIN THE DRAWING AND/OR IN RELATION TO ANY DISCREPANCY OR AMBIGUITY WITH ANY OTHER PROJECT DOCUMENTS SHOULD ANY DATA AND OR INFORMATION UPON WHICH RELIANCE HAS BEEN BASED BE INCORRECT OR OUT OF DATE OR HAVE BEEN CONCEALED, WITHHELD OR BE INCOMPLETE, MISREPRESENTED OR OTHERWISE NOT FULLY DISCLOSED TO WSP UK LIMITED.

UNLIT TECHNICAL APPROVAL HAS BEEN OBTAINED FROM THE RELEVANT LOCAL AUTHORITIES OR STATUTORY BODIES. IT SHOULD BE UNDERSTOOD THAT ALL DRAWINGS ARE ISSUED AS PRELIMINARY AND NOT FOR CONSTRUCTION. SHOULD THE CONTRACTOR AND / OR EMPLOYER COMMENCE WORK PRIOR TO APPROVAL BEING GIVEN, IT IS ENTIRELY AT THEIR OWN RISK

PO4	10/10/24	AB	UPDATED FOLLOWING CLIENT REQUIREMENTS	JS	PWW
PO3	03/09/24	NR	UPDATED FOLLOWING CLIENT REVIEW	JS	PWW
PO2	31/07/24	NR	ISSUED FOR CLIENT APPROVAL	JS	PWW
PO1	23/07/24	NR	ISSUE FOR COSTING	JS	PWW
DR	15/07/24	NR	DRAFT ISSUE	JS	PWW
REV	DATE	BY	DESCRIPTION	CHK	APP

DRAWING STATUS:

wsp

Amber Court, William Armstrong Drive, Newcastle upon Tyne, NE4 7YQ, UK
T+44 (0) 191 226 2000, F+44 (0) 191 226 2104
wsp.com

CLIENT:

TEES VALLEY

SITE PROJECT:
LINTHORPE ROAD RESTORATION

TITLE:
SITE CLEARANCE (SHEET 4 OF 4)

SCALE @ A1:	1:250	CHECKED:	JS	APPROVED:	PWW
PROJECT NO:	2024UK293315	DESIGNED:	NR	DATE:	JULY 2024
DRAWING NO:	288933-WSP-XX-XX-DR-C-0204	REV:	P04		

SAFETY HEALTH & ENVIRONMENTAL INFORMATION BOX

HEALTH AND SAFETY SYMBOLS LEGEND

	INDICATES A RESIDUAL RISK REQUIRING A COMPULSORY ACTION		INDICATES A RESIDUAL RISK FOR INFORMATION
	INDICATES A RESIDUAL RISK REQUIRING A PROHIBITIVE ACTION		INDICATES A RESIDUAL RISK AS A WARNING
	INDICATES AN ENVIRONMENTAL HAZARD		

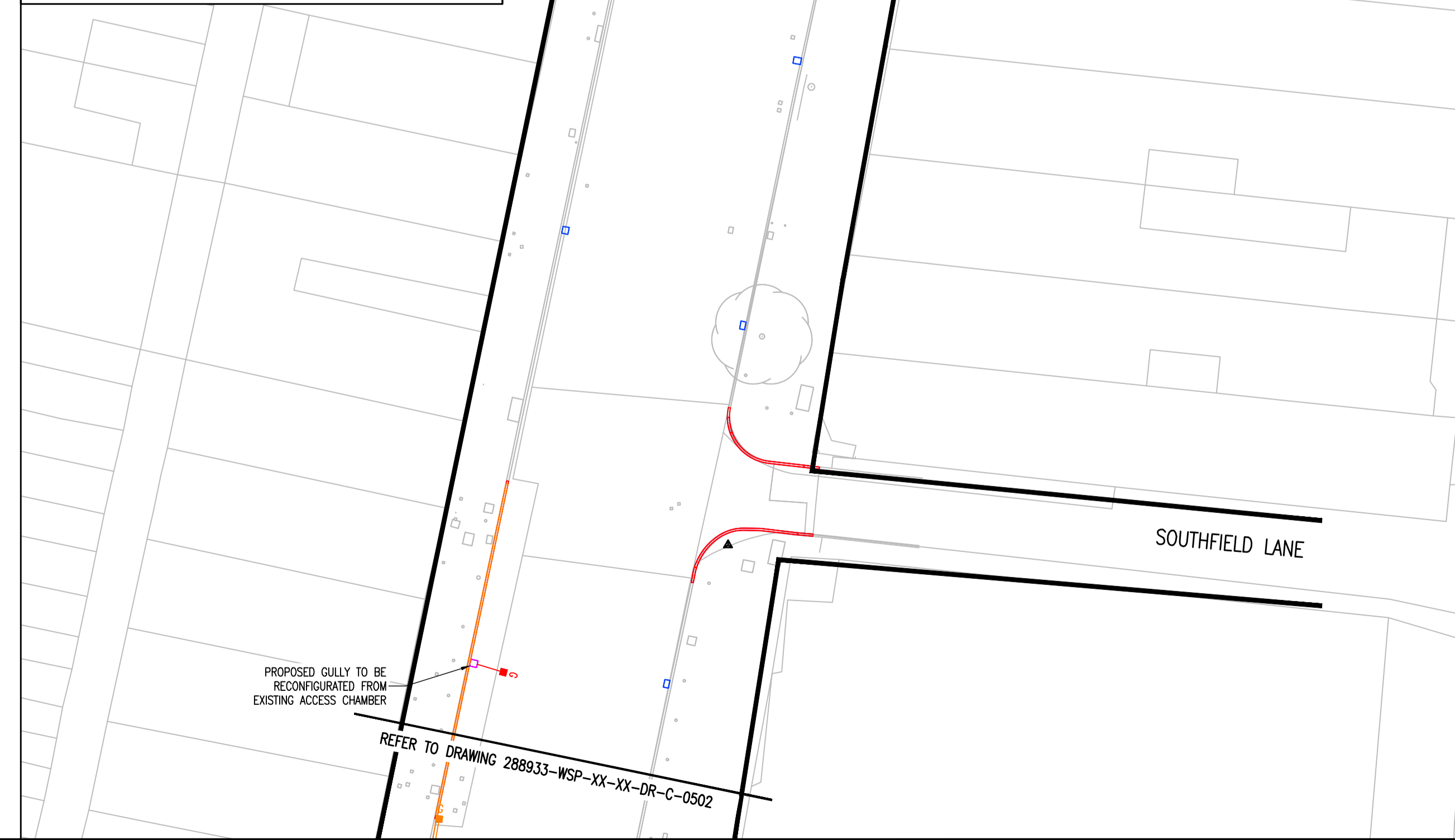
This table is provided to assist the Principal Contractor to fulfill their obligations under the Construction Design & Management Regulations, 2015. It does not include residual risks that a competent Contractor will be aware of nor does it absolve the Principal Contractor of their legal responsibilities.

The reference number corresponds to the Design Risk Management Schedule.

File name: C:\Users\A1\Documents\WSP\2024\2024UK293315\TVA\LINTHORPE ROAD RESTORATION - DOCUMENTS\00_MPC\01_CIVIL_ENGINEERING\03_DRAWINGS\288933-WSP-XX-XX-DR-C-0204-SITE CLEARANCE.DWG, printed on 10 October 2024 10:37:41 by Boundless Alan



- NOTES**
- THIS DRAWING SHOULD BE READ IN CONJUNCTION WITH ALL RELEVANT DOCUMENTATION DRAWINGS AND STANDARD DETAILS BUT WITH PARTICULAR REFERENCE TO THE FOLLOWING
 -288933-WSP-XX-XX-DR-C-0101-0104
 -288933-WSP-XX-XX-DR-C-1101-1104
 -288933-WSP-XX-XX-DR-C-0511
 -288933-WSP-XX-XX-DR-C-0201-0204
 - ALL DIMENSIONS ARE IN METRES UNLESS NOTED OTHERWISE.
 - THE ORIGINAL OF THIS DRAWING WAS PRODUCED IN COLOUR - A MONOCHROME COPY CANNOT BE RELIED ON.
 - INFORMATION REGARDING THE LOCATION AND DEPTH OF EXISTING SERVICES CANNOT BE GUARANTEED BY THE STATUTORY UNDERTAKER AND SHOULD BE INVESTIGATED. THE CONTRACTOR SHALL ESTABLISH THE PRECISE LINE AND DEPTH OF ALL EXISTING UTILITIES PRIOR TO COMMENCING ANY EXCAVATION WORK.
 - THE CONTRACTOR SHALL CONDUCT THE WORKS WITH DUE REGARDS TO ANY ECOLOGICAL AND ENVIRONMENTAL REQUIREMENTS OF THE SCHEME. THE CONTRACTOR SHALL TAKE SUCH STEPS TO SAFEGUARD AGAINST CONTAMINATION OF LOCAL WATERCOURSES.
 - UPON COMPLETION OF THE WORKS, THE CONTRACTOR SHALL ENSURE THAT ALL DRAINAGE APPARATUS IS JETTED AND FREE FROM OBSTRUCTIONS AND SUBSEQUENT SUBJECT TO CCTV SURVEY AND TESTING IN ACCORDANCE WITH THE DRAINAGE SPECIFICATION.
 - GRATINGS OF GULLIES LOCATED IN THE CYCLEWAY ARE TO BE CYCLE FRIENDLY. EXISTING GULLY GRATINGS ARE TO BE ASSESSED FOR THE SUITABILITY AND REPLACED WHERE APPROPRIATE
 - NEW DRAINAGE CHANNELS ARE TO BE ACO PARK DRAIN UNIT REFERENCE PD100C 25.1 (SUBJECT TO CONFIRMATION OF CONNECTING CHANNEL SIZES). FOR CONSTRUCTION DETAILS REFER TO WSP DRAWING 288933-WSP-XX-XX-DR-C-0511.
 - WHERE EXISTING GULLY OUTLETS TO BE KEPT LIVE AND GRATING TO BE REPLACED WITH ACCESS COVER, THEY SHOULD BE ADJUSTED TO FINISH LEVEL AND MOVED IF FEASIBLE TO AVOID CLASH WITH KERBLINE.
 - WHERE GULLIES ARE TO BE REPLACED WITH SHALLOW INSPECTION CHAMBERS, THEY ARE TO BE LOCATED OUTSIDE THE CARRIAGEWAY WHERE POSSIBLE AND THE EXISTING GULLY CONNECTION IS TO BE RE-USED.
 - WHERE SHALLOW GULLIES ARE REQUIRED DUE TO EXISTING SHALLOW CONNECTION DEPTHS OR UTILITY CONSTRAINTS, WREKIN GULLY CHUTE CONNECTOR (OR SIMILAR APPROVED) IS TO BE USED.
 - THE DESIGN IS BASED ON TOPOGRAPHICAL SURVEY REFERENCE ASS 2963 BY ADH SITE SURVEYS LIMITED RECEIVED FEBRUARY 2021.
 - THIS DRAWING IS BASED IN A WHOLE OR IN PART ON INFORMATION AND/OR DATA APPROVED BY A THIRD PARTY INCLUDING MIDDLESBROUGH COUNCIL AND AS SUCH IN CONTINGENT UPON THE ACCURACY AND COMPLETENESS OF THAT INFORMATION AND/OR DATA. WSP UK LIMITED SHALL NOT BE LIABLE IN RELATION TO ANY INACCURACY PRESENTED WITHIN THE DRAWING AND/OR IN RELATION TO ANY DISCREPANCY OR AMBIGUITY WITH ANY OTHER PROJECT DOCUMENTS SHOULD ANY DATA AND/OR INFORMATION UPON WHICH RELIANCE HAS BEEN BASED BE INCORRECT OR OUT OF DATE OR HAVE BEEN CONCEALED, WITHHELD OR BE INCOMPLETE, MISREPRESENTED OR OTHERWISE NOT FULLY DISCLOSED TO WSP UK LIMITED.
 - REFERENCE SHOULD BE MADE TO INDIVIDUAL DISCIPLINE DRAWINGS AND THE DESIGN RISK MANAGEMENT SCHEDULE FOR FURTHER HEALTH AND SAFETY INFORMATION - NOT ALL RISKS ARE IDENTIFIED WITHIN THIS DRAWING.
 - ALL PROPOSED BELOW GROUND DRAINAGE PIPELINES/GULLY CONNECTIONS TO BE 150MM DIAMETER UNLESS NOTED OTHERWISE.
 - TEMPORARY WORKS DEIGN ASSOCIATED WITH THE CONSTRUCTION OF THE WORKS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
 - ALL GULLIES IN FOOTWAYS ARE TO INCLUDE PEDESTRIAN FRIENDLY GULLY GRATINGS TO REDUCE TRIP HAZARDS



DO NOT SCALE

KEY

- HIGHWAY BOUNDARY (REFER TO NOTE 13)
- PROPOSED KERB
- PROPOSED GULLY
- PROPOSED GULLY CONNECTION
- EXISTING GULLY TO BE RETAINED
- EXISTING GULLY TO BE REMOVED
- EXISTING GULLY CONNECTION TO BE REMOVED
- EXISTING DRAINAGE CONNECTION CHAMBER TO BE RETAINED
- PROPOSED DRAINAGE KERB
- PROPOSED DRAINAGE CHANNEL
- EXISTING DRAINAGE CHANNEL TO BE RETAINED
- EXISTING DRAINAGE CHANNEL TO BE REMOVED
- EXISTING DRAINAGE MANHOLE CHAMBER
- EXISTING GULLY TO BE ADJUSTED
- EXISTING DRAINAGE CONNECTION CHAMBER TO BE ADJUSTED

SAFETY HEALTH & ENVIRONMENTAL INFORMATION BOX

HEALTH AND SAFETY SYMBOLS LEGEND

	INDICATES A RESIDUAL RISK REQUIRING A COMPULSORY ACTION		INDICATES A RESIDUAL RISK FOR INFORMATION
	INDICATES A RESIDUAL RISK REQUIRING A PROHIBITIVE ACTION		INDICATES A RESIDUAL RISK AS A WARNING
	INDICATES AN ENVIRONMENTAL HAZARD		

This table is provided to assist the Principal Contractor to fulfill their obligations under the Construction Design & Management Regulations, 2015. It does not include residual risks that a competent Contractor will be aware of nor does it absolve the Principal Contractor of their legal responsibilities.
The reference number corresponds to the Design Risk Management Schedule.

UNTIL TECHNICAL APPROVAL HAS BEEN OBTAINED FROM THE RELEVANT LOCAL AUTHORITIES OR STATUTORY BODIES, IT SHOULD BE UNDERSTOOD THAT ALL DRAWINGS ARE ISSUED AS PRELIMINARY AND NOT FOR CONSTRUCTION. SHOULD THE CONTRACTOR AND / OR EMPLOYER COMMENCE WORK PRIOR TO APPROVAL BEING GIVEN, IT IS ENTIRELY AT THEIR OWN RISK

REV	DATE	BY	DESCRIPTION	CHK	APP
P04	10/10/24	AB	UPDATED FOLLOWING CLIENT REQUIREMENTS	JS	PWW
P03	03/09/24	NR	UPDATED FOLLOWING CLIENT REVIEW	JS	PWW
P02	31/07/24	NR	ISSUED FOR CLIENT APPROVAL	JS	PWW
P01	23/07/24	NR	ISSUE FOR COST ESTIMATE	JS	PWW
DR	16/07/24	NR	DRAFT ISSUE	JS	PWW

DRAWING STATUS:

Amber Court, William Armstrong Drive, Newcastle upon Tyne, NE4 7YQ, UK
 T+ 44 (0) 191 226 2000, F+ 44 (0) 191 226 2104
 wsp.com

CLIENT:

SITE/PROJECT:

LINTHORPE ROAD RESTORATION

TITLE:

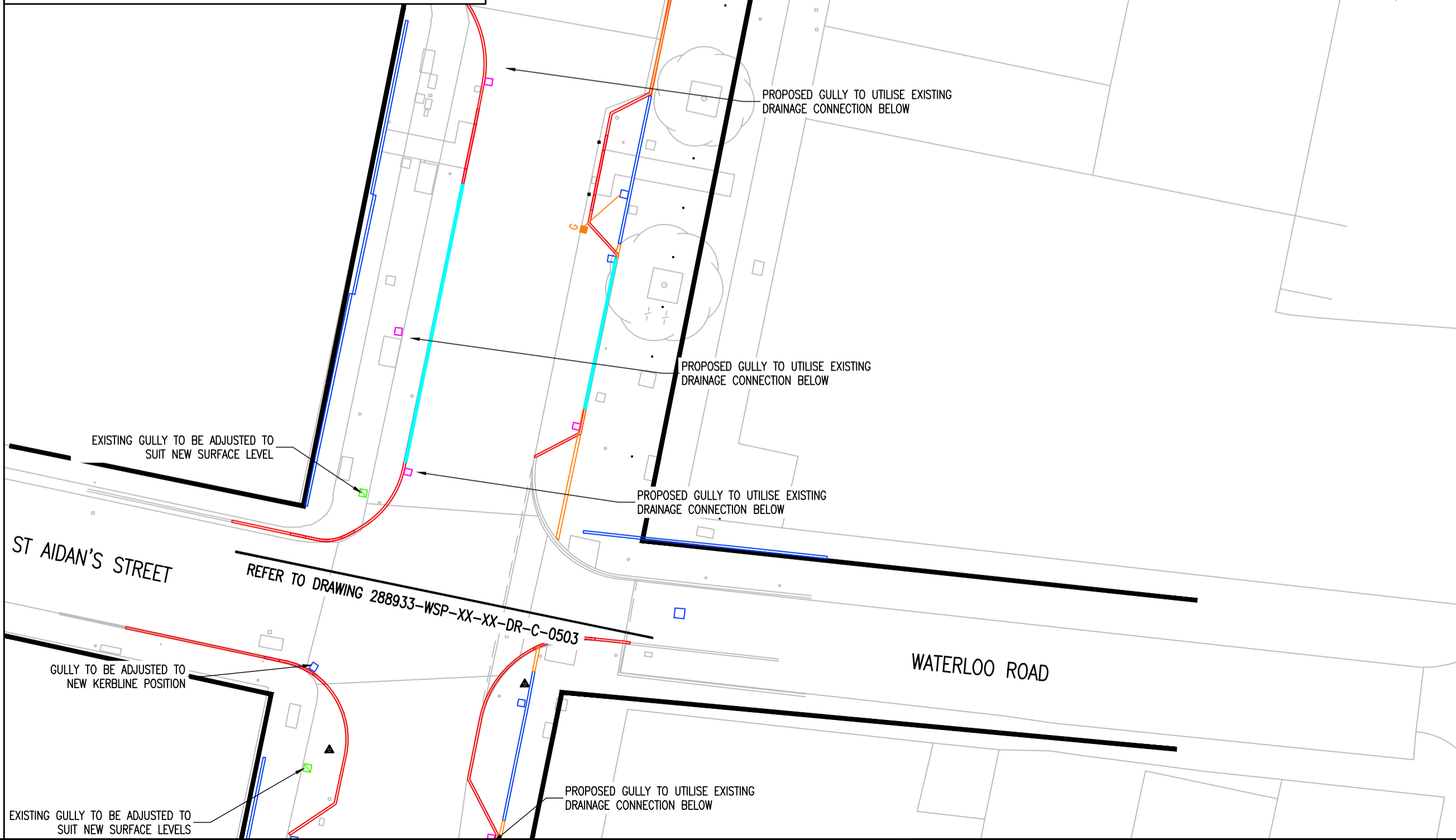
**BELOW GROUND DRAINAGE LAYOUT
(SHEET 1 OF 4)**

<small>SCALE @ A1:</small>	1:250	<small>CHECKED:</small>	JS	<small>APPROVED:</small>	PWW	
<small>PROJECT NO:</small>	2024UK293315	<small>DESIGNED:</small>	NR	<small>DRAWN:</small>	NR	
<small>DRAWING NO:</small>	288933-WSP-XX-XX-DR-C-0501				<small>REV:</small>	P04

File name: C:\USERS\WATBOW\WSP_0360\2024\UK293315\T\CLINTHORPE ROAD RESTORATION - DOCUMENTS\03 MPO\01 CIVIL ENGINEERING\03 DRAWINGS\03 WSP\XX-XX-DR-C-0501-0501 DRAINAGE DWG.dwg, printed on: 10 October 2024 12:47:31, by: Rowland, Alan



- NOTES**
- THIS DRAWING SHOULD BE READ IN CONJUNCTION WITH ALL RELEVANT DOCUMENTATION DRAWINGS AND STANDARD DETAILS BUT WITH PARTICULAR REFERENCE TO THE FOLLOWING
 -288933-WSP-XX-XX-DR-C-0101-0104
 -288933-WSP-XX-XX-DR-C-1101-1104
 -288933-WSP-XX-XX-DR-C-0511
 -288933-WSP-XX-XX-DR-C-0201-0204
 - ALL DIMENSIONS ARE IN METRES UNLESS NOTED OTHERWISE.
 - THE ORIGINAL OF THIS DRAWING WAS PRODUCED IN COLOUR - A MONOCHROME COPY CANNOT BE RELIED ON.
 - INFORMATION REGARDING THE LOCATION AND DEPTH OF EXISTING SERVICES CANNOT BE GUARANTEED BY THE STATUTORY UNDERTAKER AND SHOULD BE INVESTIGATED. THE CONTRACTOR SHALL ESTABLISH THE PRECISE LINE AND DEPTH OF ALL EXISTING UTILITIES PRIOR TO COMMENCING ANY EXCAVATION WORK.
 - THE CONTRACTOR SHALL CONDUCT THE WORKS WITH DUE REGARD TO ANY ECOLOGICAL AND ENVIRONMENTAL REQUIREMENTS OF THE SCHEME. THE CONTRACTOR SHALL TAKE SUCH STEPS TO SAFEGUARD AGAINST CONTAMINATION OF LOCAL WATERCOURSES.
 - UPON COMPLETION OF THE WORKS, THE CONTRACTOR SHALL ENSURE THAT ALL DRAINAGE APPARATUS IS JETTED AND FREE FROM OBSTRUCTIONS AND SUBSEQUENT SUBJECT TO CCTV SURVEY AND TESTING IN ACCORDANCE WITH THE DRAINAGE SPECIFICATION.
 - GRATINGS OF GULLIES LOCATED IN THE CYCLEWAY ARE TO BE CYCLE FRIENDLY. EXISTING GULLY GRATINGS ARE TO BE ASSESSED FOR THE SUITABILITY AND REPLACED WHERE APPROPRIATE
 - NEW DRAINAGE CHANNELS ARE TO BE ACO PARK DRAIN UNIT REFERENCE PD100C 25.1 (SUBJECT TO CONFIRMATION OF CONNECTING CHANNEL SIZES). FOR CONSTRUCTION DETAILS REFER TO WSP DRAWING 288933-WSP-XX-XX-DR-C-0511.
 - WHERE EXISTING GULLY OUTLETS TO BE KEPT LIVE AND GRATING TO BE REPLACED WITH ACCESS COVER, THEY SHOULD BE ADJUSTED TO FINISH LEVEL AND MOVED IF FEASIBLE TO AVOID CLASH WITH KERBLINE.
 - WHERE GULLIES ARE TO BE REPLACED WITH SHALLOW INSPECTION CHAMBERS, THEY ARE TO BE LOCATED OUTSIDE THE CARRIAGEWAY WHERE POSSIBLE AND THE EXISTING GULLY CONNECTION IS TO BE RE-USED.
 - WHERE SHALLOW GULLIES ARE REQUIRED DUE TO EXISTING SHALLOW CONNECTION DEPTHS OR UTILITY CONSTRAINTS, WREKIN GULLY CHUTE CONNECTOR (OR SIMILAR APPROVED) IS TO BE USED.
 - THE DESIGN IS BASED ON TOPOGRAPHICAL SURVEY REFERENCE ASS 2963 BY ADH SITE SURVEYS LIMITED RECEIVED FEBRUARY 2021.
 - THIS DRAWING IS BASED IN A WHOLE OR IN PART ON INFORMATION AND/OR DATA APPROVED BY A THIRD PARTY INCLUDING MIDDLESBROUGH COUNCIL AND AS SUCH IN CONTINGENT UPON THE ACCURACY AND COMPLETENESS OF THAT INFORMATION AND/OR DATA. WSP UK LIMITED SHALL NOT BE LIABLE IN RELATION TO ANY INACCURACY PRESENTED WITHIN THE DRAWING AND/OR IN RELATION TO ANY DISCREPANCY OR AMBIGUITY WITH ANY OTHER PROJECT DOCUMENTS. SHOULD ANY DATA AND/OR INFORMATION UPON WHICH RELIANCE HAS BEEN BASED BE INCORRECT OR OUT OF DATE OR HAVE BEEN CONCEALED, WITHHELD OR BE INCOMPLETE, MISREPRESENTED OR OTHERWISE NOT FULLY DISCLOSED TO WSP UK LIMITED.
 - REFERENCE SHOULD BE MADE TO INDIVIDUAL DISCIPLINE DRAWINGS AND THE DESIGN RISK MANAGEMENT SCHEDULE FOR FURTHER HEALTH AND SAFETY INFORMATION - NOT ALL RISKS ARE IDENTIFIED WITHIN THIS DRAWING.
 - ALL PROPOSED BELOW GROUND DRAINAGE PIPELINES/GULLY CONNECTIONS TO BE 150MM DIAMETER UNLESS NOTED OTHERWISE.
 - TEMPORARY WORKS DEIGN ASSOCIATED WITH THE CONSTRUCTION OF THE WORKS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
 - ALL GULLIES IN FOOTWAYS ARE TO INCLUDE PEDESTRIAN FRIENDLY GULLY GRATINGS TO REDUCE TRIP HAZARDS



DO NOT SCALE

KEY

- HIGHWAY BOUNDARY (REFER TO NOTE 13)
- PROPOSED KERB
- PROPOSED GULLY
- PROPOSED GULLY CONNECTION
- EXISTING GULLY TO BE RETAINED
- EXISTING GULLY TO BE REMOVED
- EXISTING GULLY CONNECTION TO BE REMOVED
- EXISTING DRAINAGE CONNECTION CHAMBER TO BE RETAINED
- PROPOSED DRAINAGE KERB
- PROPOSED DRAINAGE CHANNEL
- EXISTING DRAINAGE CHANNEL TO BE RETAINED
- EXISTING DRAINAGE CHANNEL TO BE REMOVED
- EXISTING DRAINAGE MANHOLE CHAMBER
- EXISTING GULLY TO BE ADJUSTED
- EXISTING DRAINAGE CONNECTION CHAMBER TO BE ADJUSTED

SAFETY HEALTH & ENVIRONMENTAL INFORMATION BOX

HEALTH AND SAFETY SYMBOLS LEGEND

	INDICATES A RESIDUAL RISK REQUIRING A COMPULSORY ACTION		INDICATES A RESIDUAL RISK FOR INFORMATION
	INDICATES A RESIDUAL RISK REQUIRING A PROHIBITIVE ACTION		INDICATES A RESIDUAL RISK AS A WARNING
	INDICATES AN ENVIRONMENTAL HAZARD		

This table is provided to assist the Principal Contractor to fulfill their obligations under the Construction Design & Management Regulations, 2015. It does not include residual risks that a competent Contractor will be aware of nor does it absolve the Principal Contractor of their legal responsibilities.

The reference number corresponds to the Design Risk Management Schedule.

UNTIL TECHNICAL APPROVAL HAS BEEN OBTAINED FROM THE RELEVANT LOCAL AUTHORITIES OR STATUTORY BODIES, IT SHOULD BE UNDERSTOOD THAT ALL DRAWINGS ARE ISSUED AS PRELIMINARY AND NOT FOR CONSTRUCTION. SHOULD THE CONTRACTOR AND / OR EMPLOYER COMMENCE WORK PRIOR TO APPROVAL BEING GIVEN, IT IS ENTIRELY AT THEIR OWN RISK

REV	DATE	BY	DESCRIPTION	CHK	APP
PD4	10/10/24	AB	UPDATED FOLLOWING CLIENT REQUIREMENTS	JS	PWW
PD3	03/09/24	NR	UPDATED FOLLOWING CLIENT REVIEW	JS	PWW
PD2	31/07/24	NR	ISSUED FOR CLIENT APPROVAL	JS	PWW
PD1	23/07/24	NR	ISSUE FOR COST ESTIMATE	JS	PWW
DR	16/07/24	NR	DRAFT ISSUE	JS	PWW

wsp

Amber Court, William Armstrong Drive, Newcastle upon Tyne, NE4 7YQ, UK
 T+ 44 (0) 191 226 2000, F+ 44 (0) 191 226 2104
 wsp.com

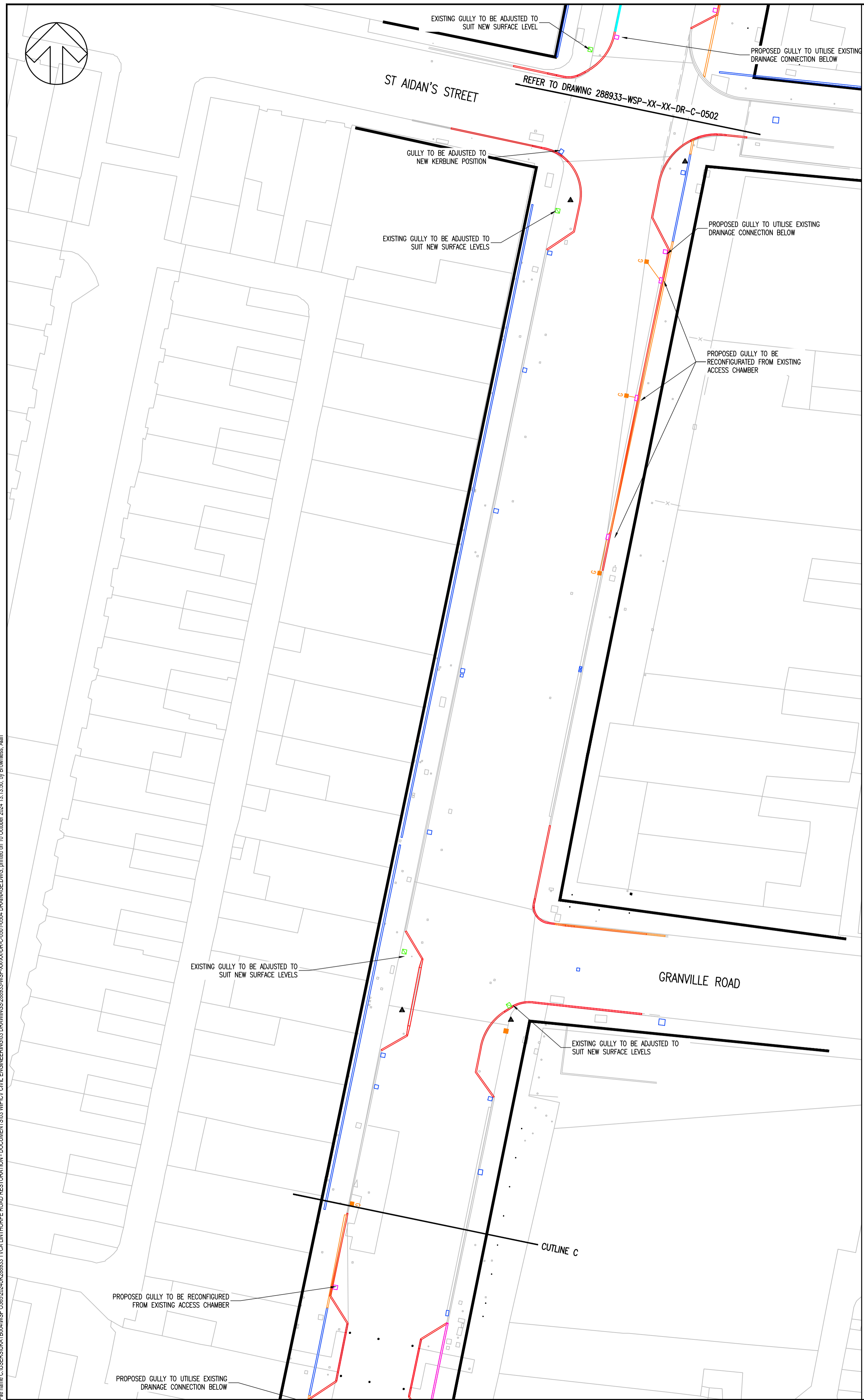
TEES VALLEY

SITE/PROJECT:
LINTHORPE ROAD RESTORATION

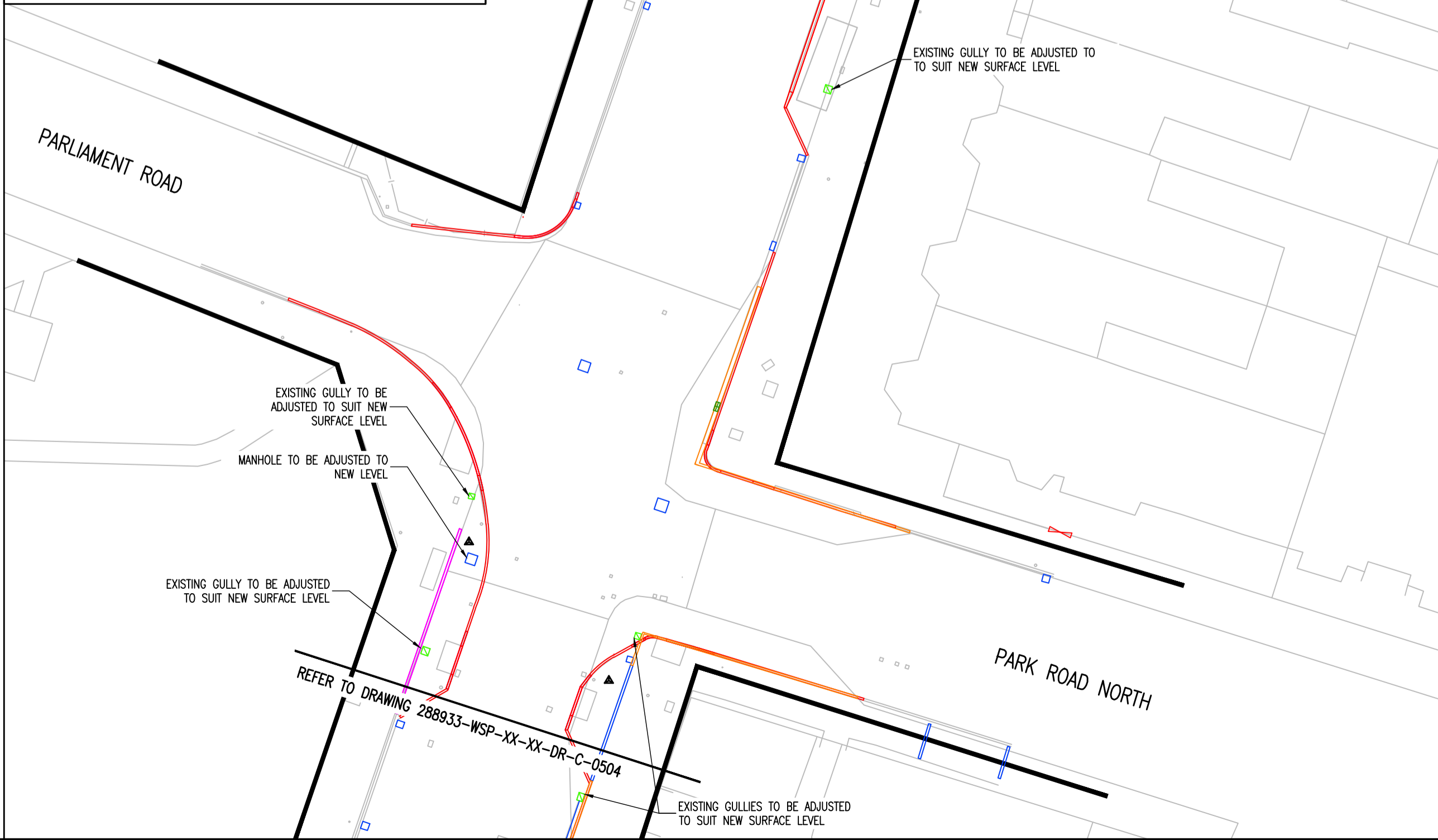
TITLE:
BELOW GROUND DRAINAGE LAYOUT (SHEET 2 OF 4)

SCALE @ A1: 1:250	CHECKED: JS	APPROVED: PWW
PROJECT NO: 2024UK293315	DESIGNED: NR	DRAWN: NR
DATE: JULY 2024	REV: P04	

File name: C:\USERS\WATBOW\WSP_0360\2024\288933\TICALINTHORPEROADRESTORATION_DOCUMENTS\DWG\ENGINEERING\DWG\288933-WSP-XX-XX-DR-C-0501-DRAINAGE.DWG, created on: 10 October 2024 13:02:27 by: Rowan.Bass, Adb



- NOTES**
- THIS DRAWING SHOULD BE READ IN CONJUNCTION WITH ALL RELEVANT DOCUMENTATION DRAWINGS AND STANDARD DETAILS BUT WITH PARTICULAR REFERENCE TO THE FOLLOWING
 -288933-WSP-XX-XX-DR-C-0101-0104
 -288933-WSP-XX-XX-DR-C-1101-1104
 -288933-WSP-XX-XX-DR-C-0511
 -288933-WSP-XX-XX-DR-C-0201-0204
 - ALL DIMENSIONS ARE IN METRES UNLESS NOTED OTHERWISE.
 - THE ORIGINAL OF THIS DRAWING WAS PRODUCED IN COLOUR - A MONOCHROME COPY CANNOT BE RELIED ON.
 - INFORMATION REGARDING THE LOCATION AND DEPTH OF EXISTING SERVICES CANNOT BE GUARANTEED BY THE STATUTORY UNDERTAKER AND SHOULD BE INVESTIGATED. THE CONTRACTOR SHALL ESTABLISH THE PRECISE LINE AND DEPTH OF ALL EXISTING UTILITIES PRIOR TO COMMENCING ANY EXCAVATION WORK.
 - THE CONTRACTOR SHALL CONDUCT THE WORKS WITH DUE REGARD TO ANY ECOLOGICAL AND ENVIRONMENTAL REQUIREMENTS OF THE SCHEME. THE CONTRACTOR SHALL TAKE SUCH STEPS TO SAFEGUARD AGAINST CONTAMINATION OF LOCAL WATERCOURSES.
 - UPON COMPLETION OF THE WORKS, THE CONTRACTOR SHALL ENSURE THAT ALL DRAINAGE APPARATUS IS JETTED AND FREE FROM OBSTRUCTIONS AND SUBSEQUENT SUBJECT TO CCTV SURVEY AND TESTING IN ACCORDANCE WITH THE DRAINAGE SPECIFICATION.
 - GRATINGS OF GULLIES LOCATED IN THE CYCLEWAY ARE TO BE CYCLE FRIENDLY. EXISTING GULLY GRATINGS ARE TO BE ASSESSED FOR THE SUITABILITY AND REPLACED WHERE APPROPRIATE.
 - NEW DRAINAGE CHANNELS ARE TO BE ACCORDING TO NEW DRAIN UNIT REFERENCE PD100C 25.1 (SUBJECT TO CONFIRMATION OF CONNECTING CHANNEL SIZES). FOR CONSTRUCTION DETAILS REFER TO WSP DRAWING 288933-WSP-XX-XX-DR-C-0511.
 - WHERE EXISTING GULLY OUTLETS TO BE KEPT LIVE AND GRATING TO BE REPLACED WITH ACCESS COVER, THEY SHOULD BE ADJUSTED TO FINISH LEVEL AND MOVED IF FEASIBLE TO AVOID CLASH WITH KERBLINE.
 - WHERE GULLIES ARE TO BE REPLACED WITH SHALLOW INSPECTION CHAMBERS, THEY ARE TO BE LOCATED OUTSIDE THE CARRIAGEWAY WHERE POSSIBLE AND THE EXISTING GULLY CONNECTION IS TO BE RE-USED.
 - WHERE SHALLOW GULLIES ARE REQUIRED DUE TO EXISTING SHALLOW CONNECTION DEPTHS OR UTILITY CONSTRAINTS, WREKIN GULLY CHUTE CONNECTOR (OR SIMILAR APPROVED) IS TO BE USED.
 - THE DESIGN IS BASED ON TOPOGRAPHICAL SURVEY REFERENCE ASS 2963 BY ADH SITE SURVEYS LIMITED RECEIVED FEBRUARY 2021.
 - THIS DRAWING IS BASED IN A WHOLE OR IN PART ON INFORMATION AND/OR DATA APPROVED BY A THIRD PARTY INCLUDING MIDDLESBROUGH COUNCIL AND AS SUCH IN CONTINGENT UPON THE ACCURACY AND COMPLETENESS OF THAT INFORMATION AND/OR DATA. WSP UK LIMITED SHALL NOT BE LIABLE IN RELATION TO ANY INACCURACY PRESENTED WITHIN THE DRAWING AND/OR IN RELATION TO ANY DISCREPANCY OR AMBIGUITY WITH ANY OTHER PROJECT DOCUMENTS. SHOULD ANY DATA AND/OR INFORMATION UPON WHICH RELIANCE HAS BEEN BASED BE INCORRECT OR OUT OF DATE OR HAVE BEEN CONCEALED, WITHHELD OR BE INCOMPLETE, MISREPRESENTED OR OTHERWISE NOT FULLY DISCLOSED TO WSP UK LIMITED.
 - REFERENCE SHOULD BE MADE TO INDIVIDUAL DISCIPLINE DRAWINGS AND THE DESIGN RISK MANAGEMENT SCHEDULE FOR FURTHER HEALTH AND SAFETY INFORMATION - NOT ALL RISKS ARE IDENTIFIED WITHIN THIS DRAWING.
 - ALL PROPOSED BELOW GROUND DRAINAGE PIPELINES/GULLY CONNECTIONS TO BE 150MM DIAMETER UNLESS NOTED OTHERWISE.
 - TEMPORARY WORKS DESIGN ASSOCIATED WITH THE CONSTRUCTION OF THE WORKS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
 - ALL GULLIES IN FOOTWAYS ARE TO INCLUDE PEDESTRIAN FRIENDLY GULLY GRATINGS TO REDUCE TRIP HAZARDS



DO NOT SCALE

KEY

- HIGHWAY BOUNDARY (REFER TO NOTE 13)
- PROPOSED KERB
- PROPOSED GULLY
- PROPOSED GULLY CONNECTION
- EXISTING GULLY TO BE RETAINED
- EXISTING GULLY TO BE REMOVED
- EXISTING GULLY CONNECTION TO BE REMOVED
- EXISTING DRAINAGE CONNECTION CHAMBER TO BE RETAINED
- PROPOSED DRAINAGE KERB
- PROPOSED DRAINAGE CHANNEL
- EXISTING DRAINAGE CHANNEL TO BE RETAINED
- EXISTING DRAINAGE CHANNEL TO BE REMOVED
- EXISTING DRAINAGE MANHOLE CHAMBER
- EXISTING GULLY TO BE ADJUSTED
- EXISTING DRAINAGE CONNECTION CHAMBER TO BE ADJUSTED

SAFETY HEALTH & ENVIRONMENTAL INFORMATION BOX

HEALTH AND SAFETY SYMBOLS LEGEND

	INDICATES A RESIDUAL RISK REQUIRING A COMPULSORY ACTION		INDICATES A RESIDUAL RISK FOR INFORMATION
	INDICATES A RESIDUAL RISK REQUIRING A PROHIBITIVE ACTION		INDICATES A RESIDUAL RISK AS A WARNING
	INDICATES AN ENVIRONMENTAL HAZARD		

This table is provided to assist the Principal Contractor to fulfil their obligations under the Construction Design & Management Regulations, 2015. It does not include residual risks that a competent Contractor will be aware of nor does it absolve the Principal Contractor of their legal responsibilities.
 The reference number corresponds to the Design Risk Management Schedule.

UNTIL TECHNICAL APPROVAL HAS BEEN OBTAINED FROM THE RELEVANT LOCAL AUTHORITIES OR STATUTORY BODIES, IT SHOULD BE UNDERSTOOD THAT ALL DRAWINGS ARE ISSUED AS PRELIMINARY AND NOT FOR CONSTRUCTION. SHOULD THE CONTRACTOR AND / OR EMPLOYER COMMENCE WORK PRIOR TO APPROVAL BEING GIVEN, IT IS ENTIRELY AT THEIR OWN RISK

REV	DATE	BY	DESCRIPTION	CHK	APP
P04	10/10/24	AB	UPDATED FOLLOWING CLIENT REQUIREMENTS	JS	PWW
P03	03/09/24	NR	UPDATED FOLLOWING CLIENT REVIEW	JS	PWW
P02	31/07/24	NR	ISSUED FOR CLIENT APPROVAL	JS	PWW
P01	23/07/24	NR	ISSUE FOR COST ESTIMATE	JS	PWW
DR	16/07/24	NR	DRAFT ISSUE	JS	PWW

DRAWING STATUS:

wsp

Amber Court, William Armstrong Drive, Newcastle upon Tyne, NE4 7YQ, UK
 T+ 44 (0) 191 226 2000, F+ 44 (0) 191 226 2104
 wsp.com

CLIENT:

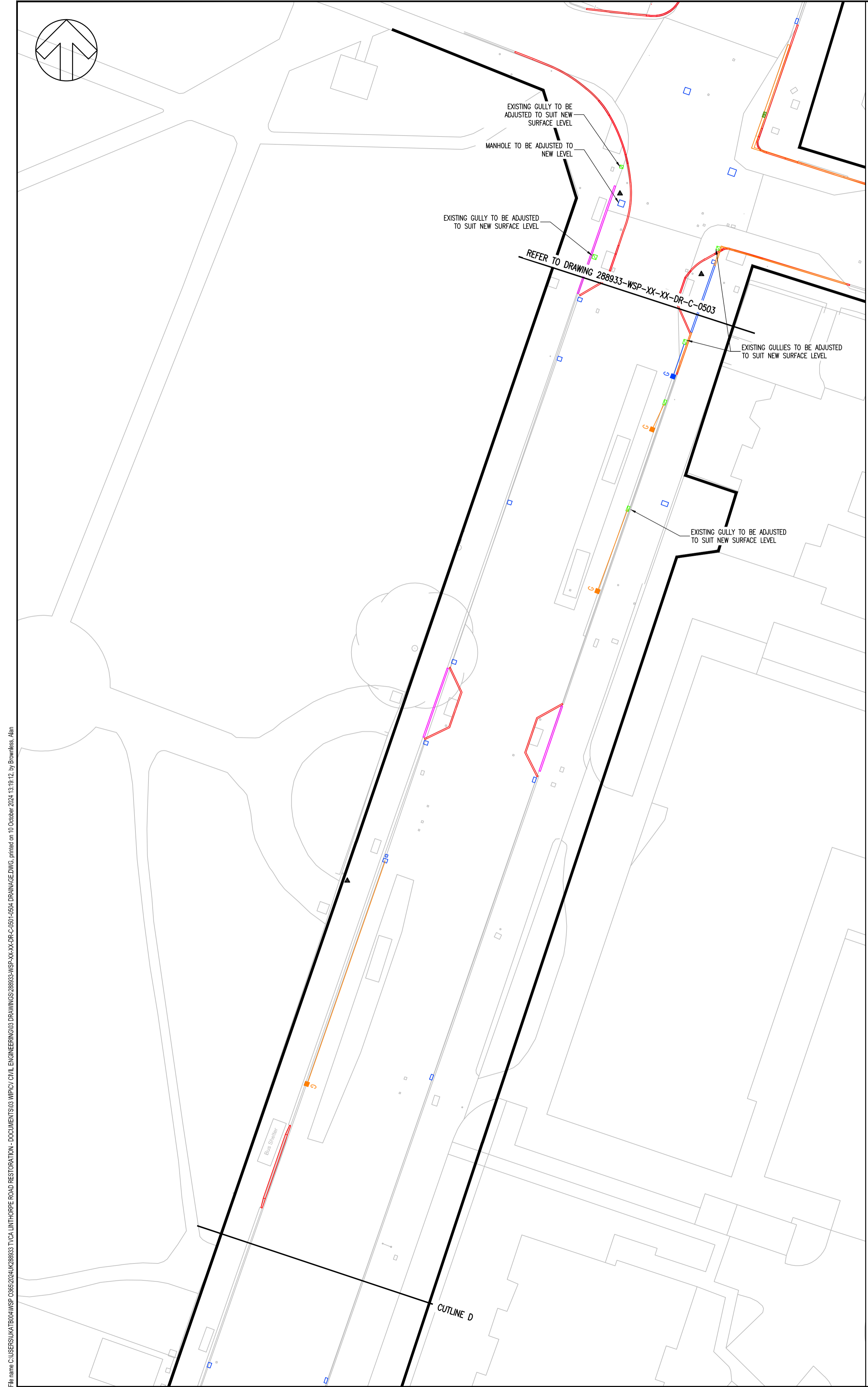
TEES VALLEY

SITE/PROJECT: LINTHORPE ROAD RESTORATION

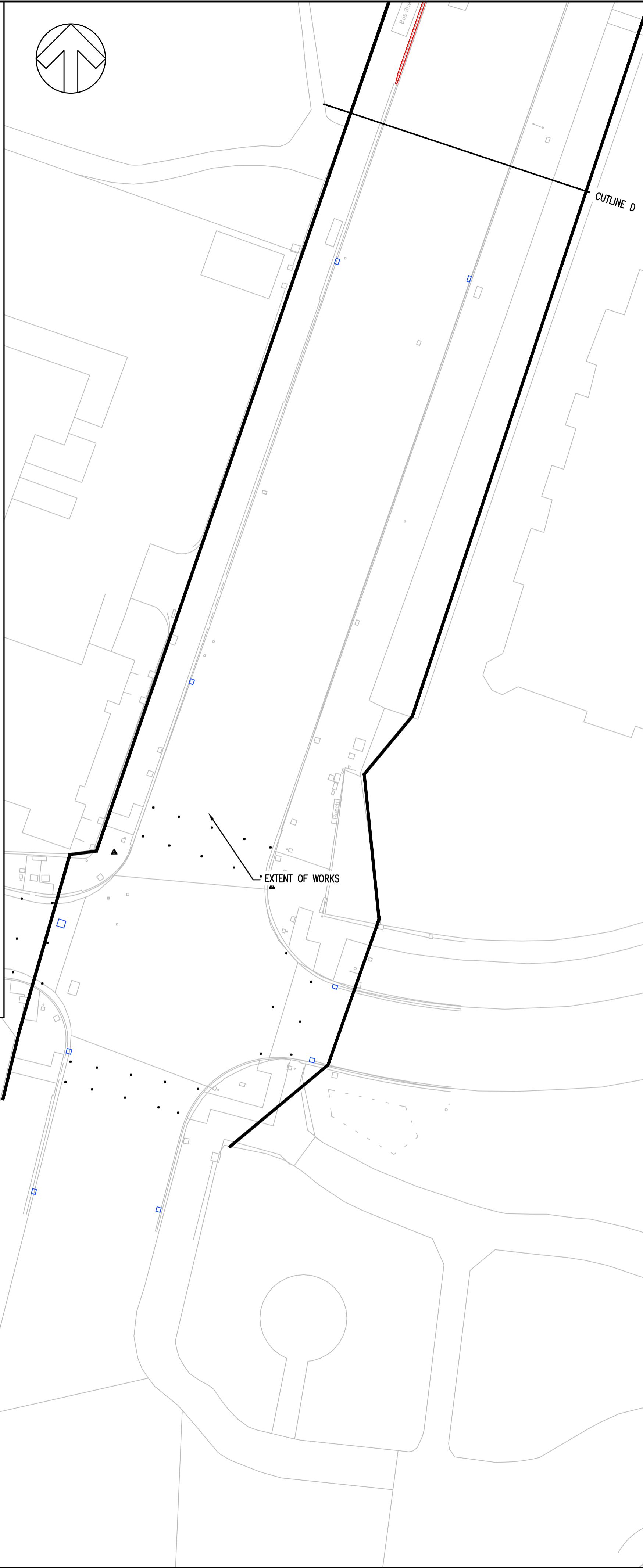
TITLE: BELOW GROUND DRAINAGE LAYOUT (SHEET 3 OF 4)

SCALE @ A1:	1:250	CHECKED:	JS	APPROVED:	PWW
PROJECT NO:	2024UK293315	DESIGNED:	NR	DATE:	JULY 2024
DRAWING NO:	288933-WSP-XX-XX-DR-C-0503	REV:	P04		

File name: C:\Users\WATBOW\OneDrive\Documents\2024\288933\TVC\LINTHORPE ROAD RESTORATION - DOCUMENTS\03 MP\03 CIVIL ENGINEERING\03 DRAWINGS\288933-WSP-XX-XX-DR-C-0503-DRAINAGE.DWG, printed on 10 October 2024 13:15:50, by Rowland, Alan



- NOTES**
- THIS DRAWING SHOULD BE READ IN CONJUNCTION WITH ALL RELEVANT DOCUMENTATION DRAWINGS AND STANDARD DETAILS BUT WITH PARTICULAR REFERENCE TO THE FOLLOWING
 -288933-WSP-XX-XX-DR-C-0101-0104
 -288933-WSP-XX-XX-DR-C-1101-1104
 -288933-WSP-XX-XX-DR-C-0511
 -288933-WSP-XX-XX-DR-C-0201-0204
 - ALL DIMENSIONS ARE IN METRES UNLESS NOTED OTHERWISE.
 - THE ORIGINAL OF THIS DRAWING WAS PRODUCED IN COLOUR - A MONOCHROME COPY CANNOT BE RELIED ON.
 - INFORMATION REGARDING THE LOCATION AND DEPTH OF EXISTING SERVICES CANNOT BE GUARANTEED BY THE STATUTORY UNDERTAKER AND SHOULD BE INVESTIGATED. THE CONTRACTOR SHALL ESTABLISH THE PRECISE LINE AND DEPTH OF ALL EXISTING UTILITIES PRIOR TO COMMENCING ANY EXCAVATION WORK.
 - THE CONTRACTOR SHALL CONDUCT THE WORKS WITH DUE REGARDS TO ANY ECOLOGICAL AND ENVIRONMENTAL REQUIREMENTS OF THE SCHEME. THE CONTRACTOR SHALL TAKE SUCH STEPS TO SAFEGUARD AGAINST CONTAMINATION OF LOCAL WATERCOURSES.
 - UPON COMPLETION OF THE WORKS, THE CONTRACTOR SHALL ENSURE THAT ALL DRAINAGE APPARATUS IS JETTED AND FREE FROM OBSTRUCTIONS AND SUBSEQUENT SUBJECT TO CCTV SURVEY AND TESTING IN ACCORDANCE WITH THE DRAINAGE SPECIFICATION.
 - GRATINGS OF GULLIES LOCATED IN THE CYCLEWAY ARE TO BE CYCLE FRIENDLY. EXISTING GULLY GRATINGS ARE TO BE ASSESSED FOR THE SUITABILITY AND REPLACED WHERE APPROPRIATE
 - NEW DRAINAGE CHANNELS ARE TO BE ACO PARK DRAIN UNIT REFERENCE PD100C 25.1 (SUBJECT TO CONFIRMATION OF CONNECTING CHANNEL SIZES). FOR CONSTRUCTION DETAILS REFER TO WSP DRAWING 288933-WSP-XX-XX-DR-C-0511.
 - WHERE EXISTING GULLY OUTLETS TO BE KEPT LIVE AND GRATING TO BE REPLACED WITH ACCESS COVER, THEY SHOULD BE ADJUSTED TO FINISH LEVEL AND MOVED IF FEASIBLE TO AVOID CLASH WITH KERBLINE.
 - WHERE GULLIES ARE TO BE REPLACED WITH SHALLOW INSPECTION CHAMBERS, THEY ARE TO BE LOCATED OUTSIDE THE CARRIAGEWAY WHERE POSSIBLE AND THE EXISTING GULLY CONNECTION IS TO BE RE-USED.
 - WHERE SHALLOW GULLIES ARE REQUIRED DUE TO EXISTING SHALLOW CONNECTION DEPTHS OR UTILITY CONSTRAINTS, WREKIN GULLY CHUTE CONNECTOR (OR SIMILAR APPROVED) IS TO BE USED.
 - THE DESIGN IS BASED ON TOPOGRAPHICAL SURVEY REFERENCE ASS 2963 BY ADH SITE SURVEYS LIMITED RECEIVED FEBRUARY 2021.
 - THIS DRAWING IS BASED IN A WHOLE OR IN PART ON INFORMATION AND/OR DATA APPROVED BY A THIRD PARTY INCLUDING MIDDLESBROUGH COUNCIL AND AS SUCH IN CONTINGENT UPON THE ACCURACY AND COMPLETENESS OF THAT INFORMATION AND/OR DATA. WSP UK LIMITED SHALL NOT BE LIABLE IN RELATION TO ANY INACCURACY PRESENTED WITHIN THE DRAWING AND/OR IN RELATION TO ANY DISCREPANCY OR AMBIGUITY WITH ANY OTHER PROJECT DOCUMENTS SHOULD ANY DATA AND/OR INFORMATION UPON WHICH RELIANCE HAS BEEN BASED BE INCORRECT OR OUT OF DATE OR HAVE BEEN CONCEALED, WITHHELD OR BE INCOMPLETE, MISREPRESENTED OR OTHERWISE NOT FULLY DISCLOSED TO WSP UK LIMITED.
 - REFERENCE SHOULD BE MADE TO INDIVIDUAL DISCIPLINE DRAWINGS AND THE DESIGN RISK MANAGEMENT SCHEDULE FOR FURTHER HEALTH AND SAFETY INFORMATION - NOT ALL RISKS ARE IDENTIFIED WITHIN THIS DRAWING.
 - ALL PROPOSED BELOW GROUND DRAINAGE PIPELINES/GULLY CONNECTIONS TO BE 150MM DIAMETER UNLESS NOTED OTHERWISE.
 - TEMPORARY WORKS DEIGN ASSOCIATED WITH THE CONSTRUCTION OF THE WORKS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
 - ALL GULLIES IN FOOTWAYS ARE TO INCLUDE PEDESTRIAN FRIENDLY GULLY GRATINGS TO REDUCE TRIP HAZARDS



DO NOT SCALE

KEY

- HIGHWAY BOUNDARY (REFER TO NOTE 13)
- PROPOSED KERB
- PROPOSED GULLY
- PROPOSED GULLY CONNECTION
- EXISTING GULLY TO BE RETAINED
- EXISTING GULLY TO BE REMOVED
- EXISTING GULLY CONNECTION TO BE REMOVED
- EXISTING DRAINAGE CONNECTION CHAMBER TO BE RETAINED
- PROPOSED DRAINAGE KERB
- PROPOSED DRAINAGE CHANNEL
- EXISTING DRAINAGE CHANNEL TO BE RETAINED
- EXISTING DRAINAGE CHANNEL TO BE REMOVED
- EXISTING DRAINAGE MANHOLE CHAMBER
- EXISTING GULLY TO BE ADJUSTED
- EXISTING DRAINAGE CONNECTION CHAMBER TO BE ADJUSTED

SAFETY HEALTH & ENVIRONMENTAL INFORMATION BOX

HEALTH AND SAFETY SYMBOLS LEGEND

	XXX INDICATES A RESIDUAL RISK REQUIRING A COMPULSORY ACTION		XXX INDICATES A RESIDUAL RISK FOR INFORMATION
	XXX INDICATES A RESIDUAL RISK REQUIRING A PROHIBITIVE ACTION		XXX INDICATES A RESIDUAL RISK AS A WARNING
	XXX INDICATES AN ENVIRONMENTAL HAZARD		

This table is provided to assist the Principal Contractor to fulfill their obligations under the Construction Design & Management Regulations, 2015. It does not include residual risks that a competent Contractor will be aware of nor does it absolve the Principal Contractor of their legal responsibilities.
The reference number corresponds to the Design Risk Management Schedule.

UNTIL TECHNICAL APPROVAL HAS BEEN OBTAINED FROM THE RELEVANT LOCAL AUTHORITIES OR STATUTORY BODIES, IT SHOULD BE UNDERSTOOD THAT ALL DRAWINGS ARE ISSUED AS PRELIMINARY AND NOT FOR CONSTRUCTION. SHOULD THE CONTRACTOR AND / OR EMPLOYER COMMENCE WORK PRIOR TO APPROVAL BEING GIVEN, IT IS ENTIRELY AT THEIR OWN RISK

REV	DATE	BY	DESCRIPTION	CHK	APP
P04	10/10/24	AB	UPDATED FOLLOWING CLIENT REQUIREMENTS	JS	PWW
P03	03/09/24	NR	UPDATED FOLLOWING CLIENT REVIEW	JS	PWW
P02	31/07/24	NR	ISSUED FOR CLIENT APPROVAL	JS	PWW
P01	23/07/24	NR	ISSUE FOR COST ESTIMATE	JS	PWW
DR	16/07/24	NR	DRAFT ISSUE	JS	PWW

DRAWING STATUS:

Amber Court, William Armstrong Drive, Newcastle upon Tyne, NE4 7YQ, UK
 T+ 44 (0) 191 226 2000, F+ 44 (0) 191 226 2104
 wsp.com

CLIENT:

SITE/PROJECT:

LINTHORPE ROAD RESTORATION

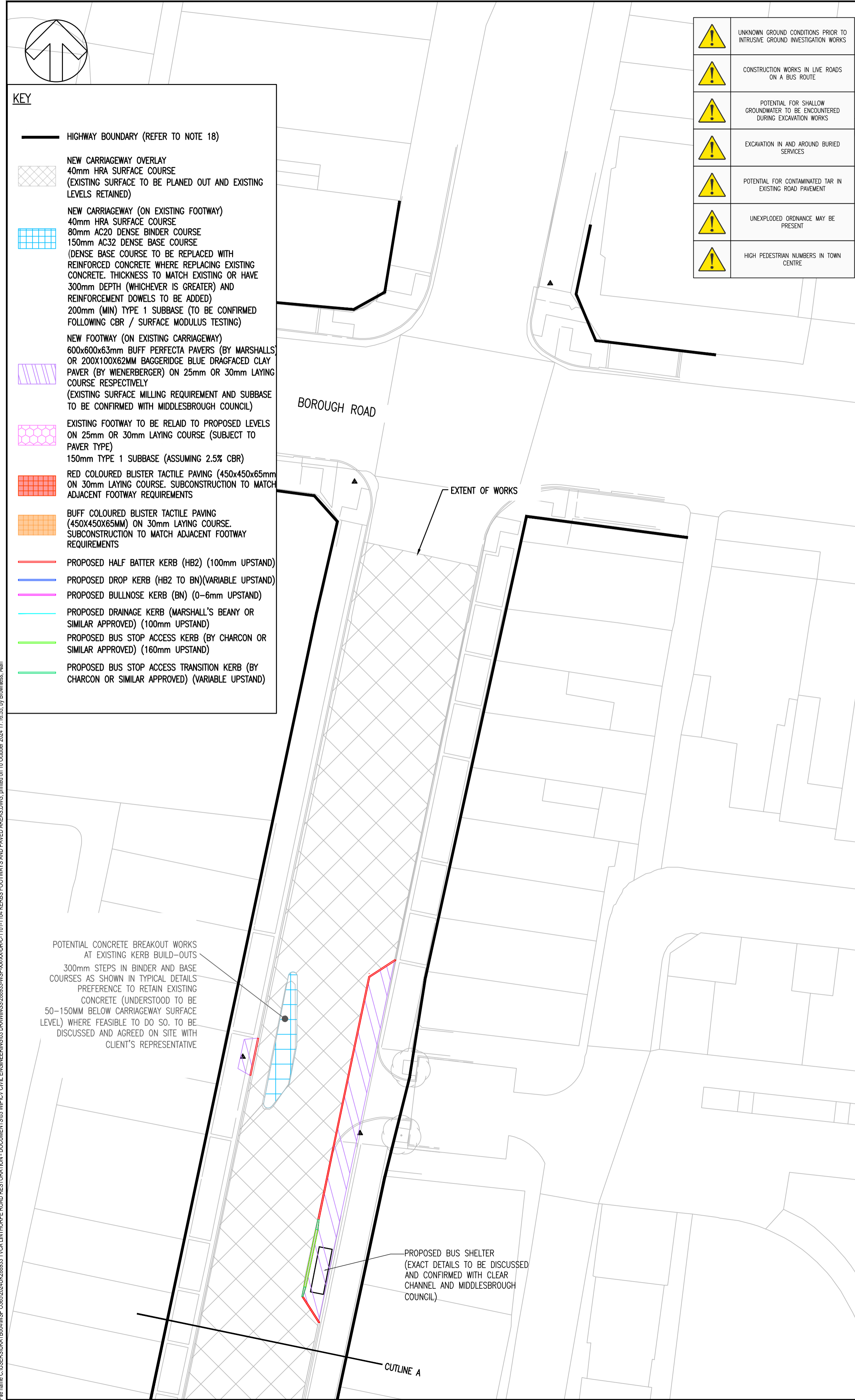
TITLE:

**BELOW GROUND DRAINAGE LAYOUT
(SHEET 4 OF 4)**

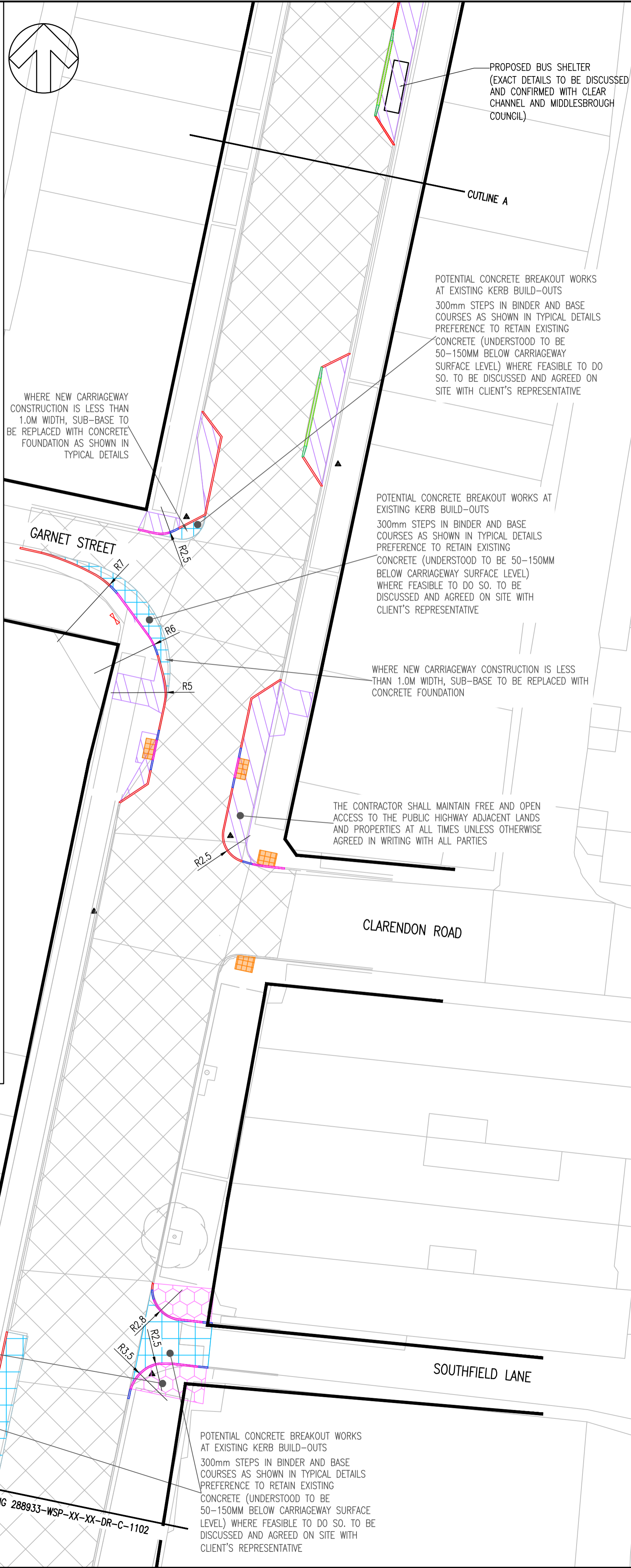
<small>SCALE @ A1:</small> 1:250	<small>CHECKED:</small> JS	<small>APPROVED:</small> PWW
<small>PROJECT NO:</small> 2024UK293315	<small>DESIGNED:</small> NR	<small>DRAWN:</small> NR
<small>DATE:</small> JULY 2024		

DRAWING NO: 288933-WSP-XX-XX-DR-C-0504 REV: P04

File name: C:\USERS\WATBOW\SP_03602024\288933-TVC\LINTHORPE ROAD RESTORATION - DOCUMENTS\00_MPC\01_CIVIL ENGINEERING\03_DRAWINGS\288933-WSP-XX-XX-DR-C-0504-DRAINAGE.DWG, printed on: 10 October 2024 13:19:12, by: Rowland.Aden



- NOTES**
- THIS DRAWING SHOULD BE READ IN CONJUNCTION WITH ALL RELEVANT DOCUMENTATION DRAWINGS AND STANDARD DETAILS BUT WITH PARTICULAR REFERENCE TO THE FOLLOWING
-288933-WSP-XX-XX-DR-C-0101-0104
-288933-WSP-XX-XX-DR-C-1101-1104
-288933-WSP-XX-XX-DR-C-0201-0204
-288933-WSP-XX-XX-DR-C-0111
 - ALL DIMENSIONS ARE IN METRES UNLESS NOTED OTHERWISE.
 - THE ORIGINAL OF THIS DRAWING WAS PRODUCED IN COLOUR - A MONOCHROME COPY CANNOT BE RELIED ON.
 - INFORMATION REGARDING THE LOCATION AND DEPTH OF EXISTING SERVICES CANNOT BE GUARANTEED BY THE STATUTORY UNDERTAKER AND SHOULD BE INVESTIGATED. THE CONTRACTOR SHALL ESTABLISH THE PRECISE LINE AND DEPTH OF ALL EXISTING UTILITIES PRIOR TO COMMENCING ANY EXCAVATION WORK.
 - ALL PROPOSED KERBS ARE TO TIE INTO EXISTING KERB ALIGNMENTS.
 - ALL PROPOSED PAVING LEVELS ARE TO TIE INTO EXISTING PAVING LEVELS AND BE FREE FROM PROTRUDING STEPS FROM THE FINISHED LEVEL.
 - STREET FURNITURE, KERBS, GULLIES, ETC. SHALL BE COVERED WITH A SUITABLE MASKING MATERIAL TO PREVENT OVER SPRAY OF BITUMINOUS MATERIALS.
 - IN THE AREAS OF NEW CARRIAGEWAY CONSTRUCTION, WHERE THE WIDTH OF CARRIAGEWAY IS LESS THAN 1.0M, CONCRETE FILL TYPE ST1 / ST2 OR OTHER SIMILAR SPECIFICATION AS APPROVED BY MIDDLESBROUGH COUNCIL TO BE USED AS A REPLACEMENT FOR TYPE 1 SUBBASE.
 - AREAS OF FOOTWAY / HARDSTANDING & GRASSED VERGES TO BE MADE GOOD ONCE NEW KERBING / PAVING HAS BEEN INSTALLED.
 - ALL FOUNDATIONS SHOULD BE PROOF ROLLED PRIOR TO CONSTRUCTION WITH AND SOFT OR LOOSE POCKETS REPLACED WITH SUITABLE GRANULAR FILL MATERIAL.
 - WHERE SUBGRADE SURFACE MODULUS HAS A VALUE OF LESS THAN 30MPa (APPROX.2.5% CBR), IT MUST BE IMPROVED IN ACCORDANCE WITH DMRB CD 225.
 - THE DESIGN IS BASED ON TOPOGRAPHICAL SURVEY REFERENCE ASS 2963 BY ADH SITE SURVEYS LIMITED RECEIVED FEBRUARY 2021.
 - CONFLICTING INFORMATION SHOWN ON THE ENGINEER'S DRAWINGS OR DISCREPANCIES BETWEEN THE INFORMATION GIVEN BY THE ENGINEER AND THAT PROVIDED BY OTHERS MUST BE REFERRED TO THE ENGINEER BEFORE THE WORKS COMMENCE.
 - ALL REPAIRS TO THE EXISTING FOOTWAY RESULTING FROM CONSTRUCTION, NOT INDICATED ON THIS DRAWING, SHALL BE REINSTATED TO MATCH EXISTING.
 - WHERE IT IS NECESSARY TO AMEND BLOCK PAVING / KERB SIZES, IT IS ADVISED TO MEASURE AND CUT MATERIALS OFF-SITE, OR THE USE OF A BLOCK SPLITTER, TO REDUCE EXPOSURE TO DUST AND NOISE POLLUTION.
 - WHERE IT IS NECESSARY TO AMEND BLOCK PAVING / KERB SIZES, IT IS ADVISED TO MEASURE AND CUT MATERIALS OFF-SITE, OR THE USE OF A BLOCK SPLITTER, TO REDUCE EXPOSURE TO DUST AND NOISE POLLUTION.
 - THIS DRAWING SHOULD NOT BE USED FOR SETTING OUT AS THIS INFORMATION IS PROVIDED SEPARATELY.
 - THIS DRAWING IS BASED IN A WHOLE OR IN PART ON INFORMATION AND/OR DATA APPROVED BY A THIRD PARTY INCLUDING MIDDLESBROUGH COUNCIL AND AS SUCH IN CONTINGENT UPON THE ACCURACY AND COMPLETENESS OF THAT INFORMATION AND/OR DATA. WSP UK LIMITED SHALL NOT BE LIABLE IN RELATION TO ANY INACCURACY PRESENTED WITHIN THE DRAWING AND/OR IN RELATION TO ANY DISCREPANCY OR AMBIGUITY WITH ANY OTHER PROJECT DOCUMENTS SHOULD ANY DATA AND OR INFORMATION UPON WHICH RELIANCE HAS BEEN BASED BE INCORRECT OR OUT OF DATE OR HAVE BEEN CONCEALED, WITHHELD OR BE INCOMPLETE, MISREPRESENTED OR OTHERWISE NOT FULLY DISCLOSED TO WSP UK LIMITED.
 - REFERENCE SHOULD BE MADE TO INDIVIDUAL DISCIPLINE DRAWINGS AND THE DESIGN RISK MANAGEMENT SCHEDULE FOR FURTHER HEALTH AND SAFETY INFORMATION - NOT ALL RISKS ARE IDENTIFIED WITHIN THIS DRAWING



DO NOT SCALE

SAFETY HEALTH & ENVIRONMENTAL INFORMATION BOX

HEALTH AND SAFETY SYMBOLS LEGEND

	INDICATES A RESIDUAL RISK REQUIRING A COMPULSORY ACTION		INDICATES A RESIDUAL RISK FOR INFORMATION
	INDICATES A RESIDUAL RISK REQUIRING A PROHIBITIVE ACTION		INDICATES A RESIDUAL RISK AS A WARNING
	INDICATES AN ENVIRONMENTAL HAZARD		

This table is provided to assist the Principal Contractor to fulfil their obligations under the Construction Design & Management Regulations, 2015. It does not include residual risks that a competent Contractor will be aware of nor does it absolve the Principal Contractor of their legal responsibilities.
The reference number corresponds to the Design Risk Management Schedule.

CARRIAGEWAY GRANULAR SUBBASE (TYPE 1) THICKNESS DEPENDENT ON DESIGN SUBGRADE SURFACE MODULUS (DSSM)

DSSM (MPa)	30	40	50	70	100	150
THICKNESS (mm)	420	330	270	230	200	200

UNTIL TECHNICAL APPROVAL HAS BEEN OBTAINED FROM THE RELEVANT LOCAL AUTHORITIES OR STATUTORY BODIES, IT SHOULD BE UNDERSTOOD THAT ALL DRAWINGS ARE ISSUED AS PRELIMINARY AND NOT FOR CONSTRUCTION. SHOULD THE CONTRACTOR AND / OR EMPLOYER COMMENCE WORK PRIOR TO APPROVAL BEING GIVEN, IT IS ENTIRELY AT THEIR OWN RISK

REV	DATE	BY	DESCRIPTION	CHK	APP
P04	10/10/24	AB	UPDATED FOLLOWING CLIENT REQUIREMENTS	JS	PWW
P03	12/09/24	NR	UPDATED FOLLOWING CLIENT REVIEW	JS	PWW
P02	31/07/24	NR	ISSUED FOR CLIENT APPROVAL	JS	PWW
P01	23/07/24	NR	ISSUE FOR COST ESTIMATE	JS	PWW
DR	16/07/24	NR	DRAFT ISSUE	JS	PWW

DRAWING STATUS:

wsp

Amber Court, William Armstrong Drive, Newcastle upon Tyne, NE4 7YQ, UK
T+ 44 (0) 191 226 2000, F+ 44 (0) 191 226 2104
wsp.com

CLIENT:

TEES VALLEY

SITE/PROJECT:

LINTHORPE ROAD RESTORATION

TITLE:

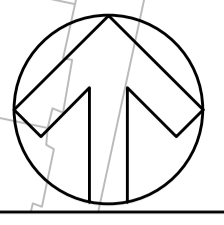
KERBS, FOOTWAYS AND PAVED AREAS (SHEET 1 OF 4)

SCALE @ A1:	CHECKED:	APPROVED:
1:250	JS	PWW

PROJECT NO:	DESIGNED:	DRAWN:	DATE:
2024UK293315	NR	NR	JULY 2024

DRAWING NO:	REV:
288933-WSP-XX-XX-DR-C-1101	P04

File name: C:\USERS\WATB001\WSP_0360\2024\288933\TVC\LINTHORPE ROAD RESTORATION - DOCUMENTS\03 MP\01 CIVIL ENGINEERING\03 DRAWINGS\288933-WSP-XX-XX-DR-C-1101-1101_KERBS FOOTWAYS AND PAVED AREAS.DWG, printed on 10 October 2024 17:18:55, by: Bownless, Alan

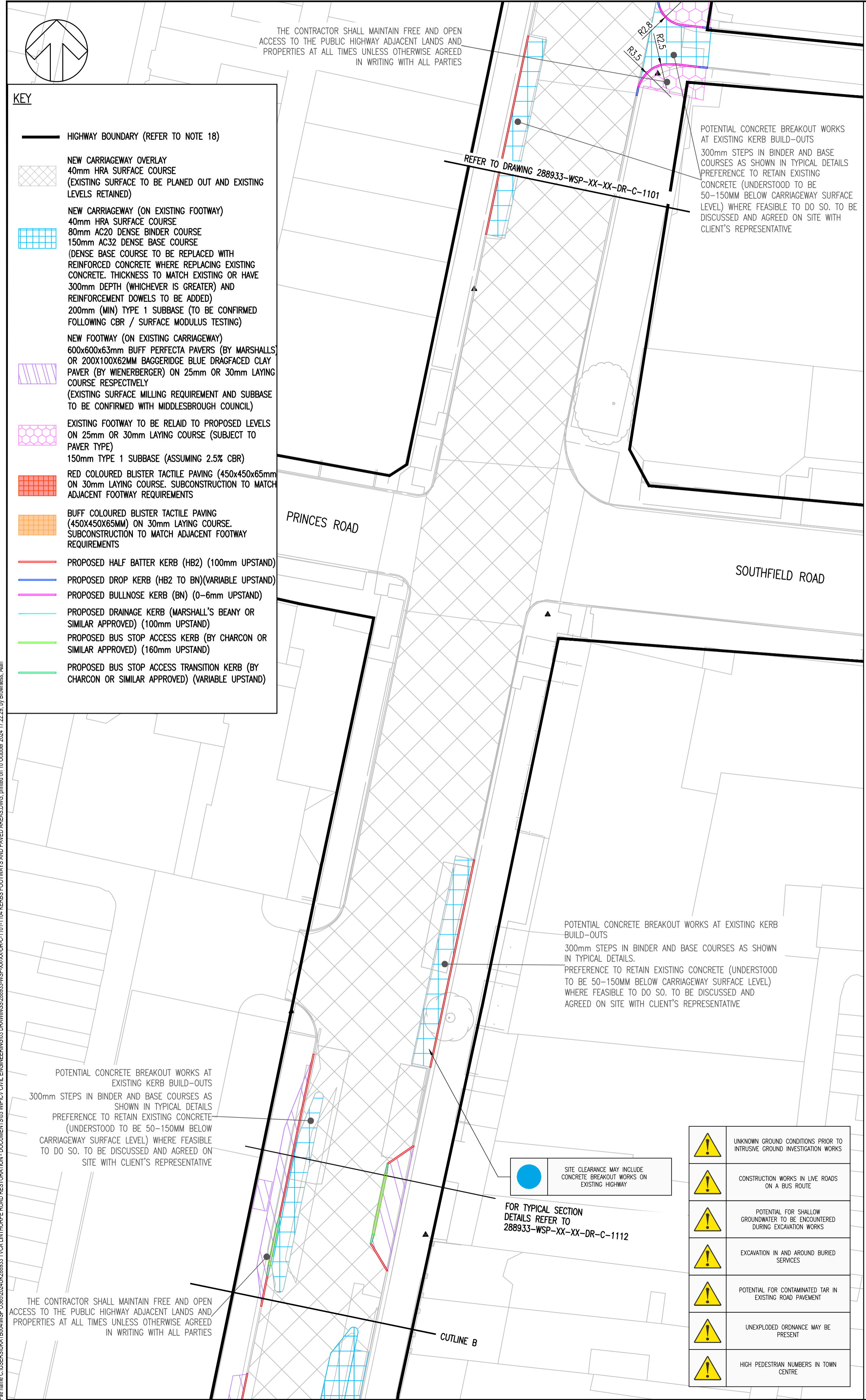


THE CONTRACTOR SHALL MAINTAIN FREE AND OPEN ACCESS TO THE PUBLIC HIGHWAY ADJACENT LANDS AND PROPERTIES AT ALL TIMES UNLESS OTHERWISE AGREED IN WRITING WITH ALL PARTIES

KEY

- HIGHWAY BOUNDARY (REFER TO NOTE 18)
- NEW CARRIAGEWAY OVERLAY
40mm HRA SURFACE COURSE
(EXISTING SURFACE TO BE PLANED OUT AND EXISTING LEVELS RETAINED)
- NEW CARRIAGEWAY (ON EXISTING FOOTWAY)
40mm HRA SURFACE COURSE
80mm AC20 DENSE BINDER COURSE
150mm AC32 DENSE BASE COURSE
(DENSE BASE COURSE TO BE REPLACED WITH REINFORCED CONCRETE WHERE REPLACING EXISTING CONCRETE. THICKNESS TO MATCH EXISTING OR HAVE 300mm DEPTH (WHICHEVER IS GREATER) AND REINFORCEMENT DONELS TO BE ADDED)
200mm (MIN) TYPE 1 SUBBASE (TO BE CONFIRMED FOLLOWING CBR / SURFACE MODULUS TESTING)
- NEW FOOTWAY (ON EXISTING CARRIAGEWAY)
600x600x63mm BUFF PERFECTA PAVERS (BY MARSHALLS OR 200X100X62MM BAGGERIDGE BLUE DRAGFACED CLAY PAVER (BY WIENERBERGER) ON 25mm OR 30mm LAYING COURSE RESPECTIVELY
(EXISTING SURFACE MILLING REQUIREMENT AND SUBBASE TO BE CONFIRMED WITH MIDDLESBROUGH COUNCIL)
- EXISTING FOOTWAY TO BE RELAI TO PROPOSED LEVELS ON 25mm OR 30mm LAYING COURSE (SUBJECT TO PAVEMENT TYPE)
150mm TYPE 1 SUBBASE (ASSUMING 2.5% CBR)
- RED COLOURED BLISTER TACTILE PAVING (450x450x65mm) ON 30mm LAYING COURSE. SUBCONSTRUCTION TO MATCH ADJACENT FOOTWAY REQUIREMENTS
- BUFF COLOURED BLISTER TACTILE PAVING (450x450x65mm) ON 30mm LAYING COURSE. SUBCONSTRUCTION TO MATCH ADJACENT FOOTWAY REQUIREMENTS
- PROPOSED HALF BATTER KERB (HB2) (100mm UPSTAND)
- PROPOSED DROP KERB (HB2 TO BN)(VARIABLE UPSTAND)
- PROPOSED BULLNOSE KERB (BN) (0-6mm UPSTAND)
- PROPOSED DRAINAGE KERB (MARSHALL'S BEANY OR SIMILAR APPROVED) (100mm UPSTAND)
- PROPOSED BUS STOP ACCESS KERB (BY CHARCON OR SIMILAR APPROVED) (160mm UPSTAND)
- PROPOSED BUS STOP ACCESS TRANSITION KERB (BY CHARCON OR SIMILAR APPROVED) (VARIABLE UPSTAND)

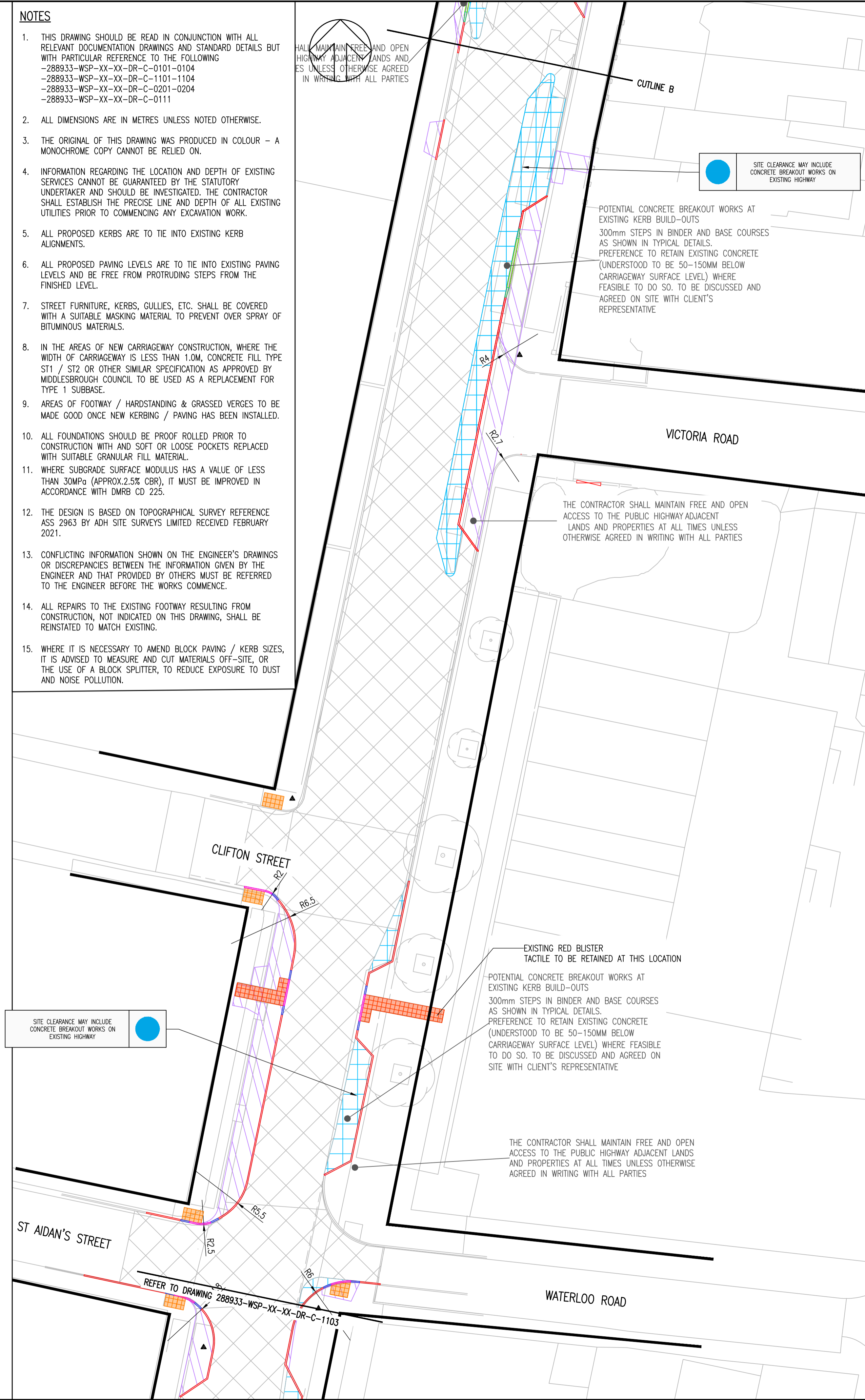
File name: CLISERSWATERBOWNSP_03602024\288933_TVAL LINTHORPE ROAD RESTORATION - DOCUMENTS\DWG\DWG\288933-WSP-XX-XX-DR-C-1103-1104 KERBS FOOTWAYS AND PAVED AREAS.DWG, printed on 10 October 2024 17:22:28, by Bawles, Alan



NOTES

- THIS DRAWING SHOULD BE READ IN CONJUNCTION WITH ALL RELEVANT DOCUMENTATION DRAWINGS AND STANDARD DETAILS BUT WITH PARTICULAR REFERENCE TO THE FOLLOWING
-288933-WSP-XX-XX-DR-C-0101-0104
-288933-WSP-XX-XX-DR-C-1101-1104
-288933-WSP-XX-XX-DR-C-0201-0204
-288933-WSP-XX-XX-DR-C-0111
- ALL DIMENSIONS ARE IN METRES UNLESS NOTED OTHERWISE.
- THE ORIGINAL OF THIS DRAWING WAS PRODUCED IN COLOUR - A MONOCHROME COPY CANNOT BE RELIED ON.
- INFORMATION REGARDING THE LOCATION AND DEPTH OF EXISTING SERVICES CANNOT BE GUARANTEED BY THE STATUTORY UNDERTAKER AND SHOULD BE INVESTIGATED. THE CONTRACTOR SHALL ESTABLISH THE PRECISE LINE AND DEPTH OF ALL EXISTING UTILITIES PRIOR TO COMMENCING ANY EXCAVATION WORK.
- ALL PROPOSED KERBS ARE TO TIE INTO EXISTING KERB ALIGNMENTS.
- ALL PROPOSED PAVING LEVELS ARE TO TIE INTO EXISTING PAVING LEVELS AND BE FREE FROM PROTRUDING STEPS FROM THE FINISHED LEVEL.
- STREET FURNITURE, KERBS, GULLIES, ETC. SHALL BE COVERED WITH A SUITABLE MASKING MATERIAL TO PREVENT OVER SPRAY OF BITUMINOUS MATERIALS.
- IN THE AREAS OF NEW CARRIAGEWAY CONSTRUCTION, WHERE THE WIDTH OF CARRIAGEWAY IS LESS THAN 1.0M, CONCRETE FILL TYPE ST1 / ST2 OR OTHER SIMILAR SPECIFICATION AS APPROVED BY MIDDLESBROUGH COUNCIL TO BE USED AS A REPLACEMENT FOR TYPE 1 SUBBASE.
- AREAS OF FOOTWAY / HARDSTANDING & GRASSED VERGES TO BE MADE GOOD ONCE NEW KERBING / PAVING HAS BEEN INSTALLED.
- ALL FOUNDATIONS SHOULD BE PROOF ROLLED PRIOR TO CONSTRUCTION WITH AND SOFT OR LOOSE POCKETS REPLACED WITH SUITABLE GRANULAR FILL MATERIAL.
- WHERE SUBGRADE SURFACE MODULUS HAS A VALUE OF LESS THAN 30MPa (APPROX.2.5% CBR), IT MUST BE IMPROVED IN ACCORDANCE WITH DMRB CD 225.
- THE DESIGN IS BASED ON TOPOGRAPHICAL SURVEY REFERENCE ASS 2963 BY ADH SITE SURVEYS LIMITED RECEIVED FEBRUARY 2021.
- CONFLICTING INFORMATION SHOWN ON THE ENGINEER'S DRAWINGS OR DISCREPANCIES BETWEEN THE INFORMATION GIVEN BY THE ENGINEER AND THAT PROVIDED BY OTHERS MUST BE REFERRED TO THE ENGINEER BEFORE THE WORKS COMMENCE.
- ALL REPAIRS TO THE EXISTING FOOTWAY RESULTING FROM CONSTRUCTION, NOT INDICATED ON THIS DRAWING, SHALL BE REINSTATED TO MATCH EXISTING.
- WHERE IT IS NECESSARY TO AMEND BLOCK PAVING / KERB SIZES, IT IS ADVISED TO MEASURE AND CUT MATERIALS OFF-SITE, OR THE USE OF A BLOCK SPLITTER, TO REDUCE EXPOSURE TO DUST AND NOISE POLLUTION.

THE CONTRACTOR SHALL MAINTAIN FREE AND OPEN ACCESS TO THE PUBLIC HIGHWAY ADJACENT LANDS AND PROPERTIES AT ALL TIMES UNLESS OTHERWISE AGREED IN WRITING WITH ALL PARTIES



DO NOT SCALE

- WHERE IT IS NECESSARY TO AMEND BLOCK PAVING / KERB SIZES, IT IS ADVISED TO MEASURE AND CUT MATERIALS OFF-SITE, OR THE USE OF A BLOCK SPLITTER, TO REDUCE EXPOSURE TO DUST AND NOISE POLLUTION.
- THIS DRAWING SHOULD NOT BE USED FOR SETTING OUT AS THIS INFORMATION IS PROVIDED SEPARATELY.
- THIS DRAWING IS BASED IN A WHOLE OR IN PART ON INFORMATION AND/OR DATA APPROVED BY A THIRD PARTY INCLUDING MIDDLESBROUGH COUNCIL AND AS SUCH IN CONTINGENT UPON THE ACCURACY AND COMPLETENESS OF THAT INFORMATION AND/OR DATA. WSP UK LIMITED SHALL NOT BE LIABLE IN RELATION TO ANY INACCURACY PRESENTED WITHIN THE DRAWING AND/OR IN RELATION TO ANY DISCREPANCY OR AMBIGUITY WITH ANY OTHER PROJECT DOCUMENTS SHOULD ANY DATA AND OR INFORMATION UPON WHICH RELIANCE HAS BEEN BASED BE INCORRECT OR OUT OF DATE OR HAVE BEEN CONCEALED, WITHHELD OR BE INCOMPLETE, MISREPRESENTED OR OTHERWISE NOT FULLY DISCLOSED TO WSP UK LIMITED.
- REFERENCE SHOULD BE MADE TO INDIVIDUAL DISCIPLINE DRAWINGS AND THE DESIGN RISK MANAGEMENT SCHEDULE FOR FURTHER HEALTH AND SAFETY INFORMATION - NOT ALL RISKS ARE IDENTIFIED WITHIN THIS DRAWING

SAFETY HEALTH & ENVIRONMENTAL INFORMATION BOX

HEALTH AND SAFETY SYMBOLS LEGEND

	INDICATES A RESIDUAL RISK REQUIRING A COMPULSORY ACTION		INDICATES A RESIDUAL RISK FOR INFORMATION
	INDICATES A RESIDUAL RISK REQUIRING A PROHIBITIVE ACTION		INDICATES A RESIDUAL RISK AS A WARNING
	INDICATES AN ENVIRONMENTAL HAZARD		

This table is provided to assist the Principal Contractor to fulfil their obligations under the Construction Design & Management Regulations, 2015. It does not include residual risks that a competent Contractor will be aware of nor does it absolve the Principal Contractor of their legal responsibilities.
The reference number corresponds to the Design Risk Management Schedule.

CARRIAGEWAY GRANULAR SUBBASE (TYPE 1) THICKNESS DEPENDENT ON DESIGN SUBGRADE SURFACE MODULUS (DSSM)

DSSM (MPa)	30	40	50	70	100	150
THICKNESS (mm)	420	330	270	230	200	200

UNTIL TECHNICAL APPROVAL HAS BEEN OBTAINED FROM THE RELEVANT LOCAL AUTHORITIES OR STATUTORY BODIES, IT SHOULD BE UNDERSTOOD THAT ALL DRAWINGS ARE ISSUED AS PRELIMINARY AND NOT FOR CONSTRUCTION. SHOULD THE CONTRACTOR AND / OR EMPLOYER COMMENCE WORK PRIOR TO APPROVAL BEING GIVEN, IT IS ENTIRELY AT THEIR OWN RISK

REV	DATE	BY	DESCRIPTION	CHK	APP
P04	10/10/24	AB	UPDATED FOLLOWING CLIENT REQUIREMENTS	JS	PWW
P03	12/09/24	NR	UPDATED FOLLOWING CLIENT REVIEW	JS	PWW
P02	31/07/24	NR	ISSUED FOR CLIENT APPROVAL	JS	PWW
P01	23/07/24	NR	ISSUE FOR COST ESTIMATE	JS	PWW
DR	16/07/24	NR	DRAFT ISSUE	JS	PWW

DRAWING STATUS:

Amber Court, William Armstrong Drive, Newcastle upon Tyne, NE4 7YQ, UK
T+ 44 (0) 191 226 2000, F+ 44 (0) 191 226 2104
wsp.com

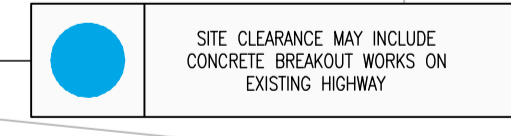
SITE PROJECT: LINTHORPE ROAD RESTORATION

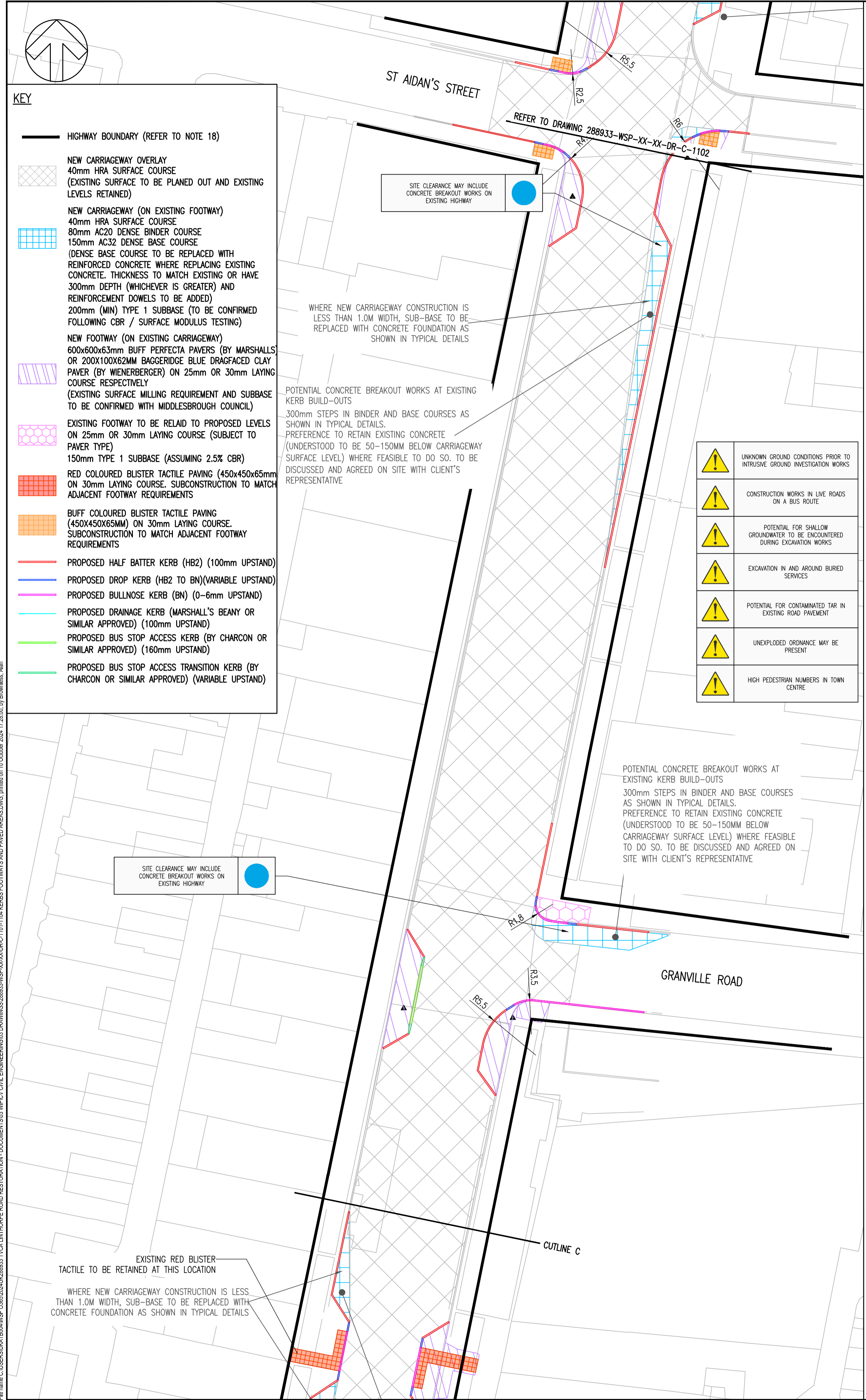
TITLE: KERBS, FOOTWAYS AND PAVED AREAS (SHEET 2 OF 4)

SCALE @ AT:	CHECKED:	APPROVED:	
1:250	JS	PWW	
PROJECT NO:	DESIGNED:	DRAWN:	DATE:
2024UK293315	NR	NR	JULY 2024
DRAWING NO:	REV:		
288933-WSP-XX-XX-DR-C-1102	P04		

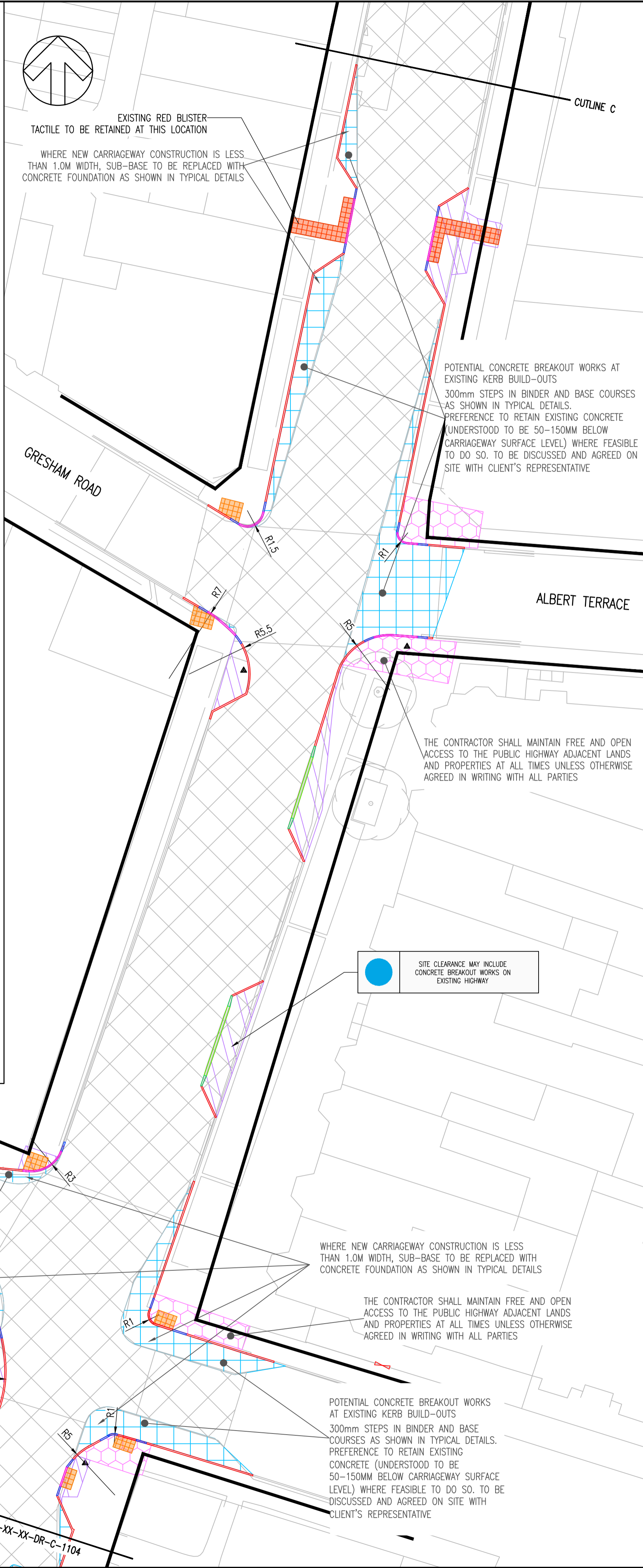
	UNKNOWN GROUND CONDITIONS PRIOR TO INTRUSIVE GROUND INVESTIGATION WORKS
	CONSTRUCTION WORKS IN LIVE ROADS ON A BUS ROUTE
	POTENTIAL FOR SHALLOW GROUNDWATER TO BE ENCOUNTERED DURING EXCAVATION WORKS
	EXCAVATION IN AND AROUND BURIED SERVICES
	POTENTIAL FOR CONTAMINATED TAR IN EXISTING ROAD PAVEMENT
	UNEXPLODED DRENANCE MAY BE PRESENT
	HIGH PEDESTRIAN NUMBERS IN TOWN CENTRE

FOR TYPICAL SECTION DETAILS REFER TO 288933-WSP-XX-XX-DR-C-1112





- NOTES**
- THIS DRAWING SHOULD BE READ IN CONJUNCTION WITH ALL RELEVANT DOCUMENTATION DRAWINGS AND STANDARD DETAILS BUT WITH PARTICULAR REFERENCE TO THE FOLLOWING
 -288933-WSP-XX-XX-DR-C-0101-0104
 -288933-WSP-XX-XX-DR-C-1101-1104
 -288933-WSP-XX-XX-DR-C-0201-0204
 -288933-WSP-XX-XX-DR-C-0111
 - ALL DIMENSIONS ARE IN METRES UNLESS NOTED OTHERWISE.
 - THE ORIGINAL OF THIS DRAWING WAS PRODUCED IN COLOUR - A MONOCHROME COPY CANNOT BE RELIED ON.
 - INFORMATION REGARDING THE LOCATION AND DEPTH OF EXISTING SERVICES CANNOT BE GUARANTEED BY THE STATUTORY UNDERTAKER AND SHOULD BE INVESTIGATED. THE CONTRACTOR SHALL ESTABLISH THE PRECISE LINE AND DEPTH OF ALL EXISTING UTILITIES PRIOR TO COMMENCING ANY EXCAVATION WORK.
 - ALL PROPOSED KERBS ARE TO TIE INTO EXISTING KERB ALIGNMENTS.
 - ALL PROPOSED PAVING LEVELS ARE TO TIE INTO EXISTING PAVING LEVELS AND BE FREE FROM PROTRUDING STEPS FROM THE FINISHED LEVEL.
 - STREET FURNITURE, KERBS, GULLIES, ETC. SHALL BE COVERED WITH A SUITABLE MASKING MATERIAL TO PREVENT OVER SPRAY OF BITUMINOUS MATERIALS.
 - IN THE AREAS OF NEW CARRIAGEWAY CONSTRUCTION, WHERE THE WIDTH OF CARRIAGEWAY IS LESS THAN 1.0M, CONCRETE FILL TYPE ST1 / ST2 OR OTHER SIMILAR SPECIFICATION AS APPROVED BY MIDDLESBROUGH COUNCIL TO BE USED AS A REPLACEMENT FOR TYPE 1 SUBBASE.
 - AREAS OF FOOTWAY / HARDSTANDING & GRASSED VERGES TO BE MADE GOOD ONCE NEW KERBING / PAVING HAS BEEN INSTALLED.
 - ALL FOUNDATIONS SHOULD BE PROOF ROLLED PRIOR TO CONSTRUCTION WITH AND SOFT OR LOOSE POCKETS REPLACED WITH SUITABLE GRANULAR FILL MATERIAL.
 - WHERE SUBGRADE SURFACE MODULUS HAS A VALUE OF LESS THAN 30MPa (APPROX. 2.5% CBR), IT MUST BE IMPROVED IN ACCORDANCE WITH DMRB CD 225.
 - THE DESIGN IS BASED ON TOPOGRAPHICAL SURVEY REFERENCE ASS 2963 BY ADH SITE SURVEYS LIMITED RECEIVED FEBRUARY 2021.
 - CONFLICTING INFORMATION SHOWN ON THE ENGINEER'S DRAWINGS OR DISCREPANCIES BETWEEN THE INFORMATION GIVEN BY THE ENGINEER AND THAT PROVIDED BY OTHERS MUST BE REFERRED TO THE ENGINEER BEFORE THE WORKS COMMENCE.
 - ALL REPAIRS TO THE EXISTING FOOTWAY RESULTING FROM CONSTRUCTION, NOT INDICATED ON THIS DRAWING, SHALL BE REINSTATED TO MATCH EXISTING.
 - WHERE IT IS NECESSARY TO AMEND BLOCK PAVING / KERB SIZES, IT IS ADVISED TO MEASURE AND CUT MATERIALS OFF-SITE, OR THE USE OF A BLOCK SPLITTER, TO REDUCE EXPOSURE TO DUST AND NOISE POLLUTION.
 - WHERE IT IS NECESSARY TO AMEND BLOCK PAVING / KERB SIZES, IT IS ADVISED TO MEASURE AND CUT MATERIALS OFF-SITE, OR THE USE OF A BLOCK SPLITTER, TO REDUCE EXPOSURE TO DUST AND NOISE POLLUTION.
 - THIS DRAWING SHOULD NOT BE USED FOR SETTING OUT AS THIS INFORMATION IS PROVIDED SEPARATELY.
 - THIS DRAWING IS BASED IN A WHOLE OR IN PART ON INFORMATION AND/OR DATA APPROVED BY A THIRD PARTY INCLUDING MIDDLESBROUGH COUNCIL AND AS SUCH IN CONTINGENT UPON THE ACCURACY AND COMPLETENESS OF THAT INFORMATION AND/OR DATA. WSP UK LIMITED SHALL NOT BE LIABLE IN RELATION TO ANY INACCURACY PRESENTED WITHIN THE DRAWING AND/OR IN RELATION TO ANY DISCREPANCY OR AMBIGUITY WITH ANY OTHER PROJECT DOCUMENTS SHOULD ANY DATA AND OR INFORMATION UPON WHICH RELIANCE HAS BEEN BASED BE INCORRECT OR OUT OF DATE OR HAVE BEEN CONCEALED, WITHHELD OR BE INCOMPLETE, MISREPRESENTED OR OTHERWISE NOT FULLY DISCLOSED TO WSP UK LIMITED.
 - REFERENCE SHOULD BE MADE TO INDIVIDUAL DISCIPLINE DRAWINGS AND THE DESIGN RISK MANAGEMENT SCHEDULE FOR FURTHER HEALTH AND SAFETY INFORMATION - NOT ALL RISKS ARE IDENTIFIED WITHIN THIS DRAWING



DO NOT SCALE

SAFETY HEALTH & ENVIRONMENTAL INFORMATION BOX

HEALTH AND SAFETY SYMBOLS LEGEND			
	INDICATES A RESIDUAL RISK REQUIRING A COMPULSORY ACTION		INDICATES A RESIDUAL RISK FOR INFORMATION
	INDICATES A RESIDUAL RISK REQUIRING A PROHIBITIVE ACTION		INDICATES A RESIDUAL RISK AS A WARNING
	INDICATES AN ENVIRONMENTAL HAZARD		

This table is provided to assist the Principal Contractor to fulfil their obligations under the Construction Design & Management Regulations, 2015. It does not include residual risks that a competent Contractor will be aware of nor does it absolve the Principal Contractor of their legal responsibilities.
The reference number corresponds to the Design Risk Management Schedule.

CARRIAGEWAY GRANULAR SUBBASE (TYPE 1) THICKNESS DEPENDENT ON DESIGN SUBGRADE SURFACE MODULUS (DSSM)

DSSM (MPa)	30	40	50	70	100	150
THICKNESS (mm)	420	330	270	230	200	200

UNTIL TECHNICAL APPROVAL HAS BEEN OBTAINED FROM THE RELEVANT LOCAL AUTHORITIES OR STATUTORY BODIES, IT SHOULD BE UNDERSTOOD THAT ALL DRAWINGS ARE ISSUED AS PRELIMINARY AND NOT FOR CONSTRUCTION. SHOULD THE CONTRACTOR AND / OR EMPLOYER COMMENCE WORK PRIOR TO APPROVAL BEING GIVEN, IT IS ENTIRELY AT THEIR OWN RISK

REV	DATE	BY	DESCRIPTION	CHK	APP
P04	10/10/24	AB	UPDATED FOLLOWING CLIENT REQUIREMENTS	JS	PWW
P03	12/09/24	NR	UPDATED FOLLOWING CLIENT REVIEW	JS	PWW
P02	31/07/24	NR	ISSUED FOR CLIENT APPROVAL	JS	PWW
P01	23/07/24	NR	ISSUE FOR COST ESTIMATE	JS	PWW
DR	16/07/24	NR	DRAFT ISSUE	JS	PWW

DRAWING STATUS:

Amber Court, William Armstrong Drive, Newcastle upon Tyne, NE4 7YQ, UK
T+ 44 (0) 191 226 2000, F+ 44 (0) 191 226 2104
wsp.com

CLIENT:

SITE/PROJECT: **LINTHORPE ROAD RESTORATION**

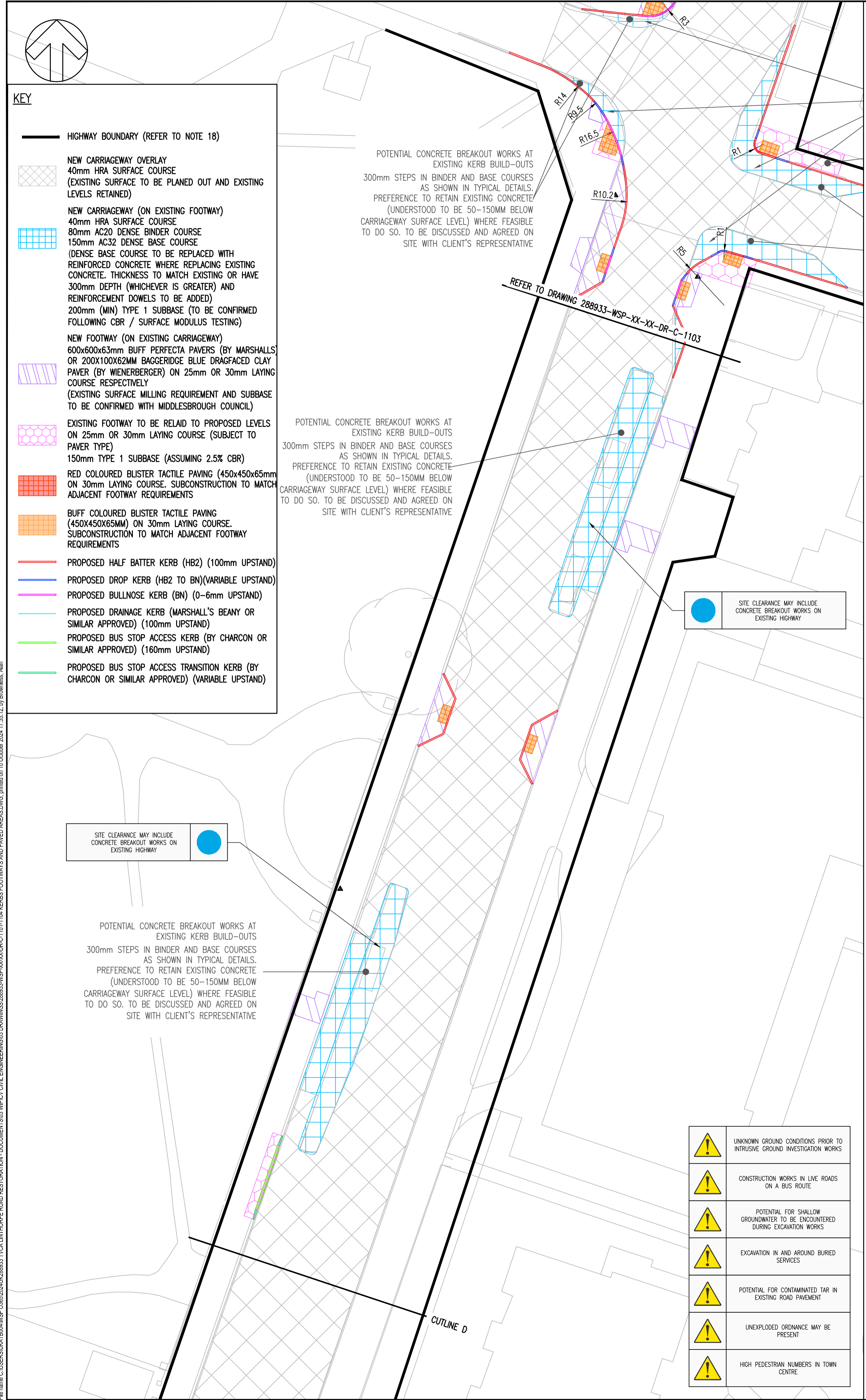
TITLE: **KERBS, FOOTWAYS AND PAVED AREAS (SHEET 3 OF 4)**

SCALE @ A1:	CHECKED:	APPROVED:
1:250	JS	PWW

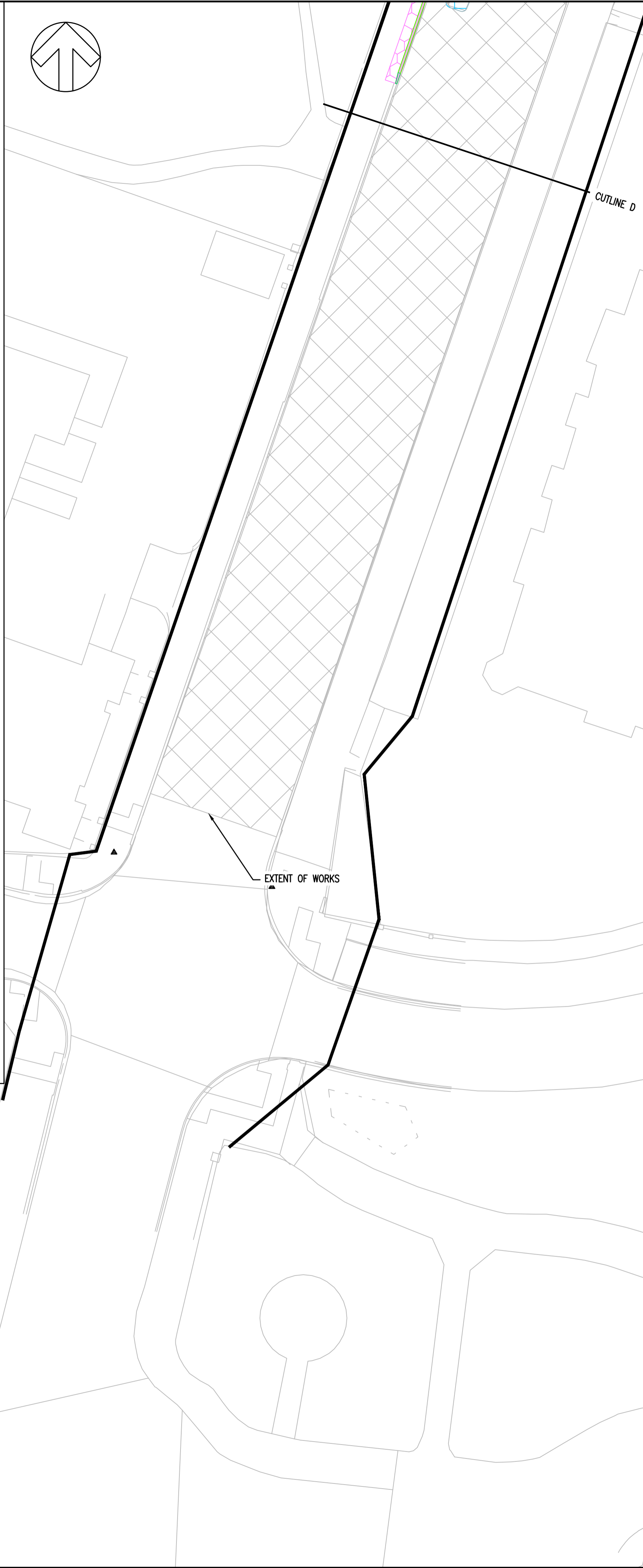
PROJECT NO:	DESIGNED:	DRAWN:	DATE:
2024UK293315	NR	NR	JULY 2024

DRAWING NO:	REV:
288933-WSP-XX-XX-DR-C-1103	P04

File name: C:\USERS\WATBOW\WSP_03602024\288933_TL\LINTHORPE ROAD RESTORATION - DOCUMENTS\03_MP\01_CIVIL_ENGINEERING\03_DRAWINGS\WSP-XX-XX-DR-C-1103-1104_KERBS FOOTWAYS AND PAVED AREAS.DWG, printed on 10 October 2024 17:28:30, by Bowles, Alan



- NOTES**
- THIS DRAWING SHOULD BE READ IN CONJUNCTION WITH ALL RELEVANT DOCUMENTATION DRAWINGS AND STANDARD DETAILS BUT WITH PARTICULAR REFERENCE TO THE FOLLOWING
-288933-WSP-XX-XX-DR-C-0101-0104
-288933-WSP-XX-XX-DR-C-1101-1104
-288933-WSP-XX-XX-DR-C-0201-0204
-288933-WSP-XX-XX-DR-C-0111
 - ALL DIMENSIONS ARE IN METRES UNLESS NOTED OTHERWISE.
 - THE ORIGINAL OF THIS DRAWING WAS PRODUCED IN COLOUR - A MONOCHROME COPY CANNOT BE RELIED ON.
 - INFORMATION REGARDING THE LOCATION AND DEPTH OF EXISTING SERVICES CANNOT BE GUARANTEED BY THE STATUTORY UNDERTAKER AND SHOULD BE INVESTIGATED. THE CONTRACTOR SHALL ESTABLISH THE PRECISE LINE AND DEPTH OF ALL EXISTING UTILITIES PRIOR TO COMMENCING ANY EXCAVATION WORK.
 - ALL PROPOSED KERBS ARE TO TIE INTO EXISTING KERB ALIGNMENTS.
 - ALL PROPOSED PAVING LEVELS ARE TO TIE INTO EXISTING PAVING LEVELS AND BE FREE FROM PROTRUDING STEPS FROM THE FINISHED LEVEL.
 - STREET FURNITURE, KERBS, GULLIES, ETC. SHALL BE COVERED WITH A SUITABLE MASKING MATERIAL TO PREVENT OVER SPRAY OF BITUMINOUS MATERIALS.
 - IN THE AREAS OF NEW CARRIAGEWAY CONSTRUCTION, WHERE THE WIDTH OF CARRIAGEWAY IS LESS THAN 1.0M, CONCRETE FILL TYPE ST1 / ST2 OR OTHER SIMILAR SPECIFICATION AS APPROVED BY MIDDLESBROUGH COUNCIL TO BE USED AS A REPLACEMENT FOR TYPE 1 SUBBASE.
 - AREAS OF FOOTWAY / HARDSTANDING & GRASSED VERGES TO BE MADE GOOD ONCE NEW KERBING / PAVING HAS BEEN INSTALLED.
 - ALL FOUNDATIONS SHOULD BE PROOF ROLLED PRIOR TO CONSTRUCTION WITH AND SOFT OR LOOSE POCKETS REPLACED WITH SUITABLE GRANULAR FILL MATERIAL.
 - WHERE SUBGRADE SURFACE MODULUS HAS A VALUE OF LESS THAN 30MPa (APPROX.2.5% CBR), IT MUST BE IMPROVED IN ACCORDANCE WITH DMRB CD 225.
 - THE DESIGN IS BASED ON TOPOGRAPHICAL SURVEY REFERENCE ASS 2963 BY ADH SITE SURVEYS LIMITED RECEIVED FEBRUARY 2021.
 - CONFLICTING INFORMATION SHOWN ON THE ENGINEER'S DRAWINGS OR DISCREPANCIES BETWEEN THE INFORMATION GIVEN BY THE ENGINEER AND THAT PROVIDED BY OTHERS MUST BE REFERRED TO THE ENGINEER BEFORE THE WORKS COMMENCE.
 - ALL REPAIRS TO THE EXISTING FOOTWAY RESULTING FROM CONSTRUCTION, NOT INDICATED ON THIS DRAWING, SHALL BE REINSTATED TO MATCH EXISTING.
 - WHERE IT IS NECESSARY TO AMEND BLOCK PAVING / KERB SIZES, IT IS ADVISED TO MEASURE AND CUT MATERIALS OFF-SITE, OR THE USE OF A BLOCK SPLITTER, TO REDUCE EXPOSURE TO DUST AND NOISE POLLUTION.
 - WHERE IT IS NECESSARY TO AMEND BLOCK PAVING / KERB SIZES, IT IS ADVISED TO MEASURE AND CUT MATERIALS OFF-SITE, OR THE USE OF A BLOCK SPLITTER, TO REDUCE EXPOSURE TO DUST AND NOISE POLLUTION.
 - THIS DRAWING SHOULD NOT BE USED FOR SETTING OUT AS THIS INFORMATION IS PROVIDED SEPARATELY.
 - THIS DRAWING IS BASED IN A WHOLE OR IN PART ON INFORMATION AND/OR DATA APPROVED BY A THIRD PARTY INCLUDING MIDDLESBROUGH COUNCIL AND AS SUCH IN CONTINGENT UPON THE ACCURACY AND COMPLETENESS OF THAT INFORMATION AND/OR DATA. WSP UK LIMITED SHALL NOT BE LIABLE IN RELATION TO ANY INACCURACY PRESENTED WITHIN THE DRAWING AND/OR IN RELATION TO ANY DISCREPANCY OR AMBIGUITY WITH ANY OTHER PROJECT DOCUMENTS SHOULD ANY DATA AND OR INFORMATION UPON WHICH RELIANCE HAS BEEN BASED BE INCORRECT OR OUT OF DATE OR HAVE BEEN CONCEALED, WITHHELD OR BE INCOMPLETE, MISREPRESENTED OR OTHERWISE NOT FULLY DISCLOSED TO WSP UK LIMITED.
 - REFERENCE SHOULD BE MADE TO INDIVIDUAL DISCIPLINE DRAWINGS AND THE DESIGN RISK MANAGEMENT SCHEDULE FOR FURTHER HEALTH AND SAFETY INFORMATION - NOT ALL RISKS ARE IDENTIFIED WITHIN THIS DRAWING



DO NOT SCALE

SAFETY HEALTH & ENVIRONMENTAL INFORMATION BOX

HEALTH AND SAFETY SYMBOLS LEGEND

	INDICATES A RESIDUAL RISK REQUIRING A COMPULSORY ACTION		INDICATES A RESIDUAL RISK FOR INFORMATION
	INDICATES A RESIDUAL RISK REQUIRING A PROHIBITIVE ACTION		INDICATES A RESIDUAL RISK AS A WARNING
	INDICATES AN ENVIRONMENTAL HAZARD		

This table is provided to assist the Principal Contractor to fulfil their obligations under the Construction Design & Management Regulations, 2015. It does not include residual risks that a competent Contractor will be aware of nor does it absolve the Principal Contractor of their legal responsibilities.
The reference number corresponds to the Design Risk Management Schedule.

DSSM (MPa)	30	40	50	70	100	150
THICKNESS (mm)	420	330	270	230	200	200

UNTIL TECHNICAL APPROVAL HAS BEEN OBTAINED FROM THE RELEVANT LOCAL AUTHORITIES OR STATUTORY BODIES, IT SHOULD BE UNDERSTOOD THAT ALL DRAWINGS ARE ISSUED AS PRELIMINARY AND NOT FOR CONSTRUCTION. SHOULD THE CONTRACTOR AND / OR EMPLOYER COMMENCE WORK PRIOR TO APPROVAL BEING GIVEN, IT IS ENTIRELY AT THEIR OWN RISK

REV	DATE	BY	DESCRIPTION	CHK	APP
P04	10/10/24	AB	UPDATED FOLLOWING CLIENT REQUIREMENTS	JS	PWW
P03	12/09/24	NR	UPDATED FOLLOWING CLIENT REVIEW	JS	PWW
P02	31/07/24	NR	ISSUED FOR CLIENT APPROVAL	JS	PWW
P01	23/07/24	NR	ISSUE FOR COST ESTIMATE	JS	PWW
DR	16/07/24	NR	DRAFT ISSUE	JS	PWW

DRAWING STATUS:

wsp

Amber Court, William Armstrong Drive, Newcastle upon Tyne, NE4 7YQ, UK
T+ 44 (0) 191 226 2000, F+ 44 (0) 191 226 2104
wsp.com

CLIENT:

TEES VALLEY

SITE/PROJECT:

LINTHORPE ROAD RESTORATION

TITLE:

KERBS, FOOTWAYS AND PAVED AREAS
(SHEET 4 OF 4)

SCALE @ A1:	1:250	CHECKED:	JS	APPROVED:	PWW
PROJECT NO:	2024UK293315	DESIGNED:	NR	DATE:	JULY 2024
DRAWING NO:	288933-WSP-XX-XX-DR-C-1104	REV:	P04		

- | | |
|--|---|
| | UNKNOWN GROUND CONDITIONS PRIOR TO INTRUSIVE GROUND INVESTIGATION WORKS |
| | CONSTRUCTION WORKS IN LIVE ROADS ON A BUS ROUTE |
| | POTENTIAL FOR SHALLOW GROUNDWATER TO BE ENCOUNTERED DURING EXCAVATION WORKS |
| | EXCAVATION IN AND AROUND BURIED SERVICES |
| | POTENTIAL FOR CONTAMINATED TAR IN EXISTING ROAD PAVEMENT |
| | UNEXPLODED ORDNANCE MAY BE PRESENT |
| | HIGH PEDESTRIAN NUMBERS IN TOWN CENTRE |